

McCALL AREA

PLANNING AND ZONING COMMISSION

Amended Agenda

December 7th, 2021 – 4:30 p.m.

McCall City Hall – Lower Level & MStTeams Teleconference
216 East Park Street, McCall, ID 83638

This meeting will be a hybrid, in-person and virtual meeting. Use a phone to call 208-634-8900, when prompted enter Conference ID 443175772#. If there are any questions, contact Brian Parker, City Planner (see contact info below).

The Meeting Room Occupancy Capacity in Legion Hall is 15, and subject to the 6-foot social distancing recommendation of Centers for Disease Control and Prevention. This allows space for 9 people in addition to the Commissioners and staff who are anticipated to be in attendance (6).

Social Distancing will be enforced, Masks are required in city facilities. The first 9 persons who appear will be allowed to be present in the meeting location. All other persons may be in attendance virtually via Teleconference.

COMMISSION MEETING – Begins at 4:30 p.m.

CALL TO ORDER AND ROLL CALL

1. REVIEW & APPROVAL OF MINUTES

- November 2nd, 2021 Minutes

2. PRELIMINARY DEVELOPMENT PLAN REVIEW MEETINGS

Pre-Application for a PUD, CUP, & SUB

TBD Clements or TBD Divot Lane/TBD Wooley Ave

Jim Fronk for Core Building Company, Inc: A pre-application for a PUD, SUB, and CUP to construct a multifamily, residential development on 1.778 acres. The property is zoned R8 – Medium Density Residential, and is more particularly described as:

Tax lot no. 404, situate in the SE ¼ of the NE ¼ of Section 9, T18N, R3E, B.M., City of McCall, Idaho.

Pre-Application for a PUD, CUP, & SUB

TBD Dawson Ave or TBD Wooley Ave

Jim Fronk for HTW McCall, LLC: A pre-application for a Rezone, PUD, SUB, and CUP to construct a multifamily, residential development on 3.59 acres. The property is zoned R8 – Medium Density Residential, and is more particularly described as:

Tax lot no. 408 in government Lot 1, situate in Section 9, T18N, R3E, B.M., City of McCall, Idaho.

Pre-Application for a PUD, CUP, & SUB

TBD Roosevelt Ave

Jim Fronk for Tom Koltes: A pre-application for a PUD, SUB, and CUP to construct a mixed residential, single family and multifamily development on 4.95 acres. The property is zoned R4 – Low Density Residential, and is more particularly described as:

Situate in the NW ¼ of the NE ¼ of Section 9, T18N, R3E, B.M., City of McCall, Idaho.

Pre-Application for a PUD, CUP, & SUB

410 S 3rd Street

Jeff Hatch and David Ferrette: A pre-application for a PUD, SUB, and CUP to construct unmanned storage buildings to accommodate small and large storage units, as well as RV and boat storage. The perimeter will be surrounded by a wall, and entry will be via remote access. The property is zoned **CC – Community Commercial** and is more particularly described as:

McCall Acreage Tax lots 57-B, 60, and 128, situate in the SW ¼ of the SE ¼ of Section 16, T18N, R3E, B.M, City of McCall, Idaho.

Pre-Application for a PUD, CUP, & SUB

TBD Elo Road

Ross Vontver for Where Toys Live, LLC: A pre-application for a PUD, SUB, and CUP to construct privately owned garage/storage condominiums to complement surrounding subdivisions. The property is zoned **RE – Rural Estate** and is more particularly described as:

Amended Tax Parcel No. 126, situate in the NW ¼ of the SW ¼ of Section 22, T18N, R3E, B.M, Valley County, Idaho.

3. CONSENT AGENDA

All matters which are listed within the consent section of the agenda have been distributed to each member of the McCall Area Planning and Zoning Commission for reading and study. Items listed are considered routine by the Commission and will be enacted with one motion unless a Commissioner specifically requests it to be removed from the Consent Agenda to be considered separately. There are no items for the Consent Agenda on December 7th, 2021.

4. OLD BUSINESS

DR-20-44 (ACTION ITEM)

641 Stockton Drive

Charles Carpy: A Design Review application to construct an approximately 5,100 square foot, two story residence with attached garage. The property is zoned R1 – Residential and is more particularly described as:

Tax Parcel Number 43 in Lot 53, Block 1 of West Place Subdivision situate in the SW ¼ of Section 15, T18N, R3E, B.M., Valley County, Idaho.

PUBLIC HEARING

5. NEW BUSINESS

DR-21-11 (ACTION ITEM)

1040 Chris Lane

Linzie & Amber Green: A Design Review application to construct a 1,740 square foot garage with storage loft addition to an existing residence. The property is zoned R1 – Residential and is more particularly described as:

Lot 95 of the Valley View Subdivision III situate in the SW ¼ of the NE ¼ of Section 30, T18N, R3E, B.M., Valley County, Idaho.

PUBLIC HEARING

DR-21-36 (ACTION ITEM)

111 West Lake Street

Lucas Vannoy for Todd Ketlinski: An Application for Design, Shoreline, and Scenic Route Review for a new, single-family residence on the site of the property's existing ADU. The new 6,296 square foot home will include 4 bedrooms, 5.5 bathrooms and an attached 3-car garage. No changes to landscaping on the lake side of the property are proposed. The property is zoned R4 – Low Density Residential, and is more particularly described as:

Lot 1 of the South Shore Subdivision, situated in the NE ¼ of the NE ¼ of the W ½ of Section 8, T18N, R3E, B.M. City of McCall, Valley County, Idaho.

PUBLIC HEARING

DR-21-39 (ACTION ITEM)

1415 Warren Wagon Road

Todd Allen Construction for Leonard Jordan: A design review, scenic route review, and shoreline environs review application to relocate an existing single-family residence, to construct an uncovered deck attached to relocated residence, and to construct a new 6,105 square-foot single-family residence. The property is zoned R4 – Low Density Residential and is more particularly described as:

Tax Lot No. 23, situate in Government Lot 4 of Section 5 & Lot 1 of Block 1, Recorder's Plat, situate in the NE ¼ of Section 8, T18N, R3E, B.M., Valley County, Idaho.

PUBLIC HEARING

~~**CUP-21-03 (ACTION ITEM)**~~ **CONTINUE TO JANUARY 4th, 2022**

~~**111 North Samson Trail**~~

~~An application for a Conditional Use Permit and Design Review to expand the existing use permitted to the school district by constructing an addition of approximately 12,000 sq. feet of classroom space on the Payette Lakes Middle School, as well as level out the school campus playing field and update the traffic circulation between the Middle School and Barbara Morgan Elementary School. The property is zoned CV – Civic and is more particularly described as:~~

~~A parcel of land situated in the S ½ of the NW ¼ of Section 15, T18N, R3E, B.M., City of McCall, Valley County, Idaho.~~

~~**PUBLIC HEARING**~~

CUP-21-04 (ACTION ITEM)

201 Rio Vista Boulevard

Susan Evans: An application for a Conditional Use Permit and Design Review to expand the use of the Bed & Breakfast/Short Term Rental and the property to be an Event/Activities Venue. The Conditional Use Permit and Design Review applications propose to do a high level of landscaping on the property as part of the Event Venue, and expand the size of the Carport, attached to the main building, from 1 car to 3 cars for a total building square footage of approximately 5,600 square feet. The 4.86 acre property is zoned R1 – 1 acre Residential, and is more particularly described as:

Situated in the E1/2 of the SW1/4 of the NW1/4 of the NW1/4 of Section 17, T18N, R3E, B.M., City of McCall, Valley County, Idaho.

PUBLIC HEARING

6. OTHER

- Signs approved administratively
 - i. Toby's Place – 506 Pine St
- Review 2022 Planning & Zoning Meeting Calendar

7. ADJOURNMENT

American with Disabilities Act Notice: The Planning and Zoning Commission meeting room is accessible to persons with disabilities. If you need assistance, contact City Hall at 634-7142. Please allow 48 hours.

McCALL AREA
PLANNING AND ZONING COMMISSION
Minutes
November 2nd, 2021 – 4:30 p.m.
McCall City Hall – Lower Level & MStTeams Teleconference
216 East Park Street, McCall, ID 83638

COMMISSION MEETING – Began at 4:30 p.m.

CALL TO ORDER AND ROLL CALL

Chairman Rob Lyons, Commissioner Liz Rock, Commissioner Christina Nemec, Commissioner Steve Clements, and Commissioner Ryan Kinzer were present. Commissioner Johanna Krahn and Commissioner Scott Tunnell were absent. Brian Parker (City Planner), Meredith Todd (Assistant Planner), Bill Punkoney (City Land Use Attorney), Rachel Santiago-Govier (Permit Technician), and Morgan Stroud (Staff Engineer) were also present.

1. REVIEW & APPROVAL OF MINUTES

- October 5th Minutes

Commissioner Clements made a motion to approve the October minutes, Commissioner Kinzer seconded the motion. All commissioners voted Aye and the motion carried.

2. PRELIMINARY DEVELOPMENT PLAN REVIEW MEETINGS

~~Pre-Application for a VAR~~

~~315 Memo Circle~~

~~Danny Johnson: A Variance pre-application to construct a single family residence within the required front yard setback area. The property is zoned R4 – Low Density Residential and is more particularly described as:~~

~~Portions of Lots 16 and 17, Rio Vista Subdivision situate in Section 17, T18N, R3E, B.M., City of McCall, Valley County, Idaho.~~

3. CONSENT AGENDA

All matters which are listed within the consent section of the agenda have been distributed to each member of the McCall Area Planning and Zoning Commission for reading and study. Items listed are considered routine by the Commission and will be enacted with one motion unless a commissioner specifically requests it to be removed from the Consent Agenda to be considered separately. Staff recommends approval of the following ACTION ITEMS.

ROS-21-13 (ACTION ITEM)

405 & 435 Knights Road

Joel Droulard for Kari Schmitz & Jayson Hicks: A Record of Survey Application to adjust the lot line between two existing parcels of record. The properties are zoned R1 – Residential 1 Acre, and are more particularly described as:

Lot 4, Whispering Woods Subdivision and the Easterly Portion of Lot 7, Block 3, West Place Subdivision, situate in the SE ¼ of the SW ¼ of Section 15, T18N, R3E, B.M., Valley County, Idaho.

ROS-21-14 (ACTION ITEM)

1580 & 1582 Mile High Drive

Joel Droulard for Ralph and Rhonda McKenzie: A Record of Survey Application to adjust the lot line between two existing lots of record. Lot 1 totaling 0.806 acres will total 0.737 acres; Lot 8 totaling 0.808 acres will total 0.878 acres. The properties are zoned R1 – Residential 1 Acre, and are more particularly described as:

Lots 1 & 8, Mile High Estates, situate in the SE ¼ of the SW ¼ of Section 6, T18N, R3E, B.M., Valley County, City of McCall, Idaho.

ROS-21-15 (ACTION ITEM)

403 E Park Street

Joel Droulard for Crawford Olson Investments, LLC: A Record of Survey Application to create two (2) parcels from the original three (3) lots. The new lot created from Lot 26 and a portion of Lot 25 will total 4,869 square feet; the new lot created from a portion of Lot 25 and Lot 24 will total 6,143 square feet. The properties are zoned CBD – Central Business District and is more particularly described as:

Lots 24, 25 & 26, Block 12, McCall's 1st Addition, situate in Government Lot 3 of Section 9, T18N, R3E, B.M., Valley County, City of McCall Idaho.

ROS-21-16 (ACTION ITEM)

302 Edgewater Circle & 341 Mather Road

Kenneth Oakeson: A Record of Survey Application to adjust the lot line between two existing lots of record. Lot 2 totaling 7,063 square feet will total 7,671 square feet; Lot 7 totaling 16,046 square feet will total 15,438 square feet. The property is zoned R4 – Low Density Residential and is more particularly described as:

Lots 2 & 7, Edgewater Heights, situate in Government Lot 4 of Section 8, T18N, R3E, B.M., Valley County, City of McCall Idaho.

ROS-21-17 (ACTION ITEM)

1001 Mission Street

Michael, Tamara & Diane DeBoer: A Record of Survey Application to combine two (2) parcels of 45.47 acres and 3.86 acres, respectively into a single parcel of 49.33 acres in size. The 3.86 acre parcel is a strip of abandoned Railroad right-of-way. The property is zoned RR – Rural Residential and is more particularly described as:

Lying West of the Airport Landing Field, situate in the W ½ of the NW ¼ of Section 21, T18N, R3E, B.M., Valley County, Idaho.

ROS-21-18 (ACTION ITEM)

1415 Warren Wagon Rd

Steven J. Millemann for Leonard Jordan: A Record of Survey Application to adjust the lot line between two (2) existing parcels of record. Parcel A totaling 0.295 acres will total 0.375 acres; Parcel B totaling 0.859 acres will total 0.742 acres. The property is zoned R4 – Low Density Residential and is more particularly described as:

Tax Lot No. 23, situate in Government Lot 4 of Section 5 & Lot 1 of Block 1, Recorder's Plat, situate in the NE ¼ of Section 8, T18N, R3E, B.M., Valley County, Idaho.

Comissioner Nemec made a motion to approve the consent agenda, Comissioner Rock seconded the motions, all commissioners voted aye and the motion carried.

4. OLD BUSINESS

None

5. NEW BUSINESS

PUD-21-01, SUB-21-04, & DR-21-31 (ACTION ITEM)

116 Thula Street

Tim Davis for Rywest Homes, Inc: A Planned Unit Development General Plan, Subdivision Preliminary Plat, and Design Review application to construct a 26-unit condominium. The property is zoned CC – Community Commercial and is more particularly described as:

Tax parcel number 45A and 45B of McCall Acreage situated in the SE ¼ of the NW ¼ of Section 16, T18N, R3E, B.M., City of McCall, Valley County, Idaho.

PUBLIC HEARING

Tim Davis of Rywest Homes of 7872 Richey Rd, Fruitland, Idaho presented his application for the Black Bear Condominium at 116 Thula Street and mentioned his having purchased the property with its previous Planned Unit Development approval that had expired, bringing the project through the Planning and Zoning Commission again. The project will contain the same number of deed-restricted local-housing units. He mentioned being amenable to the request to add a curb-gutter sidewalk or separated pathway. Mr. Davis mentioned that Epikos is working on a design for an outdoor barbecue/kitchen area and a horseshoe pit to include as amenities in addition to the Parks recommendation of an *in-lieu* fee and/or a bike path. He hopes to only provide the Bike Path as his parks dedication because it is a good investment in itself.

Brian Parker, City Planner, presented the staff report. Mr. Parker gave the history of the application and the background of some changes to City Code since the original Entitlement. In this new entitlement process, there have been two comments from agency/advisory groups. The first, the McCall Airport expressed concerns about the developments proximity to the Airport and being placed below the flight path. The developer would be required to increase noise insulation, provide an avigation easement, and notify all future owners/tenants that the airport is nearby. The second, the Parks and Recreation Advisory Committee, requested that in addition to the originally planned bike path contributing to the Pathways Master Plan, the PRAC would also recommend a *Parks in-lieu fee* to fund future parks development. The property is zoned CC, Community Commercial, and abuts a residential neighborhood to the north, offering a strong buffer between the Commercial and Residential Zones.

Commissioners had some questions about how accurate the property valuation or the use of that number to ask for a *Parks in-lieu fee* was, given the dedication of the pathway. They also asked for confirmation that the Right-Of-Way to the north was owned by the city. Mr. Parker confirmed that in discussion with the County Assessor on the afternoon of November 2nd, all documentation indicated the city owns the unimproved Right-of-Way. As for the *in-lieu fee*, Mr. Parker confirmed that this was the first application to request both a fee and Parks Improvement. There was discussion of what kind of accommodations could balance the appreciation of property value with the already included improvements and amenities.

Commissioners also had questions about the stormwater drainage and feasibility of curb and gutter installation along Thula St. Morgan Stroud, Staff Engineer, mentioned that there will be drainage requirements needed with some level of engineering, but there are other systems in place in the area. Ms. Stroud mentioned that there was possibility to place a side-by-side sidewalk and swale instead of sidewalk, curb and gutters. Chairman Lyons and Commissioner Nemecek mentioned that a separated, paved pathway seemed much more reasonable than a curb and gutter sidewalk. Ms. Stroud said that a separated pathway would be entirely possible, it would just require further design. The primary reason for staff recommending a curb and gutter sidewalk was the recommendation from city code, but if a pathway is more suitable it may be preferred.

Chairman Lyons opened the Public Hearing.

Mary and Monte Fisher of 200 Thula St, McCall spoke against the application on the basis of their using the McBride ROW to the north of the property to access their property for more than 30 years, having paid for road maintenance and done landscaping on the property. They would like to maintain their privacy and safety and keep the ROW undeveloped. The Fisher's also submit a written comment in writing on November 2nd. Mrs. Fisher did mention that she thinks if the bike path could be relocated, then she would be in favor of the application.

With further insight on the pathway, Bill Punkoney, City Attorney, mentioned he was available to weigh in when needed. Commissioner Clements mentioned that ultimately he feels the Planning & Zoning Commission is not qualified to determine ownership of land. Mr. Punkoney mentioned that given the level of evidence suggesting City ownership of the pathway/McBride Right-of-Way, it may be better to encourage a constructive easement with the northerly neighbors to create a common solution if needed and desired.

Commissioner Kinzer asked whether landscaping along the northern property area between the northern property owners and the pathway could function as a privacy buffer. Commissioners agreed this was a valuable recommendation.

Mr. Davis weighed back in and discussed his desire to provide solutions that could be of assistance to both the Fisher's and the Condominium site. He mentioned his rough numbers to estimate the cost of the pathway from Thula Street to the bike path to the west was between \$70,000 to \$90,000 for 950 feet of path. He also mentioned he would be interested in building a fence, doing landscaping, or other privacy improvements to meet the neighboring property owners needs. He mentioned he could consider moving one of the buildings to shift the development southward, but that would likely be costly and complex.

Commissioners expressed the desire to keep the bike path and work out the placement and privacy buffering with the north neighbors. They also mentioned disagreeing with the PRAC's recommendation to require both the fee and the bike path and their desire to keep the bike path and not require the fee as both would be excessive. Commissioners asked whether the concerns of the airport had been addressed and Mr. Parker indicated that there were adequate conditions to address safety for the development in relationship to the airport. Commissioners

Mary Fisher of 200 Thula St. spoke again and mentioned she had secured legal counsel and intends to adverse possess the public right-of-way.

Mr. Punkoney told the commission that although a lawsuit had been expressed as desired, nothing had yet been served and that the Commission should consider the application based on the facts and the facts only.

Given that the Final Plat will have to return to the Planning & Zoning Commission, Commissioner Rock mentioned that the concerns with the neighbors could be readdressed through the Final Plat process, rather than the Preliminary Plat. The Commissioners did agree that a revised landscaping plan to show screening between the northern neighbors should be a condition of approval.

Commissioner Nemeč made a motion to recommend approval of PUD-21-01, SUB-21-04, and DR-21-31 with condition 4 modified to remove the curb gutter and sidewalk and changed to a pathway, condition 12 removed, condition 13 modified to include revised landscape plans to address privacy concerns, and remove condition 15.

Commissioner Rock seconded.

A Roll call vote was held:

Commissioner Clements – Yes

Commissioner Nemeč – Yes

Commissioner Rock – Yes

Commissioner Kinzer – Yes

Chairman Lyons – Yes.

- Signs approved administratively.
 - i. **SG-21-10 - 324 Lake St. – Hess-Harper**

Assistant Planner Todd presented the sign permit application to amend the existing signage entitlement for the property at 324 W Lake St., granted via a Conditional Use Permit. The applicants hope to move the current non-conforming (but permitted) free standing sign structure and landscaped berm it rests upon approximately 7-12 feet south of the right of way to a closer-to-conforming location. Given the CUP directly entitled the sign to be in a non-conforming location, amending the signage entitlement to a less-nonconforming location allows for the Conditional Use Permit to remain in good-standing.

6. ADJOURNMENT

Commissioner Clements made a motion to adjourn, Commissioner Rock seconded the motion. All commissioners voted Aye and the meeting was ended at 5:49PM

Signed:

Attest:

Robert Lyons, Chairman
McCall Area Planning and Zoning Commission

Brian Parker
City Planner

City of McCall

216 East Park Street
McCall, ID 83638
Phone (208) 634-7142
Fax (208) 634-3038



City of McCall
COMMUNITY
DEVELOPMENT

PRE-APPLICATION MEETING FORM

The Applicant will present to the Administrator and Commission, in a scheduled meeting, but a non-public hearing, a *preliminary development plan* for review and discussion. All materials to be reviewed and discussed shall be provided, or be available, to Commission members at a regular scheduled meeting of the Commission. The pre-application meeting (no fee involved) is required for all land use applications except Record of Survey, Design Review and Scenic Route applications. Please contact the Community Development Department at (208) 634-7052 to schedule a pre-application meeting.

Please check all that apply:

- Annexation or Rezone (ZON)
- Conditional Use Permit (CUP)
- Planned Unit Development (PUD) General Plan
- Planned Unit Development (PUD) Final Plan (no fee)
- Subdivision (SUB) Preliminary Plat
- Subdivision (SUB) Final Plat (no fee)
- Street Vacation (VAC)
- Variance (VAR)

Date Received: **November 16, 2021**

Applicant Information

Applicant: Core Building Company, Inc. Phone/Email: [REDACTED]

Applicant's Mailing Address [REDACTED]

Agent/Representative: Jim Fronk Phone/Fax/Email: 208 634 8093

Agent/Representative's Mailing Address: P.O. Box 56, McCall, ID 83638

Address of Subject Property: Divot Lane & Wooley Ave.

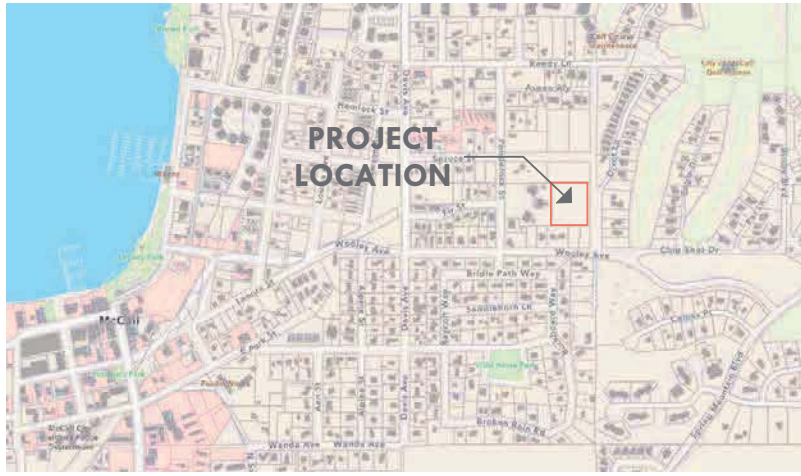
Legal Description of Property: Tax No. 404 in SE 4, NE 4, S9, T18N R3

Zoning District of Property: R8

Explain the general nature of what is proposed:

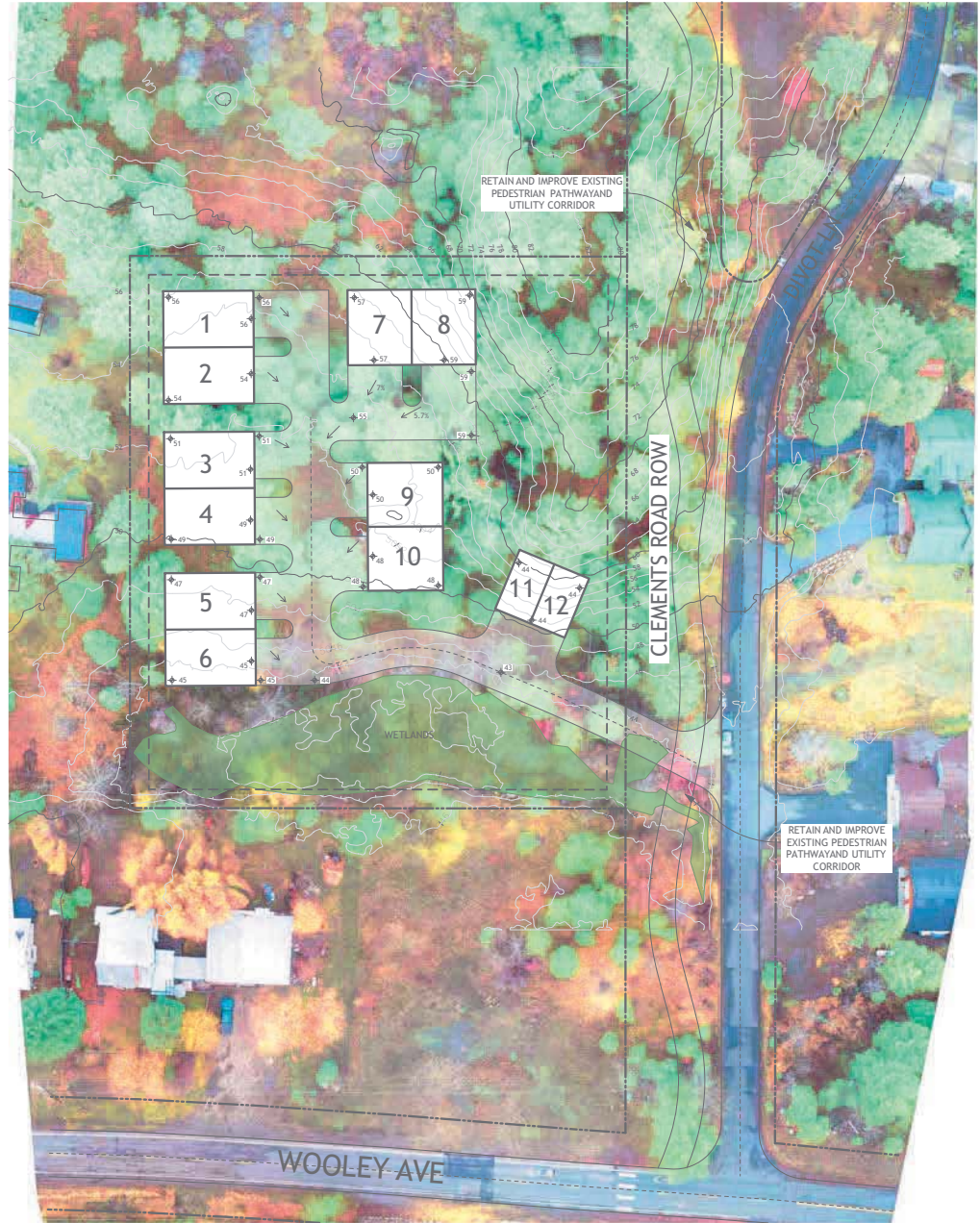
Residential - Multifamily - 1.778 acres.

*Please submit four (4) hard copies of the preliminary development plan with this form, not greater than 11" by 17" in size. Please also include a digital copy of the preliminary development plan.



PROJECT LOCATION

2 VICINITY MAP
NOT TO SCALE



1 SITE PLAN WITH AERIAL
SCALE: 1" = 30'

THIS IS INTENDED TO BE PRINTED ON A 24"x36" SHEET. ALL OTHER SHEET SIZES WILL NOT BE PRINTED TO SCALE.



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+
ARCHITECTURE

McCALL OFFICE
303 Colorado Street
P.O. Box 2490
McCall, Idaho 83638
Tel. 208.634.4540

www.EpikosDesign.com

CLEMETS ROAD
McCALL, ID 83638

Stamp:

DESIGN DEVELOPMENT SET

Revisions:

Date: 11/15/2021

Project No: 2113

Drawn by: EB

Checked by: DP

Sheet:

A2-1

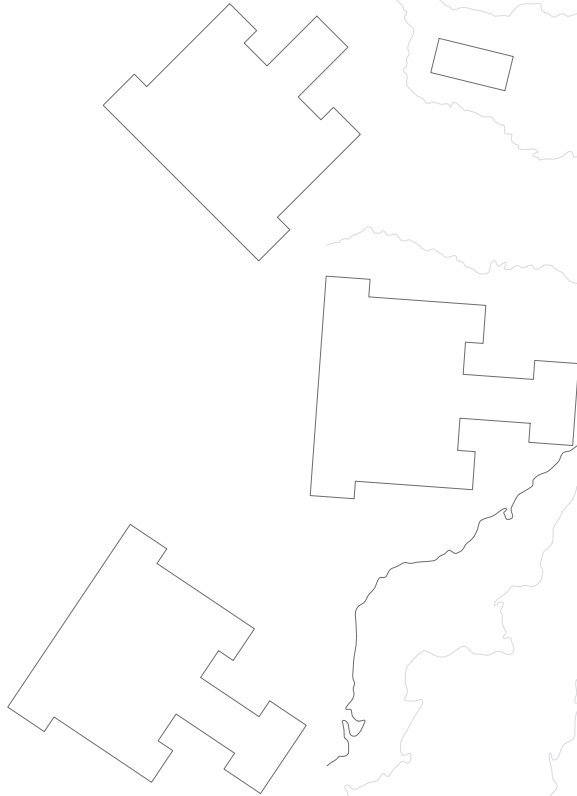
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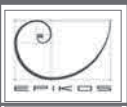
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**CLEMENTS ROAD
CONCEPTUAL DEVELOPMENT PLAN**

- 12 TOTAL UNITS
- 6 2-STORY DUPLEX UNITS @ 30' X 48' +/-
WITH PARKING BELOW
- 4 2-STORY DUPLEX UNITS @ 34 X 40' +/-
WITH PARKING BELOW
- 2 2-STORY DUPLEX UNITS @ 20' X 34' +/-
WITH PARKING BELOW



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CLEMENTS ROAD
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DESIGN DEVELOPMENT SET

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Date:	11/15/2021
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Drawn by:	EB
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Sheet:
A2-2

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1 SITE PLAN
SCALE: 1" = 20'
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216 East Park Street
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Phone (208) 634-7142
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PRE-APPLICATION MEETING FORM

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- Street Vacation (VAC)
- Variance (VAR)

Date Received: November 16, 2021

Applicant Information

Applicant: HTW McCall, LLC.

Phone/Email

Applicant's Mailing Address:

Agent/Representative: Jim Fronk

Phone/Fax/Email: 208 634 8093/jamesfronkconsulting@gmail.com

Agent/Representative's Mailing Address: P.O. Box 576, McCall, ID 83638

Address of Subject Property: Wooley and Dawson

Legal Description of Property: Tax No. 408 in Gov't Lot 1, Section 9, T18N, R3E & McCalls 1st. Add, Amend. Lot B, BK's, 23 & 24.

Zoning District of Property: R8

Explain the general nature of what is proposed:

Residential - Multifamily - 3.59 acres.

*Please submit four (4) hard copies of the preliminary development plan with this form, not greater than 11" by 17" in size. Please also include a digital copy of the preliminary development plan.

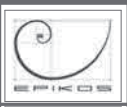


2 VICINITY MAP
NOT TO SCALE



JAMES FRONK CONSULTING
P.O. Box 576
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1 CONCEPT WITH AERIAL
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DAWSON AVENUE
McCall, ID 83638

Stamp:

DESIGN DEVELOPMENT SET

Revisions:

Date: 11/15/2021

Project No: 21-41

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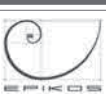
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**DAWSON AVENUE
CONCEPTUAL DEVELOPMENT PLAN**

30 TOTAL UNITS
 24 2-STORY DUPLEX UNITS @ 995 SF +/-
 WITH PARKING BELOW
 6 2-STORY TRIPLEX UNITS @ 960 SF +/-
 WITH PARKING BELOW



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DAWSON AVENUE
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A2-2

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THIS IS INTENDED TO BE PRINTED ON A 24"x36" SHEET. ALL OTHER SHEET SIZES WILL NOT BE PRINTED TO SCALE.

City of McCall
216 East Park Street
McCall, ID 83638
Phone (208) 634-7142
Fax (208) 634-3038



PRE-APPLICATION MEETING FORM

The Applicant will present to the Administrator and Commission, in a scheduled meeting, but a non-public hearing, a *preliminary development plan* for review and discussion. All materials to be reviewed and discussed shall be provided, or be available, to Commission members at a regular scheduled meeting of the Commission. The pre-application meeting (no fee involved) is required for all land use applications except Record of Survey, Design Review and Scenic Route applications. Please contact the Community Development Department at (208) 634-7052 to schedule a pre-application meeting.

Please check all that apply:

- Annexation or Rezone (ZON)
- Conditional Use Permit (CUP)
- Planned Unit Development (PUD) General Plan
- Planned Unit Development (PUD) Final Plan (no fee)
- Subdivision (SUB) Preliminary Plat
- Subdivision (SUB) Final Plat (no fee)
- Street Vacation (VAC)
- Variance (VAR)

Date Received: **November 16, 2021**

Applicant Information

Applicant: Tom Koltjes Phone/Email: [REDACTED]

Applicant's Mailing Address: [REDACTED]

Agent/Representative: Jim Fronk Phone/Fax/Email: 208 634 8093 / jamesfronkconsulting@gmail.com

Agent/Representative's Mailing Address: P.O Box 576 McCall, Id 83638

Address of Subject Property: Roosevelt Property - Undeveloped property by McCall Ave.

Legal Description of Property: NW1/4, NE1/4, Section 9, Township 18 North, Range 3 East

Zoning District of Property: R4

Explain the general nature of what is proposed:

Residential Mix Use - Single Family - Multifamily - 4.95 acres

*Please submit four (4) hard copies of the preliminary development plan with this form, not greater than 11" by 17" in size. Please also include a digital copy of the preliminary development plan.

Roosevelt - Concept One

Scale: 1 inch = Approx. 100 feet

Concept Units:
7 Single Family
6 Multi-family (Townhomes)

November 13, 2021



Roosevelt - Concept Two

Scale: 1 inch = Approx. 100 feet

Concept Units:
11 Single Family

November 13, 2021



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- Subdivision (SUB) Final Plat (no fee)
- Street Vacation (VAC)
- Variance (VAR)

Date Received: **November 16, 2021**

Applicant Information

Applicant: _____ Phone/Email: _____

Applicant's Mailing Address _____

Agent/Representative: _____ Phone/Fax/Email: _____

Agent/Representative's Mailing Address: _____

Address of Subject Property: 410 S 3rd St, McCall, ID 83638 Parcel nos. RPM00000168695 & RPM00000168690

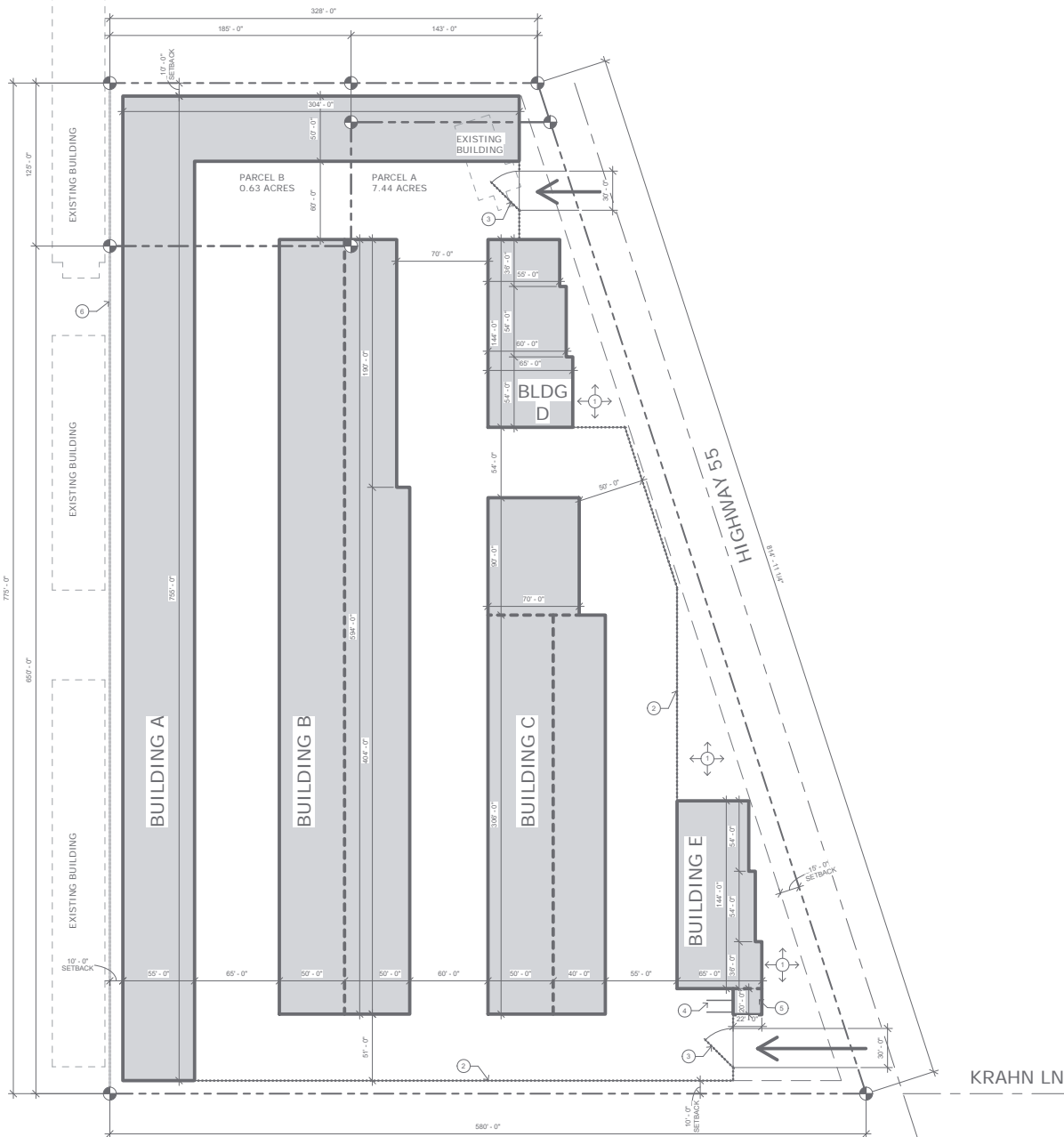
Legal Description of Property: MCCALL ACREAGE PT AMENDED TAX NO. 57-B; TAX NO. 60 IN SW4 SE4 S16 T18N R3E & MCCALL ACREAGE TAX NO. 126 IN SW4 SE4 S16 T18N R3E

Zoning District of Property: CC - Community Commercial

Explain the general nature of what is proposed:

*Please submit four (4) hard copies of the preliminary development plan with this form, not greater than 11" by 17" in size. Please also include a digital copy of the preliminary development plan.

AIRPORT



SITE PLAN
SCALE: 1" = 40'-0"



PARCEL RECAP			
PARCEL A	RPM00000168695	7.44 acres	324,289 SF
PARCEL B	RPM00000168690	0.63 acres	27,561 SF
TOTAL		8.08 acres	351,850 SF

AREA RECAP	
BUILDING A	53,975 SF
BUILDING B	57,500 SF
BUILDING C	33,840 SF
BUILDING D	8,730 SF
BUILDING E	8,990 SF
TOTAL	163,035 SF

KEYNOTES	
①	PROPOSED LANDSCAPED BERM
②	PROPOSED FENCE
③	PROPOSED GATE
④	PROPOSED PARKING
⑤	PROPOSED OFFICE BUILDING
⑥	EXISTING FENCE



HATCH DESIGN
ARCHITECTURE
200 W. WYOMING ST. SUITE 100
BOZEMAN, IDAHO 83724
OFFICE: (208) 475-3004
CELL: (208) 475-3005
COPYRIGHT © 2021
HATCH DESIGN ARCHITECTURE
BOZEMAN, MONTANA, U.S.A.

NEW CONSTRUCTION FOR:
LUXELOCKER - MCGALL
410 S 3RD ST AND 407 W COLCHESTER DR

DATE: NOVEMBER 2021
DRAWN BY: RH, NL
CHECKED BY: JLS
JOB NUMBER: MKI 211

SHEET NUMBER
SITE PLAN

A-1.0

City of McCall

216 East Park Street
McCall, ID 83638
Phone (208) 634-7142
Fax (208) 634-3038



City of McCall
COMMUNITY
DEVELOPMENT

PRE-APPLICATION MEETING FORM

The Applicant will present to the Administrator and Commission, in a scheduled meeting, but a non-public hearing, a *preliminary development plan* for review and discussion. All materials to be reviewed and discussed shall be provided, or be available, to Commission members at a regular scheduled meeting of the Commission. The pre-application meeting (no fee involved) is required for all land use applications except Record of Survey, Design Review and Scenic Route applications. Please contact the Community Development Department at (208) 634-7052 to schedule a pre-application meeting.

Please check all that apply:

- Annexation or Rezone (ZON)
- Conditional Use Permit (CUP)
- Planned Unit Development (PUD) General Plan
- Planned Unit Development (PUD) Final Plan (no fee)
- Subdivision (SUB) Preliminary Plat
- Subdivision (SUB) Final Plat (no fee)
- Street Vacation (VAC)
- Variance (VAR)

Date Received **RECEIVED**

NOV 16 2021

Applicant Information

Applicant: Where Toys Live, LLC Phone/Email: [REDACTED]

Applicant's Mailing Address: [REDACTED]

Agent/Representative: Ross Vontver Phone/Fax/Email: 425-214-3387/ross@the-trove.com

Agent/Representative's Mailing Address: 2614 Camas Ave NE Renton WA 98056

Address of Subject Property: TBD Elo Rd. McCall, ID 83638 (vacant land)

Legal Description of Property: Amended Tax # 126 IN NW SW S22 T18N R3E / Parcel ID: RP18N03E225425

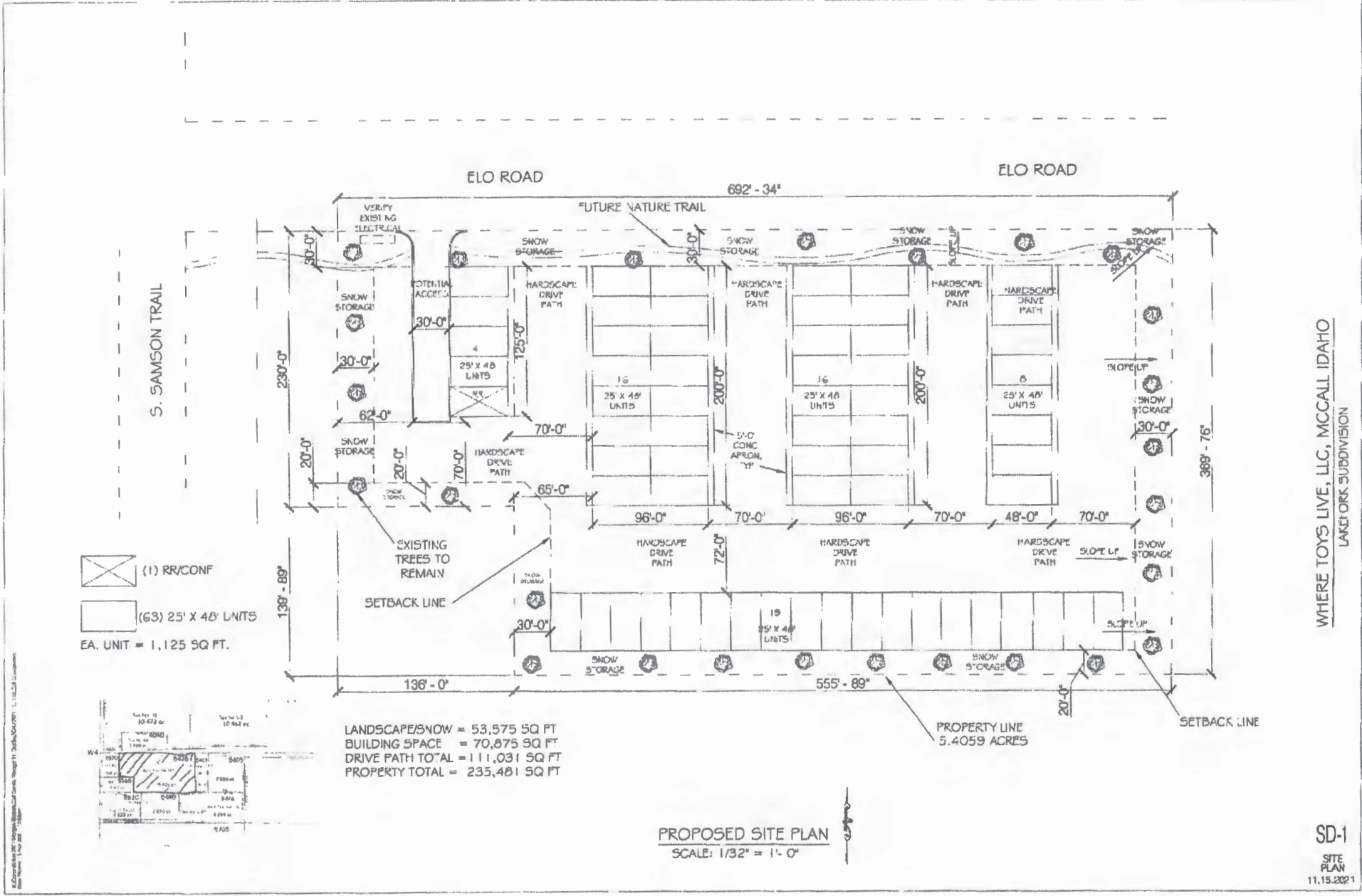
Zoning District of Property: RE

Explain the general nature of what is proposed:

New privately owned garage condominiums or "auxiliary dwelling units" to compliment surrounding subdivisions.

Roughly similar in concept to recently constructed garage condominiums near the SEC of Boydston St. and Rio Vista Blvd. in McCall

*Please submit four (4) hard copies of the preliminary development plan with this form, not greater than 11" by 17" in size. Please also include a digital copy of the preliminary development plan.



S. SAMSON TRAIL



ELO ROAD

ELO ROAD

692' - 34"

FUTURE NATURE TRAIL

WHERE TOYS LIVE, LLC, MCCALL IDAHO
LAKETOWN SUBDIVISION

-  (1) RR/CONF
-  (63) 25' X 48' UNITS
- EA. UNIT = 1,125 SQ FT.

LANDSCAPE/SNOW = 53,575 SQ FT
 BUILDING SPACE = 70,875 SQ FT
 DRIVE PATH TOTAL = 111,031 SQ FT
 PROPERTY TOTAL = 235,481 SQ FT

PROPERTY LINE
5.4059 ACRES

PROPOSED SITE PLAN
SCALE: 1/32" = 1'-0"

SD-1
SITE PLAN
11.15.2021

**McCall Area Planning and Zoning Commission
Staff Report**

DR-20-41
641 Stockton Dr.
Carpy Residence

August 3, 2021

Applicant: Charles Carpy
Application: Design Review for a Single-Family Residence in excess of 3,500 sq. ft.
Zoning: R1 – Residential 1 Acre

Background: On February 2, 2021, the Commission held a public hearing and reviewed the subject application. At that time, the application consideration was continued to March 2, 2021 to request the applicant to complete the application, including a landscape plan that would help screen the building from the street.

In an e-mail to Morgan Bressaw on February 16, 2021, the applicant’s representative indicated that they had insufficient time to put all the materials together by the deadline and requested a continuance until the Commission’s April 6 meeting. At the hearing on March 2, written communication and testimony were received in opposition to the application and in the applicant’s representation of the communications between the city and themselves. The item was continued until April 6, 2021, and then until May 4, 2021.

On May 4, 2021, the Planning & Zoning Commission heard the continued public hearing on the revised proposal. The application was remanded back to staff to work with the applicant regarding the following revisions:

- Additional landscape screening and species changes
- Additional windows on blank walls/garage doors
- Additional architectural elements to break up building mass

On June 18, 2021, the staff issued a letter requesting the above-mentioned documentation and additional information requested by the City Engineer. The City has since received revised plans showing additional windows on garage doors, side, and rear walls, additional beams on the front elevation, and additional landscaping.

On August 3, 2021, the Planning & Zoning Commission heard the continued public hearing on the revised proposal. The application was once again remanded back to staff to work with the applicant regarding the following revisions:

- Landscaping related modifications:
 - Additional trees and landscaping near the building to provide screening
 - Additional evergreen plantings to provide year-round screening
 - A 5-6’ berm near the north and western property boundary to elevate new plantings
 - A plan for irrigating new plantings to ensure survival
 - A professionally drawn site plan

- A 3D rendering of the proposed landscaping around the building
- An aerial map with the location of the building correctly shown on the site
- Building architecture related modifications:
 - Windows
 - Improved siding/trim to reduce the industrial appearance

Description

A Design Review application to construct a residence with attached garage of approximately 5,100 sq. ft. The structure is designed as a large metal barn type structure with garage on half of the lower level, and residence on the remainder of the lower level and half of the upper level.

A building permit for the structure was issued on August 5, 2020. The permit was issued in error because it is a single-family residence in excess of 3,500 sq. ft. and should have first required design review approval by the McCall Area Planning and Zoning Commission.

A neighbor complaint on September 16, 2020, brought the issue to the attention of staff. After review, a letter was issued to the property owner on September 29, 2020, alerting them to the issue and letting them know that Design Review would still be required and that any further work on the project would be at their own risk because during review the Planning and Zoning Commission could request modifications to the structure. After repeated incomplete submittals of documentation, a stop work order was issued on July 14, 2021, pending completion of the design review application.

Zoning: R1 –Residential One Acre Property Size: 1.16 acres (50,438 sq. ft.)

Lot Coverage: Per MCC 3.3.06, the allowable coverage for this parcel is 18.5% or 9,331 sq. ft. The proposed structure has a footprint of 3,366 sq. ft, and the driveway is 4,400 sf. for a lot coverage of 9.7%.

Building Height: Per MCC 3.3.03, the maximum allowed height is 35 ft. The maximum height 29’ 6”.

Setbacks: Per MCC 3.3.03, the minimum required setbacks for this parcel are 25 ft. from the front property line, 15 ft. from either side property line, and 20 ft. from the rear property line. The applicant is proposing setbacks of 64 ft. from the front and rear property lines, 50 ft. from the easterly side property line, and 21 ft. from the westerly side property line.

Design Guideline Narrative *(Staff analysis in italics)*

1. **Building Scale:** *The existing structure is large and contains minimal architectural detail to help minimize the scale. The two-story vertical walls, large blank garage doors, lack of accenting, and lack of windows all contribute to the appearance of a large, industrial structure. The applicant is proposing to improve the appearance through the addition of a shed roof over the garage doors, the addition of wood accents, the installation of additional windows in the garage doors and on the north and south elevations. Additionally, there are substantial setbacks around the structure, which the applicant is proposing to construct 6-7’ tall landscaped berms within the north and western setback areas and plant the following:*
 - *Twelve (12) 8-9’ tall Colorado Spruce trees*
 - *Ten (10) 8-9’ tall Norway Spruce trees*
 - *Seven (7) 1.5” caliper Quaking Aspen trees*
 - *Four (4) 2” caliper Quaking Aspen trees*

- Seven (7) 5-gallon Mountain Snowberry bushes
- Twenty-four (24) 2-gallon Native Rose bushes
- Twenty-five (25) 5-gallon Feather Reed Grass bunches
- Forty (40) 1-gallon assorted ground cover and perennial flowers

There is a substantial existing stand of trees along the eastern portion of the property. It is staff's opinion that the proposed landscaping and berming will provide adequate screening of the building. While the structure is still large and primarily industrial in appearance, the additional architectural details will aid in minimizing the scale.

2. **Pedestrian Character:** *Modifications have been made to the original design to accentuate the entrance to the residence with an enhanced trim around the front porch and the addition of a front patio entry. Other than the entrance there is nothing that distinguishes the structure from an industrial building, lacking pedestrian character.*
3. **Blank Walls:** *The building design includes large, uninterrupted metal walls. The applicant has proposed to add windows to the north and south walls and to the garage doors.*
4. **Parking:** *The project includes adequate parking for its residential use, pursuant to MCC 3.8.06.*

Guidelines for All Projects

Site Planning

1. **Building Siting:** *The building is sited in the northwest portion of the lot, adjacent to the driveway easement. The remaining portion of the lot will remain undisturbed.*
2. **Adjacent Buildings and Uses:** *The proposed design is incompatible with adjacent residential structures. The building design is more industrial than residential, and substantially out of character with the surrounding neighborhood residences.*
3. **Preserve Vegetation and Wildlife:** *Few trees are proposed to be removed except within the building and drive footprint. New landscaping is proposed.*
4. **Preserve Views:** *The proposed project does not block any significant views.*
5. **Preserve Skylines:** *The proposed building site is not on a skyline or ridge top.*
6. **Preserve Natural Drainage:** *Final engineering approval of such by the City Engineer would be required prior to issuance of a building permit to protect natural drainage.*
7. **Cluster Buildings:** N/A
8. **Street Alignment:** *An existing driveway easement serves the subject parcel.*
9. **Retaining Walls:** *No retaining walls are proposed.*

10. **Snow Storage:** *Snow storage areas are not identified; however, the large parcel size indicates that adequate snow storage is likely available.*
11. **Roof Design and Snow:** *The applicant is proposing to install snow rails on the primary roof and shed roof over the garage doors to prevent snow from falling in front of pedestrian and vehicle entrances.*
12. **Use the Sun:** *The proposed design does include a large, covered deck and patio that will minimize cold, dark, unpleasant areas.*
13. **Screen Service Areas:** *N/A*
14. **Off-Street Parking:** *N/A*
15. **On-Site Parking:** *Adequate on-site parking is provided for its residential use per MCC 3.3.062.*
16. **Circulation Needs – Pedestrian and Vehicles:** *N/A*

Architecture

1. **Enhance McCall Classic Styles:** *The design is not consistent with a classic McCall residential structure. Although the use of metal as a building material is part of the McCall architectural vernacular, and the building form simulates a traditional barn.*
2. **Minimize Scale:** *There are no varied roof lines, and minimal architectural details are used to help minimize the appearance of the building scale. The blank walls, large garage door, lack of ground-level windows and mostly blank walls makes the structure appear to be larger than it is.*
3. **Building Additions:** *N/A*
4. **Roof Lines:** *The project does not include varied roof lines.*
5. **Mechanical Equipment:** *No exterior mechanical equipment is proposed.*
6. **Multi-Unit Structures:** *N/A*
7. **Balconies and Porches:** *The proposed design does include a large, covered deck and patio that are the only interesting architectural features that help to break up the scale of the building and focus attention on the residential side of the structure.*
8. **Exterior Doorways:** *The exterior doorway locations complement the design of the building and serve the intended functions.*
9. **Wall Materials:** *The proposed wall materials are metal to minimize maintenance.*
10. **Shop Front Design:** *N/A*

11. **Wall Colors:** *The proposed materials are beige, green, and grey metal.*

Landscaping and Site Design

1. **Light Fixtures:** *Goose neck lighting fixtures have been located on the building elevations. Prior to issuance of a certificate of occupancy, the applicant shall provide fixture details that comply with McCall's Outdoor Lighting Ordinance.*
2. **Fences:** *A cedar split rail fence is shown along the property lines.*
3. **Retaining Walls:** *No retaining walls are proposed.*
4. **Paving and Streetscapes:** N/A
5. **Landscaping Plan:** *A landscaping plan has been provided that includes the following:*
 - *Twelve (12) 8-9' tall Colorado Spruce trees*
 - *Ten (10) 8-9' tall Norway Spruce trees*
 - *Seven (7) 1.5" caliper Quaking Aspen trees*
 - *Four (4) 2" caliper Quaking Aspen trees*
 - *Seven (7) 5-gallon Mountain Snowberry bushes*
 - *Twenty-four (24) 2-gallon Native Rose bushes*
 - *Twenty-five (25) 5-gallon Feather Reed Grass bunches*
 - *Forty (40) 1-gallon assorted ground cover and perennial flowers*
5. **Lawn Area:** *No new lawn area is proposed; disturbed areas will be reseeded with biorention wire grass.*
6. **Plants as Screening:** *More than a dozen existing trees and shrubs provide screening of the proposed structure from the easterly side of the parcel. The applicant is also proposing to construct berms and plant additional trees and shrubs as screening.*
7. **Utility Installations:** *All utilities shall be installed underground.*
8. **Irrigation System Required:** NA
9. **Retain Existing Vegetation:** *Existing vegetation has been removed in the construction area. All disturbed areas shall be reseeded with native grasses prior to issuance of a Certificate of Occupancy.*
10. **Preserve Existing Trees:** *The majority of trees are proposed to be preserved.*
11. **Grading and Drainage:** *The applicant is required to preserve natural drainage. Submittal of a stormwater plan including a signed stormwater application is required for final approval. Final engineering approval by the City Engineer is required prior to issuance of a certificate of occupancy.*

12. **Maintenance:** *Landscaping will be maintained by the property owner or their designee.*

13. **Sidewalks:** N/A

14. **Bike Paths:** N/A

Residential Districts

1. **Preserve historic residences:** *No historic residences are impacted by the project.*
2. **Preserve human scale in residential character:** *The proposed project is of rural scale and appears to be a large barn or storage structure, not a residential building.*
3. **Preserve compatibility with surrounding neighborhoods:** *The proposed residential use is incompatible with the surrounding residential uses of the neighborhood.*
4. **Preserve natural features of the immediate landscape and environment:** *Beyond the building site, all natural features will be preserved.*
5. **Provide for community, or affordable, housing as needed:** *The proposed project does not provide for community or affordable housing.*
6. **Provide open spaces to enhance and maintain the rural character:** *The project does not maximize the allowable lot coverage, and, therefore, provides more than the minimum required open space.*
7. **Provide living and moving space for native animals:** *The project does not maximize the allowable lot coverage of the parcel and, therefore, provides more than the minimum amount of living and moving space for native animals. A fence is proposed along the west and portion of the north property lines which maybe a barrier to the movement of native animals.*
8. **Promote active and safe streetscapes in residential neighborhoods that are conducive to walking and biking:** *The proposed project is not located along a route designated for pathway improvements within the McCall Area Pathway Master Plan.*

Code Narrative

Per MCC 3.16.02, design review is required for single family residences in excess of 3,500 sq. ft.

Comments

Agency –

McCall City Engineer

In an email dated January 8, 2021, the City Engineer stated that there was not enough information available to provide a review and that Public Works will need a site drainage and landscape plan and a completed Stormwater Application before providing official comments on the application.

On June 2, 2021, the City's Staff Engineer sent an email requesting a site drainage plan and a stormwater management application and report.

On June 29, 2021, the City's Staff Engineer sent an email requesting a site drainage plan and a stormwater management application and report.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

This application was submitted to the PLRWSD more than thirty (30) days prior to the McCall Area Planning and Zoning Commission's February 2, 2021 meeting. In an email dated December 17, 2021, the sewer district stated they had no comments on the application.

McCall Fire and EMS

In an email dated December 15, 2020, McCall Fire stated they had no comments on the application.

Central District Health (CDH)

In an email dated December 9, 2020, CDH stated that a septic permit was issued for the parcel on June 30, 2020.

Public –

In an email dated December 18, 2020, Teresa Wolfenden expressed her concern with the commercial appearance of the structure.

In an email dated January 7, 2021, neighbor Kim Apperson expressed her opposition to the project.

In an email dated January 12, 2021, Bob and Johanna Krahn expressed concerns regarding the project's drainage, snow storage, and aesthetics.

In an email dated January 21, 2021, Louis Beaudry expressed opposition to the project and the commercial appearance of the structure.

In a letter dated January 24, 2021, Robert and Kathleen O'Neil expressed their objection to the lack of opportunity to comment on the proposal before it was constructed and their opposition to the application.

In a letter dated January 27, 2021, Teresa Cohn expressed her concern about the on-going construction of what she perceived as a commercial structure in a residential zone, and what she viewed as commercial creep in the neighborhood. Her opinion is that the design is not consistent with the City's design requirements.

In an e-mail dated January 28, 2021, Mark Wolfenden, objected to the lack of compatibility with the neighborhood, the industrial appearance and the use of metal siding of the building, and the on-going construction without design approval.

In an e-mail dated January 28, March 2, 2021 and letter dated April 5, 2021, Pam Wissenbach expressed her opposition to the application.

In an e-mail dated February 1, 2021, Kimberly Apperson and Joseph Baugh objected to the lack of public review of the plans before construction, asked that the application be remanded and the building be dismantled.

In an e-mail dated February 1, 2021, Michael Wissenbach expressed opposition to the on-going construction without design approval, incompatibility of the structure with R-1 zoning and residential use.

In an e-mail dated February 8, 2021, Kimberley A Apperson questioning the applicant's representation on the sequence of events leading to the design review application and expressed her opposition to the application.

In an e-mail dated February 27, 2021, Lou Beaudry and Bonnie Markinson supporting Kim Apperson's position and expressed their opposition to the application.

In a letter dated April 5, 2021, Mike and Pam Wessenbach expressed concern regarding the design of the structure and the adequacy of the proposed landscaping.

In a letter received by the City on May 3, 2021, Lou Beaudry and Bonnie Markinson expressed opposition to the application.

In an email received by the City on November 22, 2021, Randy Mitchell requested more trees to be planted between the subject property and the property to the east and to express concerns regarding the driveway drainage plan.

Staff Recommendation

Staff defers to the Commission regarding the approval or denial of this application. If the Commission votes to approve the application, staff recommends including the Conditions of Approval listed below.

Conditions of Approval

1. Prior to issuance of a certificate of occupancy, the applicant shall receive final engineering approval.
2. Prior to issuance of a certificate of occupancy, the applicant shall provide fixture details that comply with McCall's Outdoor Lighting Ordinance.
3. Prior to issuance of a Certificate of Occupancy, the applicant shall reseed all disturbed areas with a native grass or wildflower mix.
4. Prior to the issuance of a Certificate of Occupancy, the site shall be planted in accordance to the approved landscape plan.

5. Prior to the issuance of a Certificate of Occupancy, the applicant shall make all modifications to the building identified in the approved set of building plans.

IN RE:)
)
CARPY RESIDENCE) **McCALL AREA PLANNING AND ZONING COMMISSION**
Design Review) **FINDINGS OF FACT, CONCLUSIONS OF LAW, AND**
) **DECISION**
)
)
Application Number:)
DR-20-44)

FINDINGS OF FACTS

Applicant: Charles Carpy

Representative: Owner

Application: A Design Review application to construct a residence with attached garage of approximately 5,100 sq. ft. The structure is designed as a large metal barn type structure with garage on half of the lower level, and residence on the remainder of the lower level and half of the upper level.

Address: 641 Stockton Drive, McCall

Location: Tax Parcel Number 53 in Lot 15, Block 1 of West Place Subdivision situate in the SW ¼ of Section 15, T18N, R3E, B.M., Valley County, Idaho.

Public Notice: Newspaper: The Notice of Hearing was published in the *Star News* on July 15, 2021.
Mailing: The Notice of Hearing was mailed by the applicant to property owners within 300 feet on July 19, 2021.
Posting: The Notice of Hearing was posted by the applicant on the subject property on July 19, 2021.

Zoning: R1 –Residential One Acre

Property Size: 1.16 acres (50,438 sq. ft.)

- Lot Coverage:** Per MCC 3.3.06, the allowable coverage for this parcel is 18.5% or 9,331 sq. ft. The proposed structure has a footprint of 3,366 sq. ft. leaving 17,000 sq. ft. available for drive and parking area. Because the parcel is accessed via a driveway easement on an adjacent parcel and very little driveway is needed on site, this appears to be more than sufficient, however, exact driveway dimensions requirements have yet to be provided.
- Building Height:** Per MCC 3.3.03, the maximum allowed height is 35-feet. The height to the eaves is 22-feet. It is unclear what the exact maximum height is to the roof peak, but it appears to be approximately 30-feet in height.
- Setbacks:** Per MCC 3.3.03, the minimum required setbacks for this parcel are 25 ft. from the front property line, 15 ft. from either side property line, and 20 ft. from the rear property line. The applicant is proposing setbacks of 64 ft. from the front and rear property lines, 50 ft. from the easterly side property line, and 21 ft. from the westerly side property line.
- Parking Spaces:** Provided: 3+ spaces
Required: 2 spaces per MCC 3.8.062

APPROVAL STANDARDS

DESIGN GUIDELINES

1. **Building Scale:** The proposed structure is large, with very little architectural detail to help minimize the scale of the façade.
2. **Pedestrian Character:** N/A
3. **Blank Walls:** The building design includes large uninterrupted metal walls on multiple facades.
4. **Storefront:** N/A
5. **Parking:** The project includes adequate parking for its residential use, pursuant to MCC 3.8.06.

Guidelines for All Projects

Site Planning

1. **Building Siting:**
2. **Adjacent Buildings and Uses:** The proposed residential use is compatible with adjacent residential uses.
3. **Preserve Vegetation and Wildlife:** Few trees are proposed to be removed unless within the building and drive footprint.
4. **Preserve Views:** The proposed project does not block any significant views.
5. **Preserve Skylines:** The proposed building site is not on a skyline or ridge top.
6. **Preserve Natural Drainage:** Final engineering approval of such by the City Engineer is required prior to issuance of a building permit.
7. **Cluster Buildings:** N/A
8. **Street Alignment:** An existing driveway easement serves the subject parcel.
9. **Retaining Walls:** No retaining walls are proposed.
10. **Snow Storage:** Snow storage areas are not identified, however, the large parcel size indicates that adequate snow storage is likely available.
11. **Roof Design and Snow:** The applicant is proposing to install snow rails on the primary roof and shed roof over the garage doors to prevent snow from falling in front of pedestrian and vehicle entrances.
12. **Use the Sun:** The proposed design does include a large covered deck and patio that will minimize cold, dark, unpleasant areas.
13. **Screen Service Areas:** N/A
14. **Off-Street Parking:** N/A
15. **On-Site Parking:** Adequate on-site parking is provided for its residential use per MCC 3.3.062.

16. **Circulation Needs – Pedestrian and Vehicles:** N/A

Architecture

1. **Enhance McCall Classic Styles:** The design is not consistent with a classic McCall residential structure. Although the use of metal as a building material is part of the McCall architectural vernacular, and the building form simulates a traditional barn.
2. **Minimize Scale:** There are no varied roof lines, and minimal architectural details are used to help minimize the appearance of the building scale. The blank walls, large garage door, lack of ground-level windows and mostly blank walls makes the structure appear to be larger than it is.
3. **Building Additions:** N/A
4. **Roof Lines:** The project does not include varied roof lines.
5. **Mechanical Equipment:** No exterior mechanical equipment is proposed.
6. **Multi-Unit Structures:** N/A
7. **Balconies and Porches:** The proposed design does include a large covered deck and patio that are the only interesting architectural features that help to break up the scale of the building.
8. **Exterior Doorways:** The exterior doorway locations complement the design of the building and serve the intended functions.
9. **Wall Materials:** The proposed wall materials are metal to minimize maintenance.
10. **Shop Front Design:** N/A
11. **Wall Colors:** The proposed materials are beige, green, and grey metal.

Landscaping and Site Design

1. **Light Fixtures:** No light fixture details have been provided. Prior to issuance of a certificate of occupancy, the applicant shall provide fixture details that comply with McCall's Outdoor Lighting Ordinance.
2. **Fences:** No fences are proposed.

3. **Retaining Walls:** No retaining walls are proposed.
4. **Paving and Streetscapes:** N/A
5. **Landscaping Plan:** A landscaping plan has been provided that includes the following:
 - Twelve (12) 8-9' tall Colorado Spruce trees
 - Ten (10) 8-9' tall Norway Spruce trees
 - Seven (7) 1.5" caliper Quaking Aspen trees
 - Four (4) 2" caliper Quaking Aspen trees
 - Seven (7) 5-gallon Mountain Snowberry bushes
 - Twenty-four (24) 2-gallon Native Rose bushes
 - Twenty-five (25) 5-gallon Feather Reed Grass bunches
 - Forty (40) 1-gallon assorted ground cover and perennial flowers
6. **Lawn Area:** No new lawn area is proposed; disturbed areas will be reseeded with native grasses.
7. **Plants as Screening:** More than a dozen existing trees and shrubs provide screening of the proposed structure from the easterly side of the parcel. The applicant is also proposing to construct berms and plant additional trees and shrubs as screening.
8. **Utility Installations:** All utilities shall be installed underground.
9. **Irrigation System Required:** NA
10. **Retain Existing Vegetation:** Existing vegetation will be retained as much as possible during construction. All disturbed areas shall be reseeded with native grasses prior to issuance of a Certificate of Occupancy.
11. **Preserve Existing Trees:** The majority of trees are proposed to be preserved.
12. **Grading and Drainage:** The applicant is required to preserve natural drainage. Submittal of a stormwater plan including a signed stormwater application is required for final approval. Final

engineering approval by the City Engineer is required prior to issuance of a certificate of occupancy.

13. **Maintenance:** Landscaping will be maintained by the property owner or their designee.

14. **Sidewalks:** N/A

15. **Bike Paths:** N/A

Residential Districts

1. **Preserve historic residences:** No historic residences are impacted by the project.
2. **Preserve human scale in residential character:** The proposed project is of rural scale and appears to be a large barn or storage structure.
3. **Preserve compatibility with surrounding neighborhoods:** The proposed residential use is compatible with the surrounding residential uses of the neighborhood.
4. **Preserve natural features of the immediate landscape and environment:**
5. **Provide for community, or affordable, housing as needed:** The proposed project does not provide for community or affordable housing.
6. **Provide open spaces to enhance and maintain the rural character:** The project does not maximize the allowable lot coverage, and, therefore, provides more than the minimum required open space.
7. **Provide living and moving space for native animals:** The project does not maximize the allowable lot coverage of the parcel and, therefore, provides more than the minimum amount of living and moving space for native animals.
8. **Promote active and safe streetscapes in residential neighborhoods that are conducive to walking and biking:** The proposed project is not located along a route designated for pathway improvements within the McCall Area Pathway Master Plan.

DEPARTMENT/AGENCY COMMENTS

McCall City Engineer

In an email dated January 8, 2021, the City Engineer stated that there was not enough information available to provide a review and that Public Works will need a site drainage and landscape plan and a completed Stormwater Application before providing official comments on the application.

On June 2, 2021, the City's Staff Engineer sent an email requesting a site drainage plan and a stormwater management application and report.

On June 29, 2021, the City's Staff Engineer sent an email requesting a site drainage plan and a stormwater management application and report.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

This application was submitted to the PLRWSD more than thirty (30) days prior to the McCall Area Planning and Zoning Commission's February 2, 2021 meeting. In an email dated December 17, 2021, the sewer district stated they had no comments on the application.

McCall Fire and EMS

In an email dated December 15, 2020, McCall Fire stated they had no comments on the application.

Central District Health (CDH)

In an email dated December 9, 2020, CDH stated that a septic permit was issued for the parcel on June 30, 2020.

CONCLUSIONS OF LAW

1. The City of McCall has provided for the processing of Design Review applications, pursuant to Title 3, Chapter 16 of McCall City Code.
2. Adequate notice of the December 7, 2021 public hearing was provided, pursuant to Section 67-6512, Idaho Code and Title 3, Chapter 15 of McCall City Code.

3. Upon compliance with the conditions noted below, the application meets the Design Review Standards set forth in Title 3, Chapter 16 of McCall City Code.

DECISION

THEREFORE, the McCall Area Planning and Zoning Commission hereby **approves** this Design Review application, provided that the following conditions are met:

1. Prior to issuance of a certificate of occupancy, the applicant shall receive final engineering approval.
2. Prior to issuance of a certificate of occupancy, the applicant shall provide fixture details that comply with McCall's Outdoor Lighting Ordinance.
3. Prior to issuance of a Certificate of Occupancy, the applicant shall reseed all disturbed areas with a native grass or wildflower mix.
4. Prior to the issuance of a Certificate of Occupancy, the site shall be planted in accordance to the approved landscape plan.
5. Prior to the issuance of a Certificate of Occupancy, the applicant shall make all modifications to the building identified in the approved set of building plans.

Findings of Fact **adopted** this 7th day of DECEMBER, 2021.

Robert Lyons, Chair
McCall Area Planning and Zoning Commission

Attest:

Brian Parker, City Planner
City of McCall

RECEIVED

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION

DEC 28 2020



City of McCall
COMMUNITY DEVELOPMENT

Date Received: _____

Fees Paid: _____

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # _____ Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # _____ Scenic Route (SR) - \$300
- # _____ Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: CHARLES G CARPY Email: CHARLESCARPY@gmail.com
 Mailing Address: P.O. BOX 2345 McCall, ID Phone: 707 495 1289
 Property Owner 2 (If Applicable): _____ Email: _____
 Mailing Address: _____ Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: _____ Email: _____
 Mailing Address: _____ Phone: _____

PROPERTY INFORMATION

Address(es) of Property: 651 Stockton Dr. McCall, ID. 83638
 Legal Description of Property: Lot 15 / western 1
 Zoning District of Property: _____ Project Sq. Footage (If Applicable): 75' x 45'
 Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: (please attach supplemental information if needed)

75 x 45 metal residence. 2 BED 2 Bath

SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

CHARLES CAPPY
Property Owner 1



Property Owner 2 (If Applicable)

Signature

Agent/Authorized Representative

Signature

FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.

Plans @ McCall, ID. US.

216 East Park Street
McCall, Idaho 83638
P: (208) 634-7142
rsantiago-govier@mccall.id.us

Building Permit APPLICATION



Submittal Date: _____ Permit Number: _____

PROPERTY INFORMATION:

Site Address: Stockton St. or Parcel #: 1
Subdivision: West Place Block: _____ Lot: 15

PROPERTY OWNER INFORMATION:

Owner's Name: CHARLES A CARPY Phone: 707 495-1289
Mailing Address: P.O. 2345 McCall ID 83638 Email: CHARLESCARPY@gmail.com

APPLICANT INFORMATION (IF DIFFERENT FROM PROPERTY OWNER):

Applicant's Name: _____ Phone: _____
Mailing Address: _____ Email: _____

CONTRACTOR/REPRESENTATIVE INFORMATION:

Contact Name: CHARLES CARPY Business Name: _____
McCall Business License #: _____
Email: _____ Phone: _____ Idaho Contractor #: _____

ARCHITECT, ENGINEER, OR DESIGNER INFORMATION:

Contact Name: ALLIED STEEL Business Name: BRYON Talley
Email: btalley@alliedbuildings.com Phone: 509 233 7721 License #: _____

PROJECT DETAILS: to be filled out by applicant

Project Narrative or Description: 2 bedroom / 2 bathroom home
slab to grade / metal building

Project Square Footage (to be constructed, added or remodeled):

Occupied/Finished 3355 + Unoccupied/Unfinished

Residential				Commercial			
<input checked="" type="radio"/> New	Addition	Remodel	Repair	New	Addition	Remodel	Repair
	Accessory Structure				Accessory Structure		

Estimated Valuation of work to be done: 400,000

(building permit application continued)

PLEASE ANSWER ALL QUESTIONS:

Circle one

City Impact area

1. Where is the property located? *m^ccall* City Impact area
2. Is the project on a public city street?
If yes, please contact Public Works at 208-634-8945 to obtain PW permit if you are connecting to water, constructing a driveway, or working in the City Right of Way. Yes No
3. Are you demoing a structure first?
If yes, and you are within the City Limits, submit the mandatory Demo Requirements Form with this application. Contact Payette Lakes Recreational Water and Sewer District 208-634-4111, if capping a sewer line. Yes No
4. Are you drilling a well?
If yes, please contact the Idaho Department of Water Resources for a Drilling Permit 208-334-2190 Yes No
5. Are you currently on or putting in a septic system?
If yes, please contact Central District Health Department at 208-634-7194, to obtain a Septic Permit. Yes No
6. Is the structure greater than 3,500 sq. ft.?
If yes, please obtain a Planning & Zoning Design Review application to submit for approval. Yes No
7. Is the property on a Scenic Route?
If yes, please obtain a Planning & Zoning Scenic Route and Design Review application to submit for approval. Yes No
8. Is the property in a Shoreline and River Environs Zone?
If yes, please obtain a Planning & Zoning Shoreline and Design Review application to submit for approval. Yes No
9. Does the project have HOA or Design Review Committee Approval?
If yes, please provide documentation with your application. Yes No
10. Does this project include an accessory structure over 1,500 square feet? Yes No

(building permit application continued)

ACKNOWLEDGMENTS:

Permit Number: _____

- Application must be submitted with hard copies of the plans, one full size and one 11 x 17, as well as electronic copies (pdfs).
- The US Environmental Protection Agency requires that renovation, repair, and painting projects that disturb lead-based paint in pre-1978 homes, childcare facilities, and schools must be performed by an EPA Certified Renovator working for an EPA Certified Firm and specific work practices must be implemented to prevent lead contamination. More information is available at 1-800-424-LEAD (5323) or <http://www2.epa.gov/lead>
- This application becomes null and void if not pursued in good faith within 180 days of submittal date.
- I certify that I have read and examined this application and all submittals and know the same to be true and correct. All provisions of laws and ordinances that govern this type of work will be complied with whether specified herein or not.
- Water connection fees are refundable within the first 180 days from the date of issuance and not refundable after 180 days for any circumstance. However, water connection fees paid ("credit") may be applied towards future water connection fees. This credit 'runs with the land' so it cannot be transferred to another property. The credit may be transferred from the existing property owner to a new property owner. The credit will be applied to the current water connection fees in effect at the time a new building permit is issued, and the new property owner will be responsible for paying the difference. Proof of payment is required for the water connection credit.

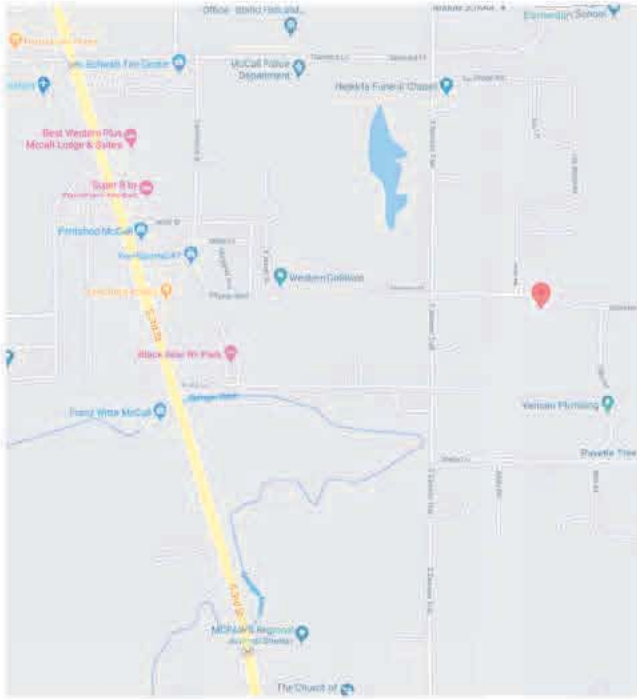
Signature _____ 

Date 7/7/20

FEES & APPROVALS:

Completed By City Staff

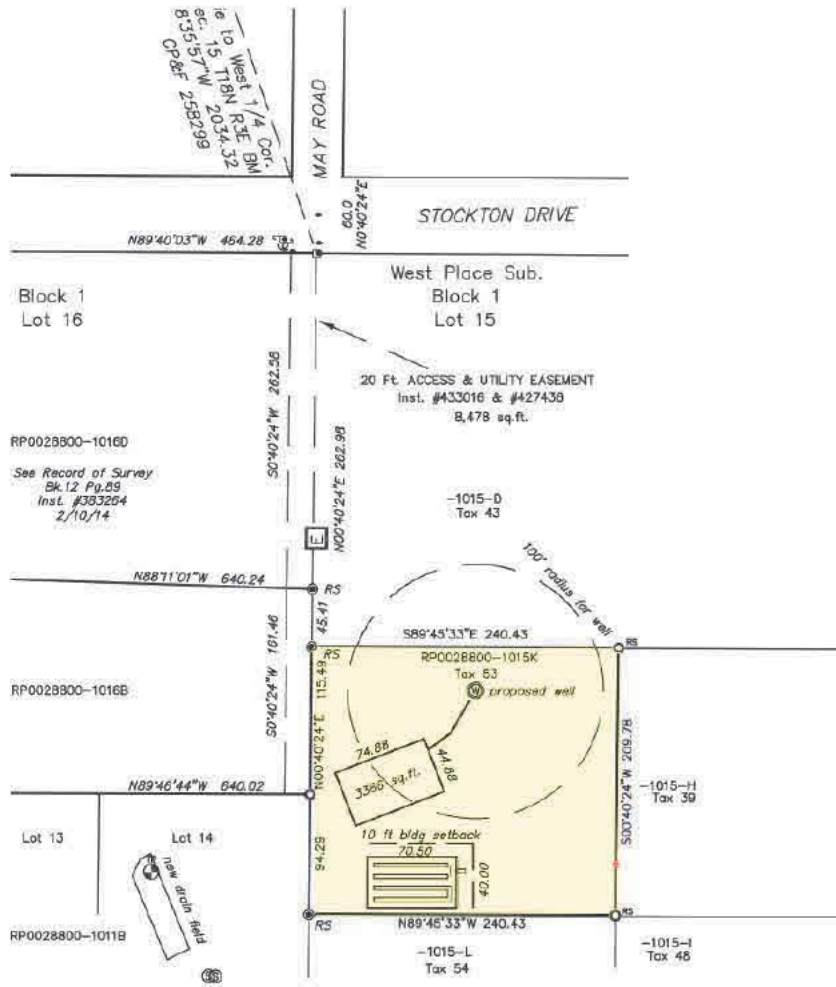
VALUATION OF WORK	Sub-Total	PLANNING DEPARTMENT	Yes
S/F Occupied Area @ \$200.00	\$	Application Number	
S/F (Upgrade of Above)	\$	Conditions Required	
S/F Un-Occupied Area @ \$100.00	\$	Approved by Planning Manager	<input type="checkbox"/>
Value used to Calculate Fee Total	\$	PUBLIC WORKS DEPARTMENT	
FEES	Sub-Total	Water ERU Count	
(Due Now) Application/Plan Check	\$	Approved by Department	<input type="checkbox"/>
Building Permit Calculated	\$	BUILDING DEPARTMENT	
Water Capitalization Fee	\$	Submittal Information Complete	<input type="checkbox"/>
Water Hook-up Fee	\$	Planning Report Complete/Approved	<input type="checkbox"/>
Public Works Permit	\$	Plans Check Complete/Approved	<input type="checkbox"/>
FEE TOTAL	\$	Approved by Building Official	<input type="checkbox"/>



VICINITY MAP

- A1 - Cover Sheet/Site Plan
- A2 - First Floor Plan
- A3 - Second Floor Plan
- A4 - Elevations
- A5 - Elevations

SHEET INDEX



SITE MAP

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By Brian Parker at 7:31 am, Jun 25, 2021

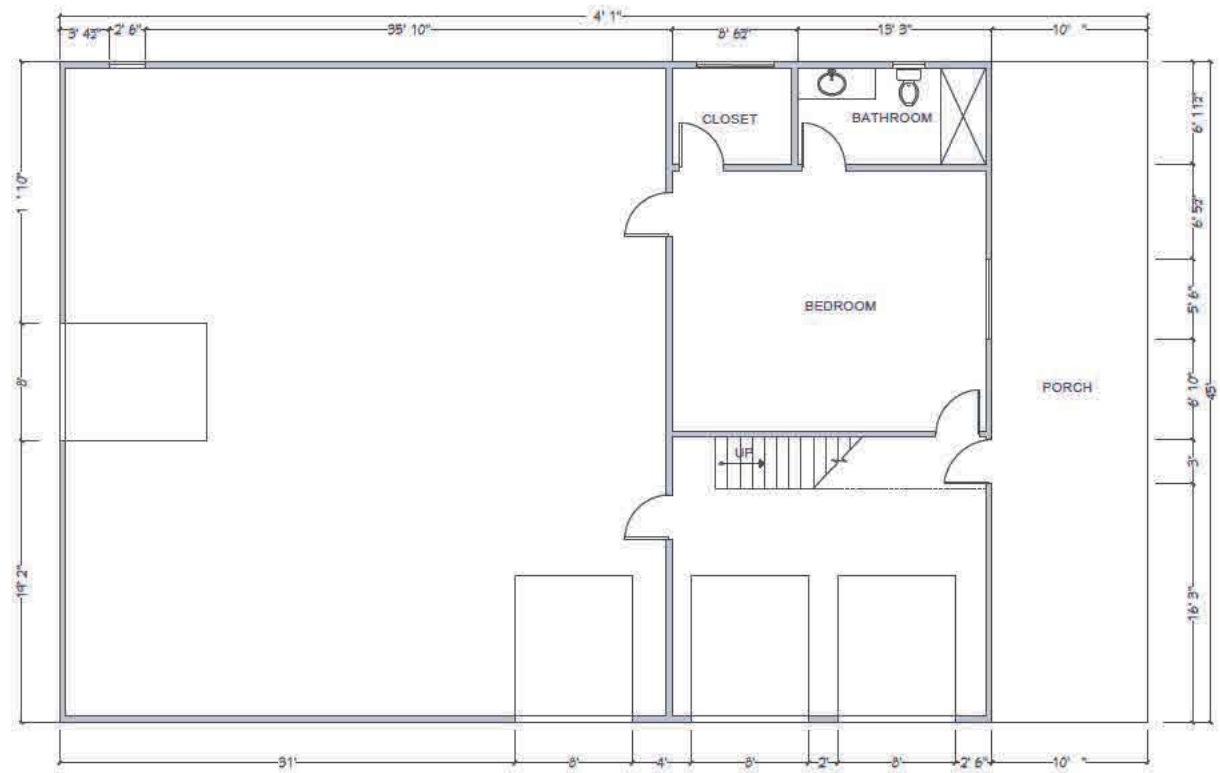
EL ZABETH WATTS
DESIGN & DRAFTING
EMAIL: ELZABETHWATTS@GMAIL.COM
941-711-1851

ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY AND THE ATTENTION OF THE DESIGNER OR CONTRACTOR FOR CLARIFICATION OR CORRECTION IF NOTIFICATION IS NOT MADE AND PLANS & CONDITIONS SHOWN SHALL BE GOVERNED BY THE CONTRACTOR OR SUBCONTRACTOR INVOLVED.

NEW CONSTRUCTION
641 STOCKTON DR, MCCALL, ID, 83603

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DOWNSTAIRS FLOOR PLAN

SCALE: 1/4" = 1'-0"

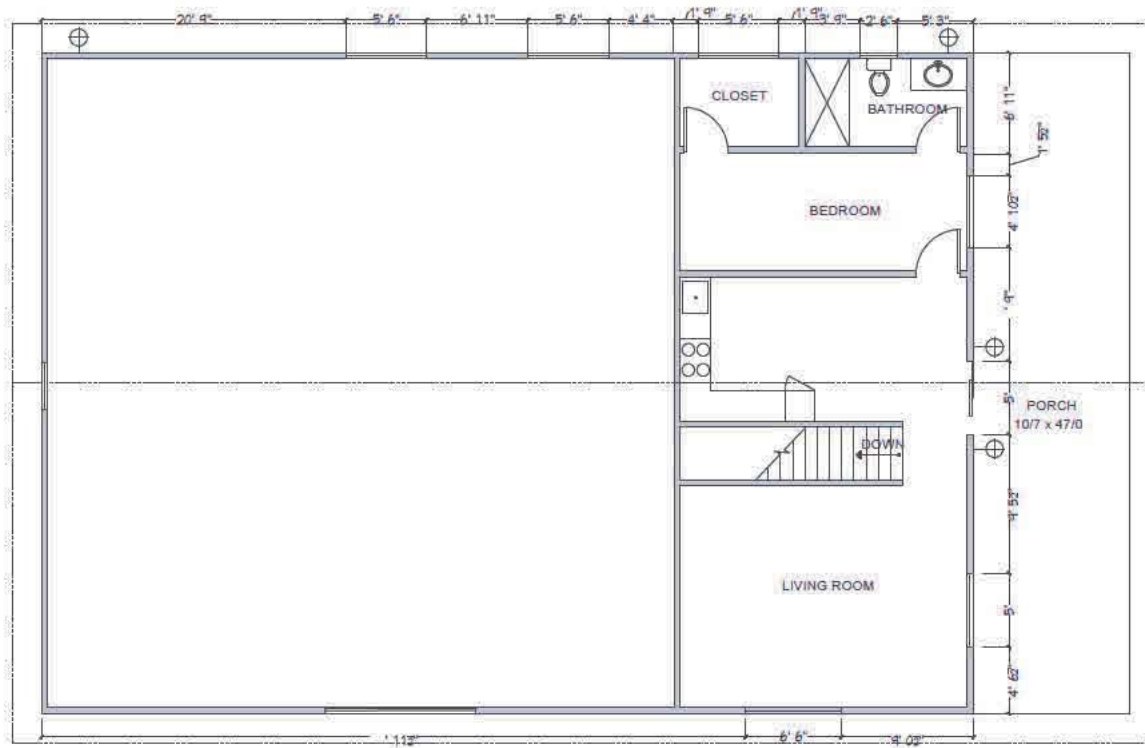
ELZABETH MATTS
DESIGN & DRAFTING
EMZELZABETHMATTSDRAWING.COM
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ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE DESIGNER OR ARCHITECT IMMEDIATELY UPON DISCOVERY OF ANY SUCH ERRORS, OMISSIONS, OR CONFLICTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY INSURANCE COVERAGE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY UTILITIES INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY SURVEYING INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY GEOTECHNICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ENVIRONMENTAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY HISTORICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ARCHAEOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PALEONTOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY BOTANICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ZOOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY GEOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY MINERALOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY METEOROLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY CLIMATOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY HYDROLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY OCEANOGRAPHICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ATMOSPHERIC INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY COSMOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ASTROLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY METEOROLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY CLIMATOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY HYDROLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY OCEANOGRAPHICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ATMOSPHERIC INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY COSMOLOGICAL INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY ASTROLOGICAL INFORMATION.

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UPSTAIRS FLOOR PLAN

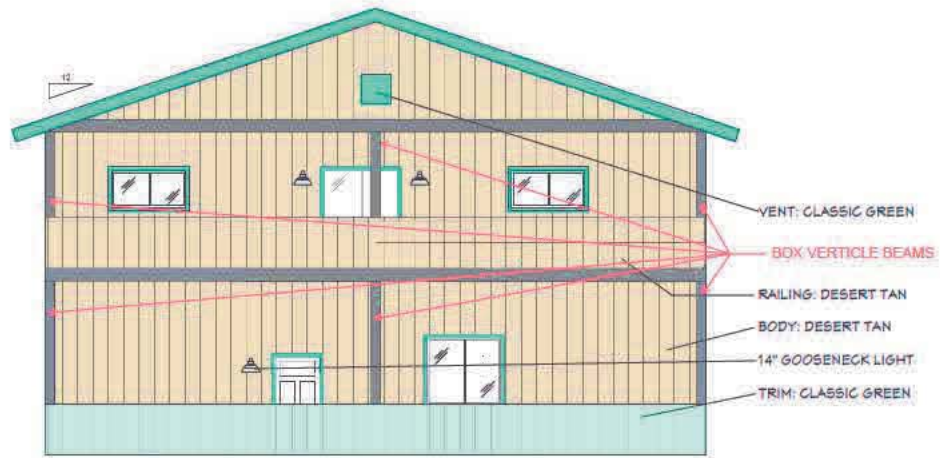
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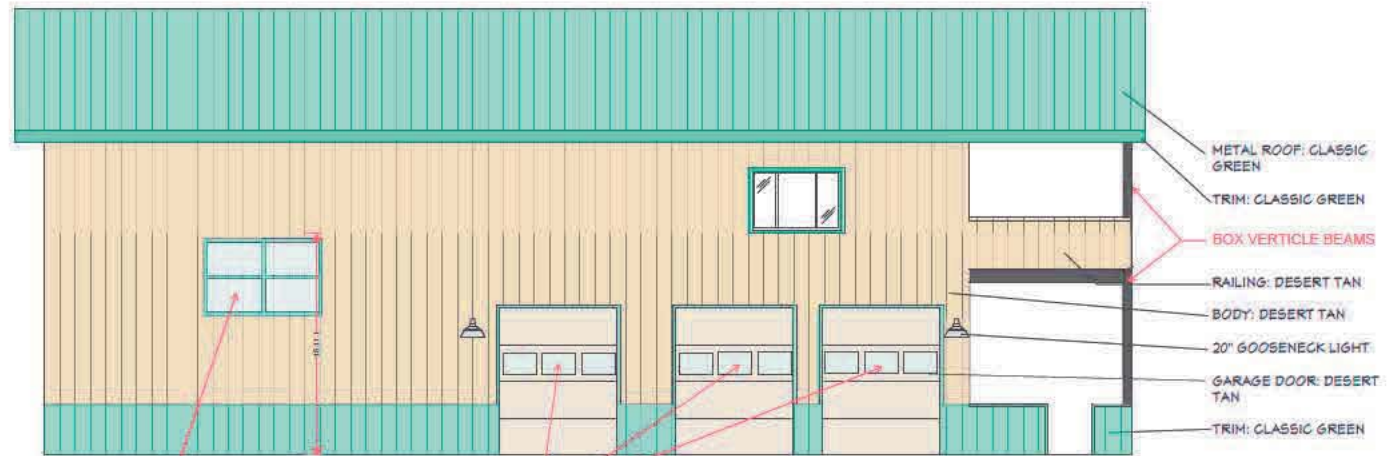
NEW CONSTRUCTION
 641 STOCKTON DR, MCGALL, D, 83638

ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE BEGINNING WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES AND STRUCTURES TO REMAIN.

ELIZABETH WATTS
 DESIGN & DRAFTING
 ELIZABETHWATTS@GMAIL.COM
 347 958 1234



A FRONT ELEVATION
SCALE: 1/4" = 1'-0"



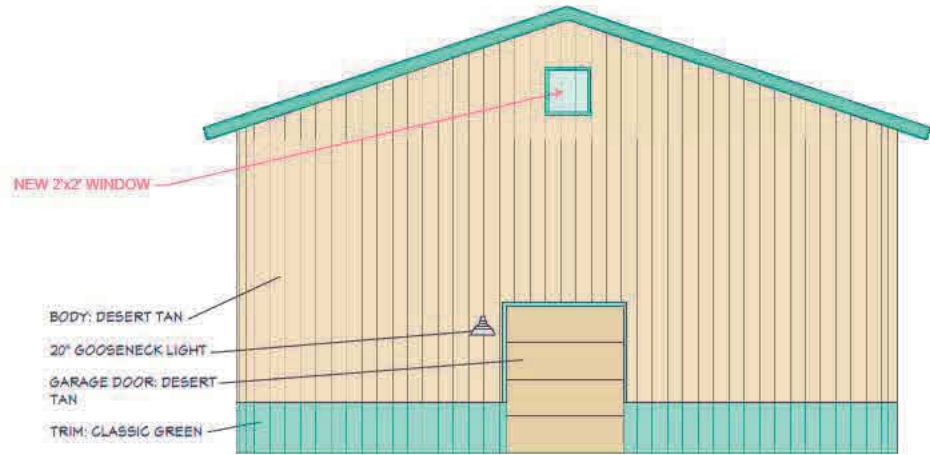
NEW 8x4 WINDOW

WINDOWS ADDED TO GARAGE DOORS

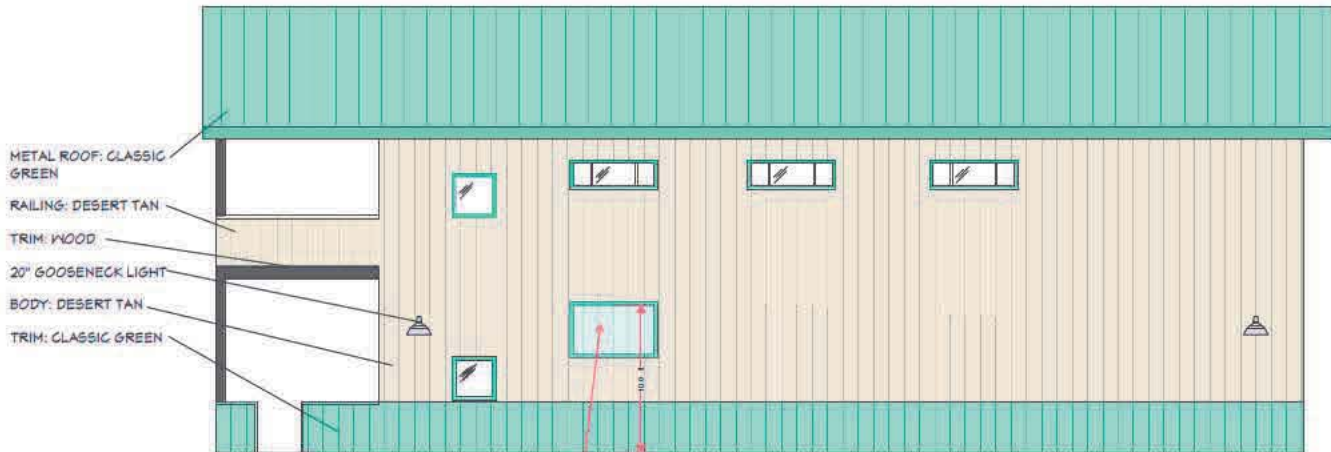
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RS: 3/22
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C BACK ELEVATION
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D RIGHT ELEVATION
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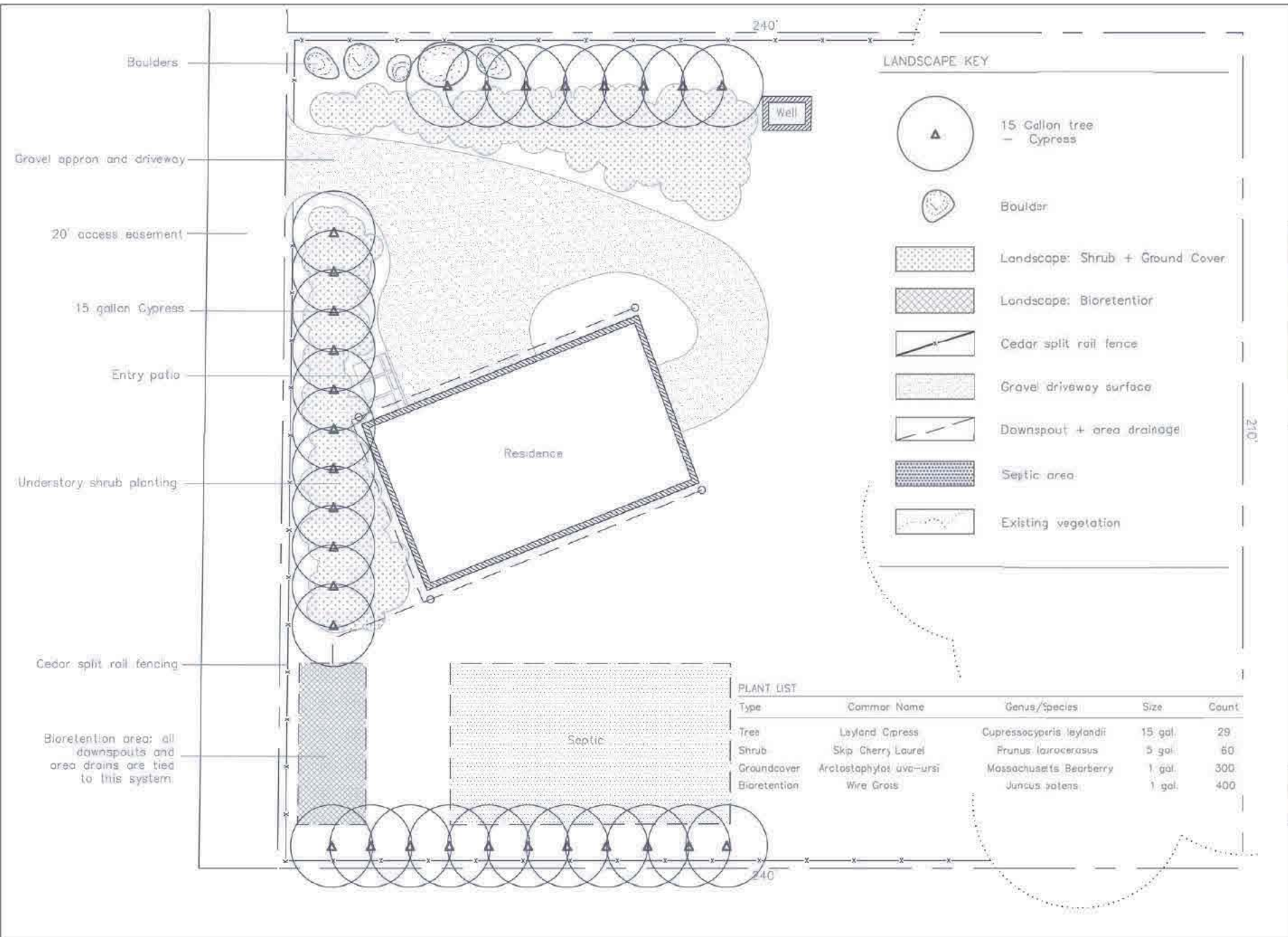
ELIZABETH WATTS
DESIGN & DRAFTING
EMAIL: ELIZABETH@WATTSDESIGN.COM
PHONE: 508.751.1595

ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY AND THE ATTENTION OF THE DESIGNER. THE CONTRACTOR FOR CLARIFICATION OR CORRECTION IF NOTIFICATION IS NOT MADE AND THE DESIGNER IS NOT ADVISED BY THE CONTRACTOR OF ANY CONFLICTS OR OMISSIONS. THE CONTRACTOR OR SUBCONTRACTOR INVOLVED

NEW CONSTRUCTION
641 STOCKTON DR, MCCALL, ID, 83638

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EL ZABETH KWATTS
DESIGN & DRAFTING
EMILIZABETHKWATTS@GMAIL.COM
34 7 98

ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR CLARIFICATION OR CORRECTION IF NOTIFICATION IS NOT MADE AND PLANNING IS CONTINUED. LIABILITY SHALL BE BORNE BY THE CONTRACTOR OR SUBCONTRACTOR INVOLVED.

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By Brian Parner at 1:28 pm, Jun 25, 2021

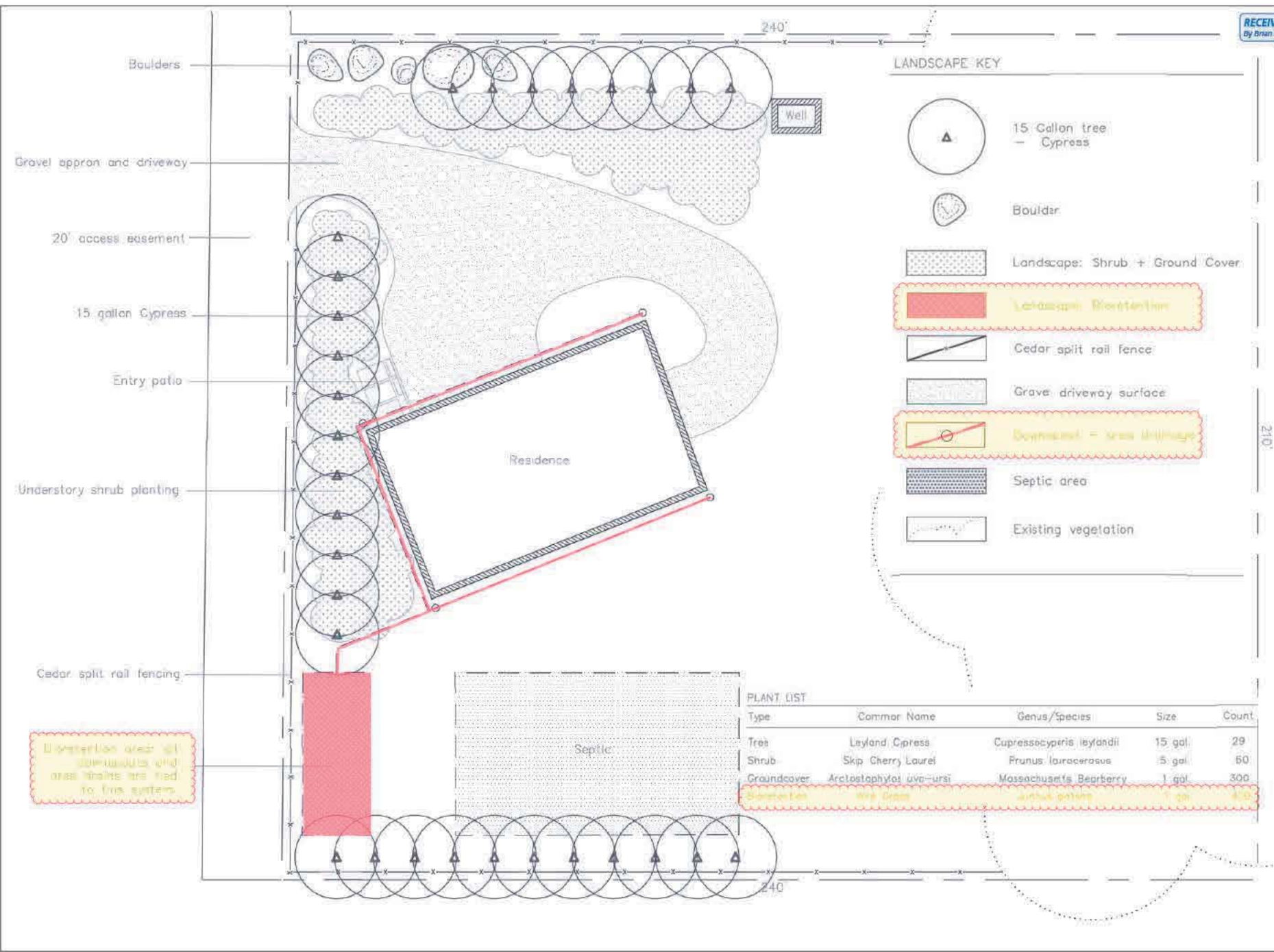
EL ZABETH KWATTS
DESIGN & DRAFTING
EMILIZABETHKWATTS@GMAIL.COM
34 7 95

ALL ERRORS, OMISSIONS, OR CONFLICTS BETWEEN THE VARIOUS ELEMENTS OF THE WORKING DRAWINGS SHALL BE THE RESPONSIBILITY AND THE ATTENTION OF THE DESIGNER. THE CONTRACTOR FOR CLARIFICATION OR CORRECTION IF NOTIFICATION IS NOT MADE AND PLANS IS CONTINGENT UPON THE CONTRACTOR'S REVIEW OF THE CONTRACTOR OR SUBCONTRACTOR INVOLVED.

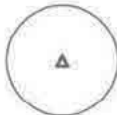








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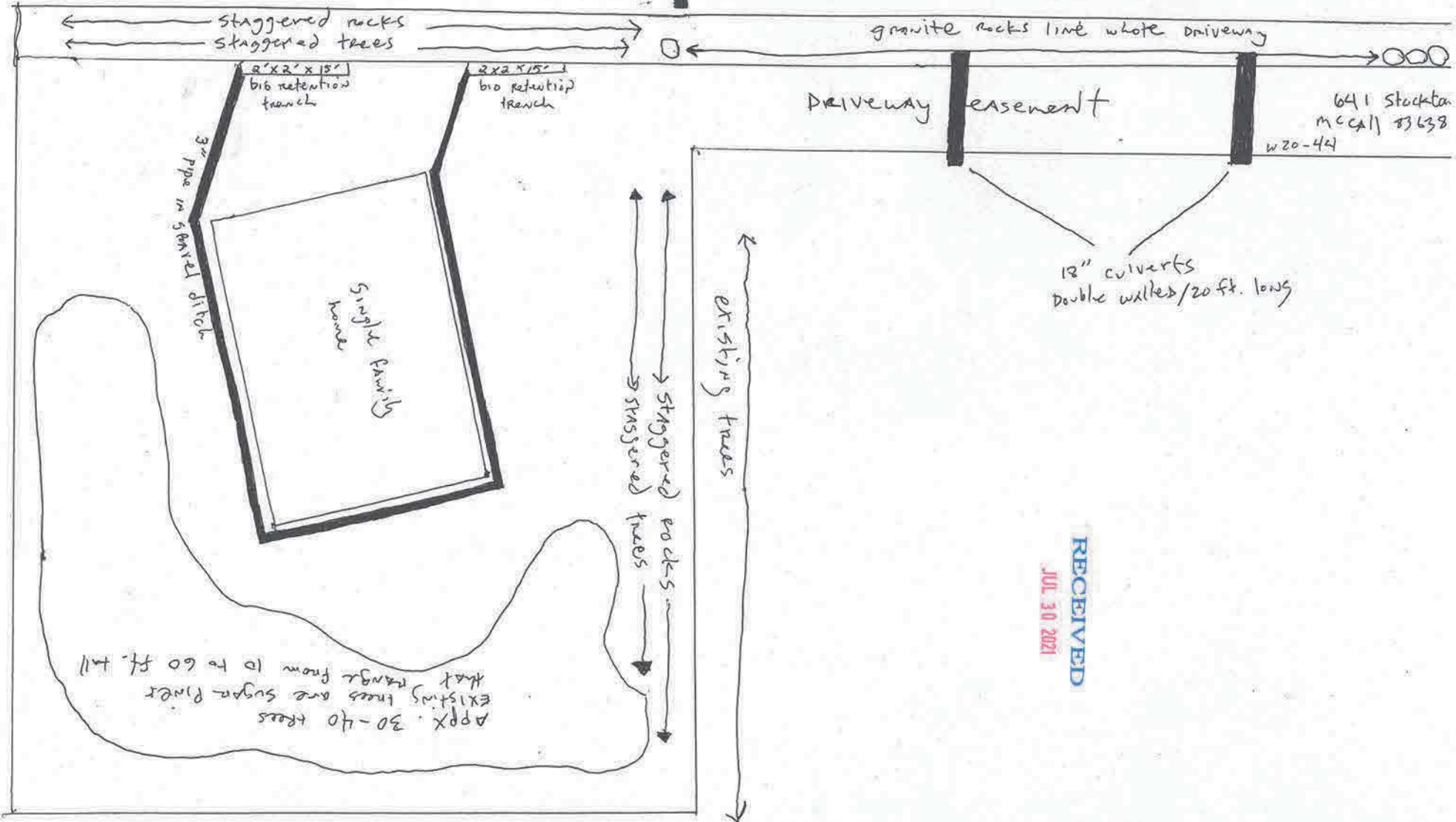
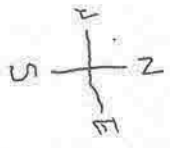
LANDSCAPE KEY

-  15 Gallon tree - Cypress
-  Boulder
-  Landscape: Shrub + Ground Cover
-  Landscape Bio-retention
-  Cedar split rail fence
-  Gravel driveway surface
-  Downspout - grass drainage
-  Septic area
-  Existing vegetation

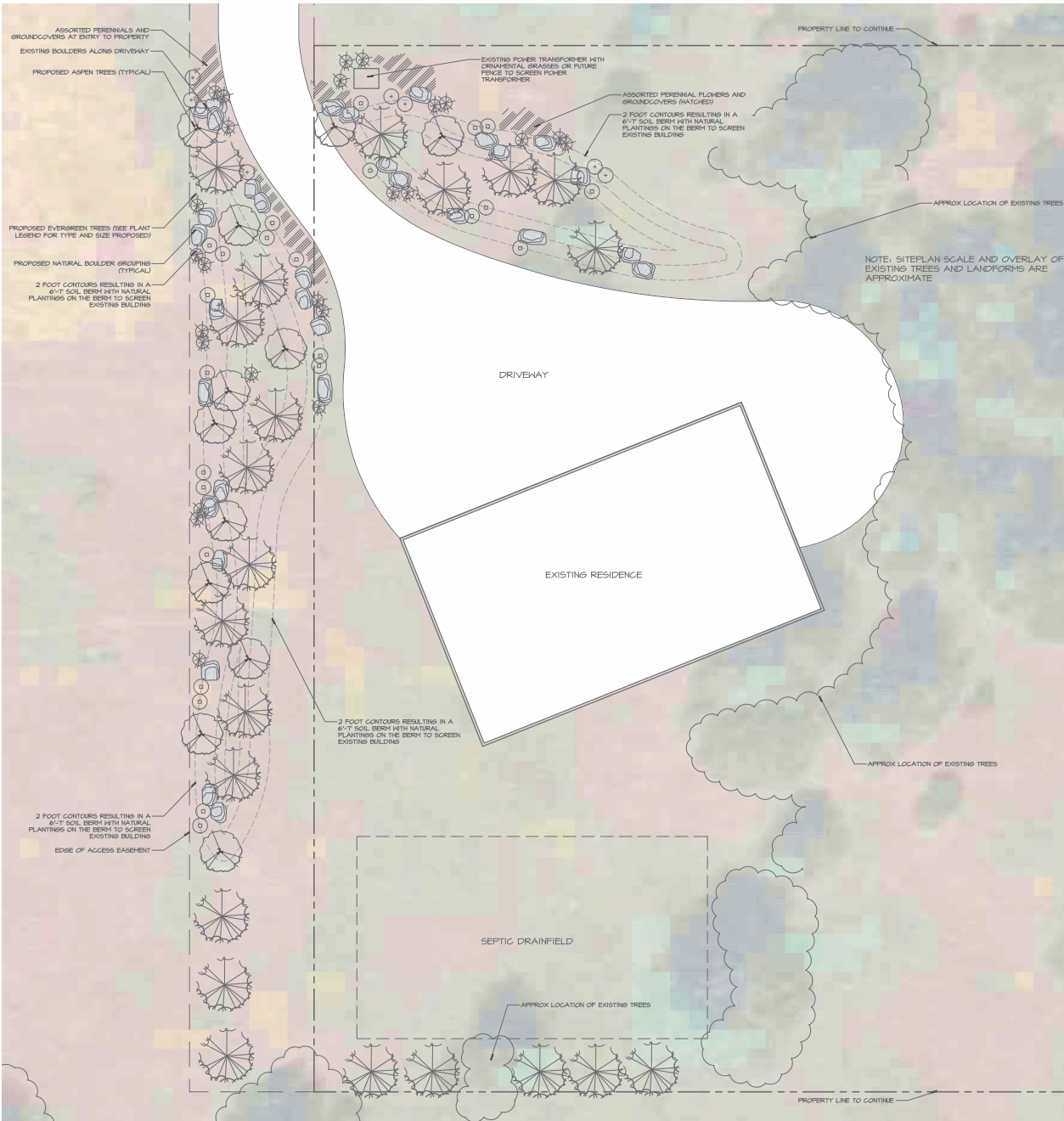
PLANT LIST

Type	Common Name	Genus/Species	Size	Count
Tree	Leyland Cypress	Cupressocyparis leylandii	15 gal	29
Shrub	Skip Cherry Laurel	Prunus laurocerasus	5 gal	60
Groundcover	Arctostaphylos uva-ursi	Massachusetts Bearberry	1 gal	300
Plantation	Wild Grass	various patterns	1 gal	400

Diversion area: all downspouts and area drains are tied to this system.



RECEIVED
JUL 30 2021

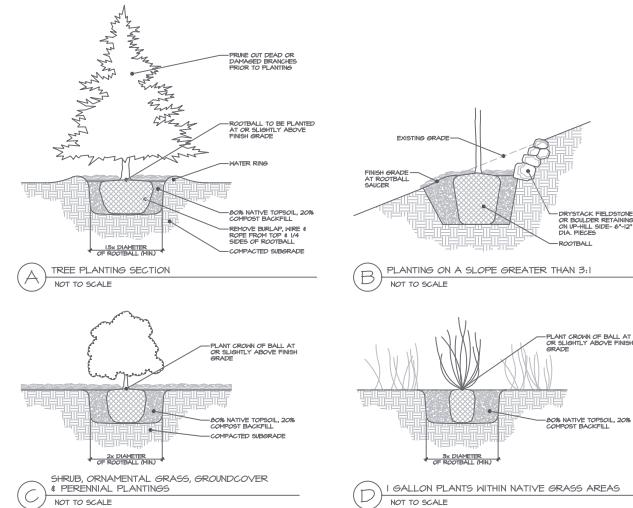


RECEIVED
By Brian Parker at 2:48 pm, Sep 28, 2021

PLANT LEGEND

SYMBOL	QTY.	BOTANICAL NAME	COMMON NAME	SIZE
	12	PICEA RESINOSA	COLORADO SPRUCE	8'-11" HT.
	10	PICEA ABIES	NORWAY SPRUCE	8'-11" HT.
	7	POPULUS TREMULOIDES	QUAKING ASPEN	15" CALIPER
	4	POPULUS TREMULOIDES	QUAKING ASPEN	2" CALIPER
	1	SYMPLOCARPUS OREOPHILUS	MOUNTAIN SNOWBERRY	5 GAL.
	24	ROSA WOODSII	NATIVE ROSE	2 GAL.
	25	CALAMAGROSTIS AGUTIFLORA	FEATHER REED GRASS	5 GAL.
	40	ASSORTED GROUND COVERS AND PERENNIAL FLOWERS		1 GAL.

NOTE:
EVERGREEN TREES SPECIFIED CAN BE SUBSTITUTED WITH OTHER SPRUCE TYPES, PINE TYPES OR FIR TYPES BASED ON AVAILABILITY
ALL PLANTINGS TO BE DRIP IRRIGATED, FOLLOW ALL CITY OF MCALL RULES RELATING TO BACKFLOW FOR THE IRRIGATION SYSTEM



SCREENING PLAN
641 STOCKTON DRIVE
MCALL, ID 83638

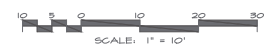
28 September 2021

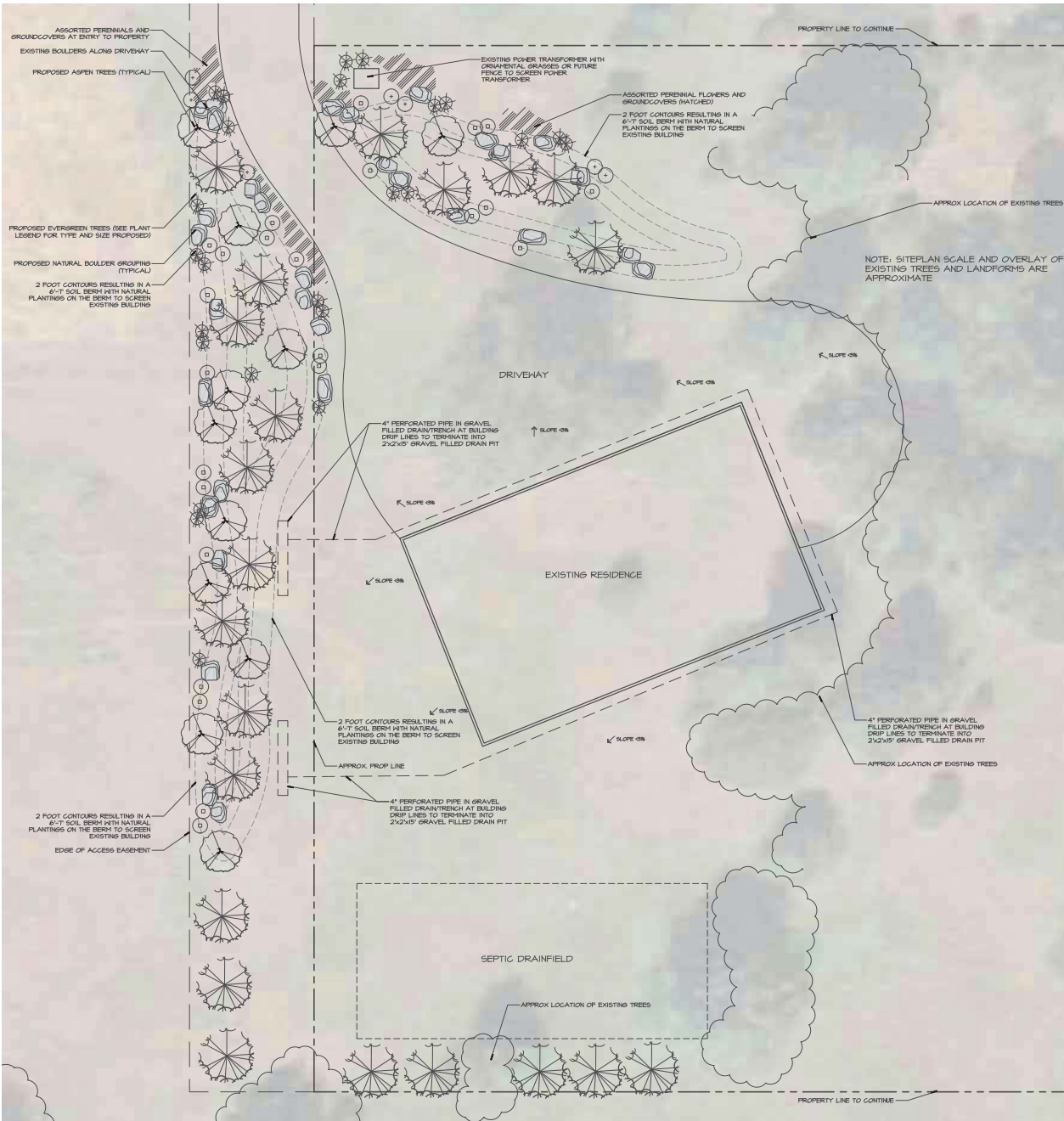
Title: SITE PLAN



Scale: 1" = 10'

Sheet: L1

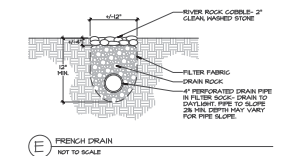
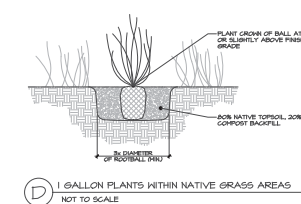
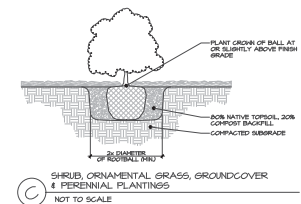
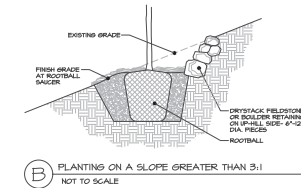
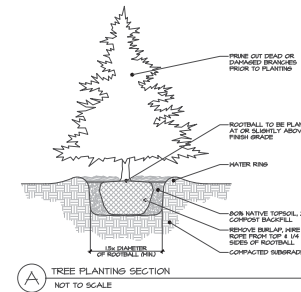




PLANT LEGEND

SYMBOL	QTY.	BOTANICAL NAME	COMMON NAME	SIZE
	12	PICEA RESINOSA	COLORADO SPRUCE	8'-11" HT.
	10	PICEA ABIES	NORWAY SPRUCE	8'-11" HT.
	7	POPULUS TREMULOIDES	QUAKING ASPEN	15" CALIPER
	4	POPULUS TREMULOIDES	QUAKING ASPEN	2" CALIPER
	1	SYMPLOCARPUS OREOPHILUS	MOUNTAIN SNOWBERRY	5 GAL.
	24	ROSA WOODSII	NATIVE ROSE	2 GAL.
	25	CALAMAGROSTIS AGUTIFLORA	FEATHER REED GRASS	5 GAL.
	40	ASSORTED GROUND COVERS AND PERENNIAL FLOWERS		1 GAL.

NOTE:
EVERGREEN TREES SPECIFIED CAN BE SUBSTITUTED WITH OTHER SPRUCE TYPES, PINE TYPES OR FIR TYPES BASED ON AVAILABILITY
ALL PLANTINGS TO BE DRIP IRRIGATED, FOLLOW ALL CITY OF MCALL RULES RELATING TO BACKFLOW FOR THE IRRIGATION SYSTEM





641 STOCKTON DRIVE

View from adjacent property at south side

RECEIVED
By Brian Parker at 2:52 pm, Sep 28, 2021



RIVERCHASE
Landscape Design & Planning



641 STOCKTON DRIVE
DRIVEWAY ENTRY VIEW



RIVERCHASE
Landscape Design & Planning

Existing



641 STOCKTON DRIVE

VIEW NEAR STOCKTON AND S. SAMSON INTERSECTION



RIVERCHASE
Landscape Design & Planning

RECEIVED

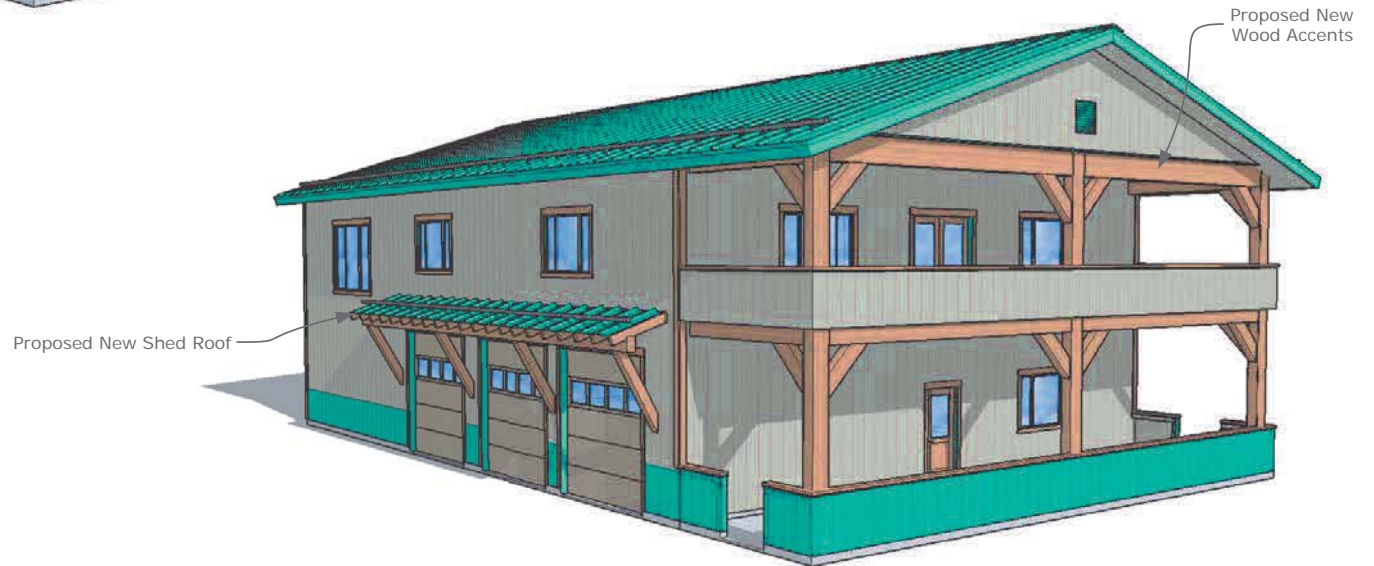
By Brian Parker at 10:15 am, Nov 04, 2021

General Notes:

1. All new natural wood finishes to be stained.
2. Other new elements to match existing.
3. Install all new roofing, hardware, doors, snow guards, etc. per manufacturer specifications.



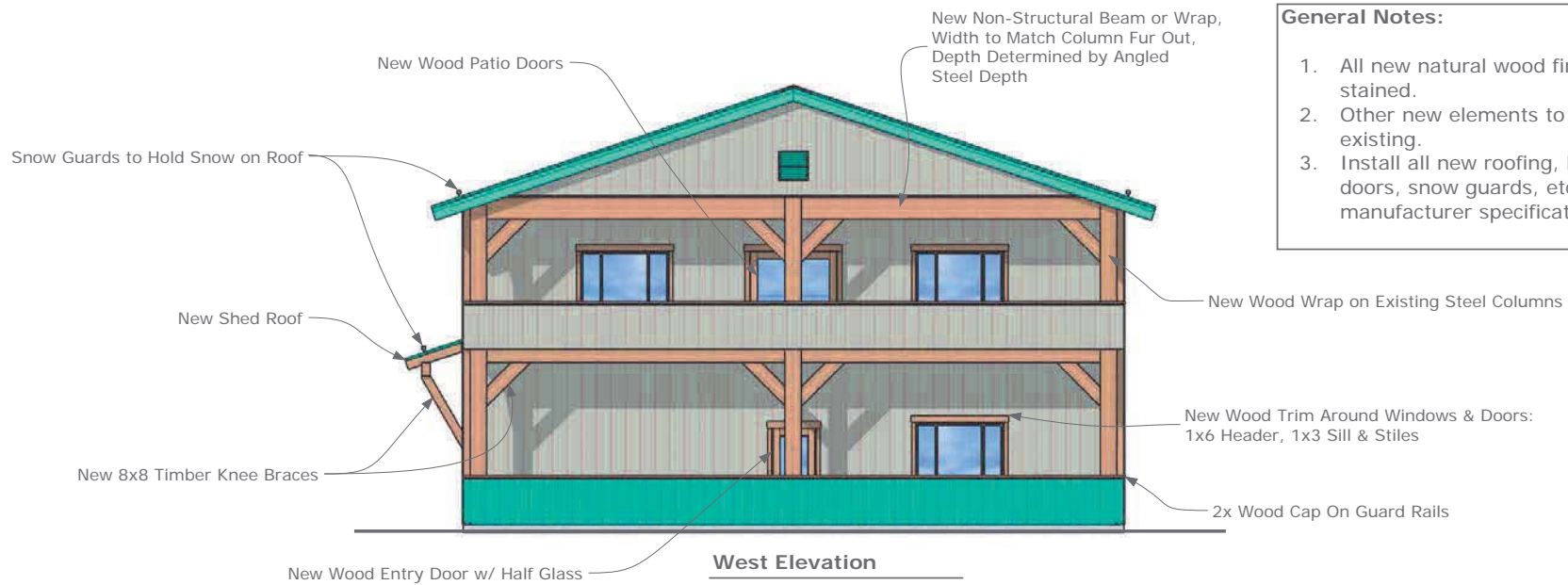
Existing Shop



Enhanced Shop

Exterior Perspectives

- General Notes:**
1. All new natural wood finishes to be stained.
 2. Other new elements to match existing.
 3. Install all new roofing, hardware, doors, snow guards, etc. per manufacturer specifications.



West Elevation



North Elevation

Elevations



- General Notes:**
1. All new natural wood finishes to be stained.
 2. Other new elements to match existing.
 3. Install all new roofing, hardware, doors, snow guards, etc. per manufacturer specifications.



Meredith Todd

From: Dan Quakenbush <dan@riverchasedesign.com>
Sent: Tuesday, November 16, 2021 4:00 PM
To: Morgan Stroud
Cc: Brian Parker; Meredith Todd
Subject: Re: DR20-44 - Stockton Shop Proposed Enhancements - Stormwater

Follow Up Flag: Follow up
Flag Status: Flagged

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

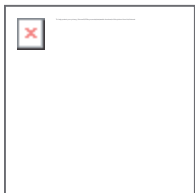
Hi Morgan,

Nice to "meet" you! In regards to the Carpy project this is the first I have heard of anything regarding the stormwater plan. I am not really in the loop as to how this project was started and stopped as I was brought on in September. Mr. Carpy employed me to set up the landscape and screening plan. Below are a couple quick questions and depending on the outcome I may have to relay this info back to him.

- Will this plan need to be stamped by a civil? On the other projects I have worked on in McCall the answer was yes (at least for the stormwater portion). I usually prepare rough grading plans for the home and the civil engineer runs the stormwater portion so my knowledge about stormwater policy and procedure in McCall is limited as I don't usually prepare this.
- Do you already have calculations or as you indicated a "minimal stormwater drainage report"?
- With this being a completed building in a grassy field with a permeable driveway not near the lake or scenic byway is it as simple as indicating the flow of water on the landscape plan and dry wells to catch any flow from the roof on two sides so stormwater remains on the property?

Thanks so much!

Dan Quakenbush



Riverchase Landscape Design and Planning

M: (208) 914-3502

dan@riverchasedesign.com

www.riverchasedesign.com



On Tue, Nov 16, 2021 at 3:18 PM Morgan Stroud <mstroud@mccall.id.us> wrote:

Hello Dan,

I have been reviewing Mr. Carpy's project regarding the stormwater and drainage requirements, and I have looked at the updated plans that were sent to the City. I did not see a plan that incorporates how the grading and stormwater drainage will be handled and we were working through items in a minimal stormwater drainage report that would document pre/post flows and an erosion control plan. I wanted to reach out to see if you had these items prepared and they didn't find their way to the City or if it is something that is being worked on currently. I have attached a previous email that listed out some of the requirements that I was still looking for.

Please let me know if you want to call and discuss this or have any questions regarding this.

Thank you,

Morgan Stroud, E.I.T. | Staff Engineer

216 E Park Street | McCall | ID 83638

Direct: 208.634.3458 | Cell: 208.315.2299



Rachel Santiago-Govier

From: Teresa Wolfenden <tcwolfenden@gmail.com>
Sent: Friday, December 18, 2020 10:22 AM
To: Pam Wissenbach
Cc: Morgan Bessaw; Michael Wissenbach; Chris Connolly; Mark Wolfenden; Bob Krahn; jkrahn86@gmail.com; Kimberly A Apperson
Subject: New Build Stockton

--- This email is from an external sender. Be cautious and DO NOT open links or attachments if the sender is unknown. ---

Hi, Morgan,

I'm writing to ask about the new structure going in on Stockton, directly south of May Road. The walls are going on today, and I'm surprised (and disappointed) at the outside appearance. This looks more like a warehouse than a residential structure.

What is the intended use of this structure? Is it indeed, residential, as it is zoned?

Is this structure compliant with lot size allowances -- it's quite tall, and enormous.

I'm again concerned about structures that encourage activities not appropriate for zoning in this area.

Thanks for any information you have on what's going on here.

Best,
Teresa

Rachel Santiago-Govier

From: Kimberly A Apperson <kimapperson@icloud.com>
Sent: Thursday, January 7, 2021 8:04 AM
To: Morgan Bessaw
Cc: Bonnie Makinson; Pam Wissenbach; Teresa Wolfenden; Michael Wissenbach
Subject: Re: Building on Parcel RP00288001015K

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Hi Morgan,

Are you aware that work is continuing on this structure? the siding appears complete. It looks very industrial. I am very disappointed that McCall's building and planning departments have allowed, once again, such a large and unsightly structure to be built in our residential neighborhood.

What are your mitigation plans for this? Thank you, Kim

Kim Apperson
kimapperson@icloud.com
208-630-4776

On Dec 23, 2020, at 4:17 PM, Morgan Bessaw <mbessaw@mccall.id.us> wrote:

Hi Kim,

Sorry for the delay in response. I was trying to determine if the application was moving forward in January or not before responding. We have been working with the property owner to put together a Design Review package for the Planning and Zoning Commission to review. I am hopeful it will be heard during the February 2nd P&Z meeting. You will receive public noticing letting you know how to call into that meeting to provide comments once it is scheduled.

Sincerely,
Morgan Bessaw

From: Kimberly A Apperson <kimapperson@icloud.com>
Sent: Wednesday, December 9, 2020 12:47 PM
To: Morgan Bessaw <mbessaw@mccall.id.us>
Cc: Pam Wissenbach <pamwissenbach@gmail.com>; Michael Wissenbach <mjw.pra4snow@gmail.com>
Subject: Re: Building on Parcel RP00288001015K

--- This email is from an external sender. Be cautious and DO NOT open links or attachments if the sender is unknown. ---

Hi Morgan,

I see some activity at that new construction on Stockton Ln. I have not seen a notice for a design review hearing for it. Can you what is happening with that? Thanks much, Kim

Kim Apperson
kimapperson@icloud.com
208-630-4776

On Nov 3, 2020, at 3:57 PM, Morgan Bessaw <mbessaw@mccall.id.us> wrote:

Hello,
You are correct, the building official did issue a building permit for that structure without design review approvals. However, we have reached out to the property owner and let him know of the error and that he will still be required to submit for design review approvals. This will include a public notice so that neighbor concerns can be addressed. He is working on pulling those materials together, but I do not yet know which P&Z agenda he will be placed on. Please let me know if you have further questions.
Sincerely,
Morgan Bessaw

From: Pam Wissenbach <pamwissenbach@gmail.com>
Sent: Monday, November 2, 2020 2:13 PM
To: Morgan Bessaw <mbessaw@mccall.id.us>
Cc: Michael Wissenbach <mjw.pra4snow@gmail.com>; Kimberly A Apperson <kimapperson@icloud.com>
Subject: Building on Parcel RP00288001015K

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Hello Morgan,

I am writing to you concerning a building under construction on Parcel RP00288001015K near the corner of South Samson Trail Road and Stockton. This area is zoned R1. This building is very large with several big bay doors that would accommodate commercial vehicles. Has this property applied for a CUP if a business is going to be housed there? (Hopefully there will be no business planned..) Did this building go through a design review since it is such a large structure? This building is quite out of character with the other residences in the area.

Thank you for your help in this matter. Maybe, someday soon, you can meet with this neighborhood for a fun topic!

--

Have a great day,

*Pam Wissenbach
280 May Road*

McCall, ID 83638
208.308.4755

January 27, 2021

Dear McCall Planning and Zoning Commission,

I am writing to express my disappointment and concern about a new build on 651 Stockton Road, which is not a residential structure and is in the process of construction in a zoned residential area. This is a commercial structure that appears to be a warehouse or large garage, made of metal siding, and a very small second story space that may be an office or small apartment. This is an inappropriate building for our residential neighborhood. Even if the current owner intends to use this structure as a residence, which appears unlikely, this structure will tend toward commercial use over time. Indeed, we have already seen this happen in our neighborhood and have spent two years working to prevent this kind of “industrial and commercial creep” into our community. Our street is one of the few neighborhoods in McCall that is home to all permanent residents. Since “building a stable year-round population” is increasingly important in McCall, and a key goal of the comprehensive plan, I ask you to please prevent this commercial creep from happening again. We simply cannot expect neighborhoods to be constantly responsible for monitoring inappropriate building and inappropriate uses in our treasured residential areas, and then ask residents to spend years combating uses that should never have existed in the first place, particularly when the Comprehensive Plan is our guiding document and appropriate permitting processes should prevent this.

According to McCall Design Guidelines for Residential Districts (Section 8.0) residential projects or individual residences should “Preserve human scale in residential character, preserve compatibility with surrounding neighborhoods, and promote active and safe streetscapes in residential neighborhoods that are conducive to walking and biking.” This new build does not preserve residential character, is not compatible with surrounding areas, and is unfortunately located in a critical thoroughfare for children to move through residential neighborhoods to school rather than on Sampson Trail. It is a terrible location for a structure that promotes commercial activity, now and when the property turns over in the future. Moreover, we understand additional lots on land adjacent to this property are expected on the market in the near future. I am very concerned that this structure sets a precedent for additional expansion of commercial use in our neighborhood.

This structure needs massive redesign in order to align with design guidelines and goals of R1 zoning compatible with the surrounding neighborhood, including new siding, additional non-industrial windows, overhauled appearance overall, and resizing of garages relative to living space. In short, in a zoned residential area, surely more of a housing structure should be dedicated to residential use than to warehouse/storage/garage.

Our neighborhood should be the kind of residential neighborhood this community treasures and protects as life becomes difficult for those who call an increasingly expensive McCall our home. We all live here. We raise kids in this neighborhood. We are teachers and business owners, volunteers, health care professionals and retirees. We plow each other’s driveways,

trade favors, and know the kids who walk down the road to school. Few neighborhoods in and around McCall can say that everyone is a permanent resident, like we can in ours. We should guard and support these neighborhoods – they are the vitality of the McCall/Valley county community. Please commit to the “quality of place-making” key to our Comprehensive Plan. Do not allow our neighborhood to turn commercial.

Sincerely,

Teresa Cohn

300 May Rd.

Rachel Santiago-Govier

From: Bob Krahn <bobkrahn70@gmail.com>
Sent: Tuesday, January 12, 2021 9:19 PM
To: Morgan Bessaw
Cc: Teresa Wolfenden; Pam Wissenbach; Kim Apperson; lewannball@frontier.com; Chris Connolly
Subject: Stockton road home

--- This email is from an external sender. Be cautious and DO NOT open links or attachments if the sender is unknown. ---

Hi Morgan,

I hear that the new home being built near Stockton Ln is going through design review in February and we have a few items that we feel should be addressed.

-The access road that connects to Stockton Ln was put in with no culverts to allow for natural water drainage. This needs to be corrected immediately as it will cause major problems if it is left until spring.

-Snow storage needs to be addressed. On 1/12/21, snow was pushed north across Stockton Ln and piled at the corner of May Rd and Stockton Ln. This could obstruct the view of drivers and cause an accident. The snow needs to be stored on their property.

-The structure itself looks to be all steel framing and siding. The northwest facing wall has 3 garage doors and 1 small window. Visually I would say this building looks more industrial or commercial than it does a single family home. Therefore we do not feel it fits in with the surrounding area.



Thank you for your consideration,
Bob and Johanna Krahn

Sent from my iPhone

Rachel Santiago-Govier

From: Lou Beaudry / Bonnie Makinson <beumak4@gmail.com>
Sent: Thursday, January 21, 2021 10:39 AM
To: Morgan Bessaw
Subject: DR-20-44 681 Stockton Drive

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McCall Area Planning and zoning Commission,

I would like to voice my opposition to the design of the new build on this property. I am homeowner within sight of this building. Simply put; The area is zoned R-1- Residential. The building looks very much commercial. Many people have asked me, "What is that large warehouse that is being built near you?" I understand there is a residence above, but the metal siding, 4 large garage doors and the size and height all scream industrial. I do not know what can be done at this point and work continues to be done on it, but it is the job of your commission to look at these things and insure they conform to the appearance of the residential zoning. Thanks for your consideration.

Sincerely,

Louis Beaudry

TO: McCall P&Z Commission

FROM: Robert and Kathleen O'Neil

DATE: January 24, 2021

RE Parcel RP00288001015K

Dear Commissioners:

As they say first impressions are very important and for anyone turning onto or looking East from Samson Trail toward Stockton Drive the first thing you will see is a **mega** building, which is totally out of place for this location. This building without any doubt cannot be classified as a house, even though it has "living quarters" above. It is so obvious the structure was not built or designed as a residence, but rather a "warehouse" to store large (perhaps commercial) equipment or recreational toys. This property is not zoned COMMERCIAL. The owner needs to apply for a conditional use permit for this warehouse.

The residents of West Place Subdivision in close proximity to this building deserved, but were not given an opportunity to voice their opinion or participate in the decision to allow the "unsightly" construction of this building. What is our recourse now that it's been done?

No doubt the owner has now given the go-ahead to his friends that have decided to come to McCall to erect the biggest, ugliest structure they want with no regard to City Planning and Zoning Codes. The residents of this "residential" impact area are supposed to be protected from this type of obscene, commercial type structure that, by law has no place in a residential neighborhood. This type of construction will have a negative effect on our property values as well as the wildlife and natural beauty of this very special location near McCall. Who will we have to thank for that?

The old adage comes to mind: Don't ask for permit, just plead for foregiveness. Unfortunately, this type of behavior is used by those with lots of money and a disregard for others and the community. Let's put a stop to this because if we don't or can't it will continue to ruin our life style in McCall.

Thank you for your time and consideration of this urgent situation.

Rachel Santiago-Govier

From: Kimberly A Apperson <kimapperson@icloud.com>
Sent: Monday, February 8, 2021 8:35 AM
To: Morgan Bessaw
Cc: Pam Wissenbach; Michael Wissenbach; Teresa Wolfenden; Bonnie Makinson; Sherpa San Huie; Lewann Ball; Bob Krahn; Michelle Groenevelt
Subject: Comments and public records request for DR-20-41
Attachments: Kim Apperson - Public Records Request Form completed 20210208.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

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Hi Morgan,

I am concerned that the P&Z Commission does not have the complete and correct information they need to direct the course of action needed for DR-20-41. At the February 2 meeting the applicant presented a very different timeline and direction that he said he received from the City than you relayed to me and other neighbors through emails that started with my first inquiry to you on September 16. I have all of your correspondence to me.

I am requesting, in the attached form, that all correspondence between the City staff and the applicant be provided to the Commission and the public prior to the continued public hearing for this application.

I am very concerned that the applicant acted like he knew nothing about needing to provide landscaping and site plans for his project, and was told to continue work on it by the building inspector. You specifically told me you were working with him to complete these plans, and that he knew all work on the structure was at risk after he was notified in late September. I also would like to know what consequences there are for falsifying a building permit. The Commission needs to be informed that the applicant specifically stated his project was less than 3,500 square feet in total area.

I request that the City provide a measured building height, as I am not convinced it is less than 30 feet tall. I also would like to have measurements of conditioned living space vs. unconditioned space of all structures on the property. Per code 3.3.08(D)1, all storage space cannot exceed 50% of living space. There is already an additional storage structure on the property that looks to be about 100 square feet.

Adding a little bit of wood trim and a few trees and rocks is not an acceptable solution for this large industrial structure in a residential zone. It is a steel structure that can, and should be, dismantled, reduced in size and redesigned to be a true residence.

And finally, I thank you in advance for your attention to this project. As a past P&Z commissioner and chair during a previous building boom I do appreciate how busy City staff and the Commission is with reviewing many large projects, and how long meetings can wear everybody down. But please do not let these smaller projects fall through the cracks, as they have a cumulative effect on our entire community. Please help us all make that a positive effect.

Please share this email and public records request with the P&Z Commission.

Sincerely,
Kim Apperson

Rachel Santiago-Govier

From: Lou Beaudry / Bonnie Makinson <beumak4@gmail.com>
Sent: Saturday, February 27, 2021 6:27 PM
To: Rachel Santiago-Govier
Subject: FW: DR-20-44, 651 Stockton Drive

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From: Lou Beaudry / Bonnie Makinson <beumak4@gmail.com>
Date: Saturday, February 27, 2021 at 6:17 PM
To: <mgroenevelt@mccall.idaho>
Subject: FW: DR-20-44, 651 Stockton Drive

From: Lou Beaudry / Bonnie Makinson <beumak4@gmail.com>
Date: Saturday, February 27, 2021 at 6:10 PM
To: "mbessaw@mccall.id.us" <mbessaw@mccall.id.us>
Subject: DR-20-44, 651 Stockton Drive

To: Morgan Bessaw and McCall Planning and Zoning Commission
Re: DR-20-44, 651 Stockton DR.

As a resident of West Place Subdivision and being within sight of the building located at 651 Stockton Drive, I strongly agree with issues brought forth by Kim Apperson. This structure is inappropriate in both size and appearance for our residential neighborhood. At the Feb. 2nd P& Z meeting, the Commission expressed the sentiment that the omission of the design review was the fault of the City, therefore they had a responsibility to work with the applicant. Mr. Carpy's building permit application specifically states that the structure would be less than 3,500 feet. It has subsequently come to light that the structure is larger than stated. Given that Mr. Carpy falsified information on his building permit, it seems that City should hold Mr. Carpy responsible. I believe that it is the duty of the Commission to insure that said construction is in compliance with the building permit. The City may have been at fault for not completing a design review prior to construction, but Mr. Carpy has the responsibility to comply with the information he submitted on his building permit.

This is a large rectangular all metal building that dominates our neighborhood and looks very out of place. Suggestions from the Feb. 2nd meeting regarding softening the appearance of the building by installing garage doors with windows, adding wooden rails to the decks and a bit of landscaping are questionable. It's difficult to believe that the building's appearance could be disguised as anything other than what it appears to be: a large commercial looking building.

Sincerely,
Louis Beaudry
Bonnie Makinson

Rachel Santiago-Govier

From: Pam Wissenbach <pamwissenbach@gmail.com>
Sent: Tuesday, March 2, 2021 10:14 AM
To: Michelle Groenevelt; Morgan Bessaw; Rachel Santiago-Govier
Subject: 651 Samson Trail DR020-44

--- This email is from an external sender. Be cautious and DO NOT open links or attachments if the sender is unknown. ---

Good morning,

I tried to send this yesterday, but I was having internet issues. I apologize for the timing of getting this to you.

I want to voice my continued concern about the structure being built at 651 Samson Trail. Since the last meeting of the Planning and Zoning Commission, Mr. Carpy has continued to do work on the building without submitting the requested and required documents to the City of McCall. As I stated earlier, he seems not to ask for permission, but will ask for forgiveness later.

This structure does not fit with the character of our neighborhood. It looks like an auto mechanic shop with the three big doors on the side. There are specific guidelines from the City of McCall that we all followed to build or remodel our homes. Mr. Carpy should be held to those requirements and guidelines also.

This structure needs a major overhaul to be compliant with the regulations of McCall and to blend in with our neighborhood. This overhaul may be that the building needs to be dismantled and redesigned.

Thank you for your work on this.

--

Pam Wissenbach
280 May Road
McCall, ID

April 5, 2021

TO: McCall Planning and Zoning Commissioners

RE: DR-20-41 Carpy residence.

We are writing to express our opposition to the Carpy residence on 641 Stockton Road. There are several issues with this new construction.

- The square footage on the building permit application was grossly understated.
- The driveway from Stockton does not have a culvert for the water to flow through.
- This structure does not comply with design review considerations set for the Commission's review:
 - Building scale
 - This building is a two story, large metal structure. The size of this structure is not compatible with the neighboring homes or with the R1 zoning.
 - Blank walls
 - This large structure has very few windows leaving large segments of unsightly blank walls that further contribute to its industrial look.
 - Adjacent buildings and uses
 - This structure is built in a residential area where the homes look like homes, with stained or painted siding, several windows on all sides of the houses, and the height is either a single level or a much more modest two story height. **This building does NOT fit into the neighborhood.**
 - Enhance McCall Classic Styles
 - This structure is in sharp contrast to the homes in our neighborhood and those typical of McCall. The architecture of this building does nothing to enhance the neighborhood or the McCall area.
 - Wall materials
 - The walls of this structure are metal, further contributing to its industrial look. The proposed boxing in of the supports will do nothing to make this building look less industrial.
 - The plants as screening
 - Although helpful, the proposed plantings would be minimally effective. To screen this structure, the trees need to be about 30 feet tall. It will take years for these trees to provide effective screening.

This structure is setting a bad precedent for new construction in our neighborhood. It is obtrusive and unattractive; it does not fit into this residential area or McCall. We are asking that you have Mr. Carpy start over, following the correct procedures and constructing a building that looks like a home, not an industrial eyesore in a residential area. We are asking that you protect the integrity of our neighborhood.

Sincerely,

Mike and Pam Wissenbach
280 May Road
McCall, ID

In To: Members of the McCall Area Planning and Zoning Committee
Re: **DR-20-44, 651 Stockton Drive, Charles Carpy. Tax Parcel 43, lot 53, block 1, West Place Subdivision.**

We're in strong opposition to the design and scale of this new build in our neighborhood. West Place Subdivision is historically a residential neighborhood with homes that are single-family dwellings and noncommercial in appearance. Mr. Carpy's structure is staggering and blatant in its non-adherence to the residential norms of homes in the area.

How did this come to pass? First of all, Mr. Carpy falsely represented the size of the structure on his building permit application, thus implying that it did not require a design review. **We feel that there should be consequences for falsifying one's application.** This was not the city's fault. The blame lies completely with the applicant.

Secondly, when City staff were made aware of the potential size of the building in Sept. of 2020, thanks to one of our neighbors, the City notified Mr. Carpy that he must apply for a Design Review. He was also notified that any further work would be **at his own risk.** Despite these notifications, Mr. Carpy dragged his feet and did not submit a Design Review application until March, 2021, **six months after notification. Throughout the six-month period Mr. Carpy continued construction "at his own risk."** Obviously, he wasn't, and isn't, too concerned about the City holding him accountable for misinformation submitted on his building permit, nor was he concerned about submitting a Design Review in a timely manner.

We're asking the P&Z Commission to uphold the norms of our residential neighborhood and stick up for the homeowners in this subdivision. **Would you like this structure in your neighborhood? Planting a few pine trees and adding some wood trim will not change the appearance of this large metal warehouse.** We suggest that Planning and Zoning require Mr. Carpy to tear down the existing building and subsequently apply for a building permit for a home that is residential in appearance.

Sincerely,
Louis Beaudry and Bonnie Makinson
481 Ellis Rd., McCall, ID 83638

From: [Kimberly A Apperson](#)
To: [Brian Parker](#)
Subject: comments on DR-20-44
Date: Thursday, July 29, 2021 10:44:29 AM

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Brian,
Please provide our comments to the P&Z Commission.

McCall Area Planning and Zoning Commission,

The landscaping and building design changes proposed for 641 Stockton Drive are, once again, very inadequate and do not even remotely adhere to McCall's residential development standards.

Mr. Carpy needs to develop a landscaping plan that follows McCall code 3.8.13 for Landscaping and Buffering. His proposal to plant three walls of Leyland Cypress along the perimeter of his property (a non-native tree that is unlikely to survive here anyway) is counter to that entire section of our code. Even if Leyland Cypress were to survive, such landscaping would look as horribly out of place in our neighborhood as does his large industrial metal building.

Proposed architectural modifications are limited to a few more windows and some outdoor lighting, which amount to small bandaids on this eyesore of a building. All exterior lighting must meet our dark sky ordinance.

I see in your meeting packet for August 3 many of the neighbor comments that have been submitted to you over the past 10 months of this project. I do not see a Staff Report. Will this issue once again be continued? Mr. Carpy has had ample time to comply with development requirements, and he is clearly not taking McCall's building ordinances seriously. This construction should now be removed completely and allowed to start again only after complete and reasonable applications are reviewed by City Staff and approved by the Commission.

At the very least, please deny this Design Review application as it has been submitted, and demand that further construction work be stopped on this property until you have reasonable architectural and landscaping plans to review.

Thank you,

Kimberly A. Apperson and Joseph C. Baugh
415 South Samson Trail
McCall

McCall Area Planning and Zoning Commission

Staff Report

DR-21-11

1040 Chris Lane

December 7, 2021

Applicant: Linzie Green
Representative: Matt Arnold
Application: Design Review
Zoning: R1 – One Dwelling Unit per Acre

Project Description:

A Design Review application to construct an attached three car garage with second story to an existing single-family. The garage would be located on the north east side of the residence in the location of an existing parking pad. The second story of the garage would be for a bonus room, including plumbing and bathroom, and unconditioned storage space. The only other site improvements that the existing driveway would be realigned to fit the new garage, stone wainscotting across the entire front elevation of the house and garage and new paint and trim color. The garage addition would match the brown wood log siding and grey trim of the existing house.

Existing residence	2,270 sf
Existing residence remodel	130 sf
Existing Patio/deck	360 sf
Bonus room	630 sf
Unconditioned attic space	485 sf
Garage	1490 sf
Total	5,365 sf

R-1 Zoning District Requirements:

Requirement	Code (MCC 3. 3.03)	Proposal
Minimum property size	1 acre	2.38 acres
Street Frontage	100'	270'
Front Property Line Setback.	25'	25+'
Rear Property Line Setback	20'	20+'
Side Property line setback	23'	42'
Total Side Property line setback	69'	84'
Lot Coverage	13%	5.9%
Building Height	35'	22'
Maximum residence size	10000 sf	5365 sf

Code Narrative

MCC 3.16.01: Design Review approval is required when a single-family residential structure is in excess of 3,500 sq. ft. and requires Planning and Zoning Commission for any over 5,000 sf.

MCC 3.3. 09 Design standards to allow development that is complementary to the natural environment, scenic mountain setting and historic small-town character.

MCC 3.8.064 Driveways: which provides standards for driveway width, grade, and setbacks.

MCC 3.8.10 Fences and Walls which establishes requirements for walls.

MCC 3.8.13 Landscaping: which requires landscape plans as part of a design review.

MCC 3.8.15 Snow storage: which requires that area be set aside on the site for snow storage.

MCC 3.8.23 Site Design: which requires that site development maintains the natural features of the site including significant vegetation, watercourses, wildlife corridors, and natural features.

MCC 3.14 Lighting: which established standards for exterior lighting to promote and maintain public safety and preservation of the night sky.

Comments

Agency –

No objections or response with no comments were received by e-mail from the McCall Fire and EMS (3/12/21), Idaho Department of Transportation (3/10/21), and Idaho Department of Lands (3/15/21).

McCall Public Works:

In a March 24, 2021 e-mail, the City of McCall Public Works department stated that they had reviewed the materials that were provided for this proposal and had the following comments and requirements:

1. It is not anticipated that the stormwater drainage will alter much or affect neighboring properties taking into account the size of this project in relation to the size of the parcel. A formal stormwater management application, plan, and report will not be needed with this project. Reevaluation of these requirements will need to occur if the size and location of the proposed project on the parcel change.
2. The City's Building Department will monitor and inspect the temporary on-site stormwater controls.

IN RE:)
)
GREEN GARAGE) **McCALL AREA PLANNING AND ZONING COMMISSION**
Design Review) **FINDINGS OF FACT, CONCLUSIONS OF LAW, AND**
Application Number:) **DECISION**
DR-21-11)

FINDINGS OF FACTS

Applicant: Linzie Green

Representative: Matt Arnold

Application: Design review of a new, two story attached garage to an existing residence.

Address: 1410 Chris Lane, McCall. Idaho

Location: Lot 95 of the Valley View Subdivision situate in the SW ¼ of Section 30, T1BN, R3E, B.M. Valley County, Idaho

Public Notice: Newspaper: The Notice of Hearing was published in the *Star News* on November 18, 2021

Mailing: The Notice of Hearing was mailed by the applicant to property owners within 300 feet on November 16, 2021.

Posting: The Notice of Hearing was posted by the applicant on the subject property on November 17, 2021.

Zoning: R1 – One Dwelling Unit per Acre

Property Size: 21, 944 sq. ft.

Lot Coverage: 4,771 sq. ft. (91% of allowable)

Building Height: 32' 11^{3/4}"

Proposed Setbacks: Per McCall City Code 3.3.03, the minimum required setbacks for this parcel are 24 ft. from the front property line, 20 ft. from the rear property line and 23 ft. from either side property line. The setbacks as proposed are:

Front: 25 ft.+ **Rear:** 30ft.+ **Sides:** 23 ft.

APPROVAL STANDARDS

Design Review

The commission or administrator shall determine the following before approval is given:

- 1. The project is in general conformance with the comprehensive plan.**
- 2. The project does not jeopardize the health, safety or welfare of the public.**
- 3. The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance, adopted by the city of McCall. Building height rule in relation to the side yards snow shed**
- 4. The project will have no substantial impact on adjacent properties or the community at large.**

Please see the review of the Design Requirements below.

MCC 3.3.09 DESIGN REQUIREMENTS

General Objectives for Residential Design

- 1. Support a residential character and is compatible with its surrounding neighborhood:**
The proposal is consistent in size, design and character with the residences in the surrounding neighborhood.
- 2. Preserves natural features:** The site has been used as a parking pad and is clear of any natural vegetation. The proposal intends to maintain the remaining undisturbed area of the site in natural vegetation.
- 3. Promotes active and safe streetscapes.** The garage will maintain the existing driveway intersection with the street and will be modified to fit the existing new garage.

Residential Review Standards

1. **Building Scale:** The proposal will add 1490 sf. to the existing residential structure for a total of with 5,365 sq. ft of living space and site coverage of 5.9% of the lot size. This is in scale with the lot and the surrounding neighborhood.

2. **Building Design**

- a. **Materials** The garage addition would match the brown wood log siding of the existing house. Stone wainscotting would be installed across the entire front elevation of the house and garage with new paint and trim color to the existing structure.
- b. **Roof:** The roof will be constructed of architectural roof shingles of a grey color.
- c. **Exterior Color** The proposed color palette is a mixture of brown and grey.
- d. **Blank Walls** There are no blank walls in the design.
- e. **Snow Loads** The structure is sufficiently setback to avoids the impacts of snow shedding on any adjoining property.

MCC 3.8 General Development Standards

MCC 3.8.03 Tree Removal: No trees will be removed.

MCC 3.8.062 Parking: which requires two parking spaces. A 1490 sq. ft. garage is included in the design which is sufficient for the parking of two vehicles.

MCC 3.8.064 Driveways: which provides standards for driveway width, grade, and setbacks.

MCC 3.8.13 Landscaping: which requires landscape plans as part of a design review. The site plan indicates that all existing landscaping and natural vegetation and trees will remain.

MCC 3.8.15 Snow storage: which requires that area be set aside on the site for snow storage. Two areas have been designated on the site plan for snow storage.

MCC 3.8.23 Site Design: which requires that site development maintains the natural features of the site including significant vegetation, watercourses, wildlife corridors, and watercourses.

- a. **Preserve natural resources:** The site plan includes 94% of the site without coverage of impervious surfaces.
- b. **Reduce land and water modifications:** The lot is relatively flat requiring no land modification.
- c. **Location to preserve prominent skylines:** N/A
- d. **Underground utilities:** All utilities will be undergrounded.

MCC 3.14 Lighting: which established standards for exterior lighting to promote and maintain public safety and preservation of the night sky. A condition of approval is for all exterior lighting fixtures to meet the code standards.

DEPARTMENT/AGENCY COMMENTS

McCall Public Works:

In a March 24, 2021 e-mail, the City of McCall Public Works department stated that they had reviewed the materials that were provided for this proposal and had the following comments and requirements:

1. It is not anticipated that the stormwater drainage will alter much or affect neighboring properties taking into account the size of this project in relation to the size of the parcel. A formal stormwater management application, plan, and report will not be needed with this project. Reevaluation of these requirements will need to occur if the size and location of the proposed project on the parcel change.
2. The City's Building Department will monitor and inspect the temporary on-site stormwater controls.

CONCLUSIONS OF LAW

1. The City of McCall has provided for the processing of Design Review applications, pursuant to Title 3, Chapter 16 of McCall City Code.

2. Adequate notice of the December 7, 2021 public hearing was provided, pursuant to Section 67-6512, Idaho Code and Title 3, Chapter 15 of McCall City Code.
3. Upon compliance with the conditions noted below, the application meets the Design Review Standards set forth in Title 3, Chapter 16 of McCall City Code.

DECISION

THEREFORE, the McCall Area Planning and Zoning Commission hereby **approves** this Design Review application DR-21-11, provided that the following conditions are met:

1. Prior to issuance of a building permit, the applicant shall provide evidence that the design review plans were prepared by an Idaho Licensed architect or an architect licensed in another state which has an agreement with Idaho that recognizes the licensing.
2. Pursuant to McCall City Code (MCC 3.16.08), design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.
3. Prior to issuance of a building permit, the applicant shall submit light fixture details for review and approval in compliance with McCall's Outdoor Lighting Ordinance.
4. Prior to the issuance of a building permit, the applicant will provide the City with a written statement acknowledging that the second story area above the garage is not for occupancy as an accessory dwelling unit or short-term rental without first obtaining the required permits or licenses from the City of McCall.

Findings of Fact **adopted** this 7th day of December, 2021.

Robert Lyons, Chair
McCall Area Planning and Zoning Commission

Attest:

Brian Parker, City Planner
City of McCall

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received: _____

Fees Paid: _____

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # _____ Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # _____ Scenic Route (SR) - \$300
- # _____ Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Administrative Approval (AA) - \$50
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: LINDIE J. GREEN Email: _____

Mailing Address: 1040 CHRIS LN. McCall, ID. 83638

Property Owner 2 (If Applicable): AMBER J. GREEN Email: greena@slhs.org

Mailing Address: 1040 CHRIS LN McCall, ID. 83638 Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: Matt C Arnold Email: MatthewArnold@yahoo.com

Mailing Address: 4/6 Floyd St Phone: 307-6892813

PROPERTY INFORMATION

Address(es) of Property: 1040 CHRIS LN. McCall, ID. 83638

Legal Description of Property: 2.39 ACRES . Valley View Subdivision Number 3 Lot 95

Zoning District of Property: R1 Project Sq. Footage (If Applicable): _____

Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: (please attach supplemental information if needed)

It is being proposed that an attached garage addition be constructed to the existing residence located at 1040 Chris Lane in McCall, ID. 3 car garage with loft area above.

SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

Luise J. Green

Property Owner 1

Imber Green

Property Owner 2 (If Applicable)

Matt C. Arnold

Agent/Authorized Representative



FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

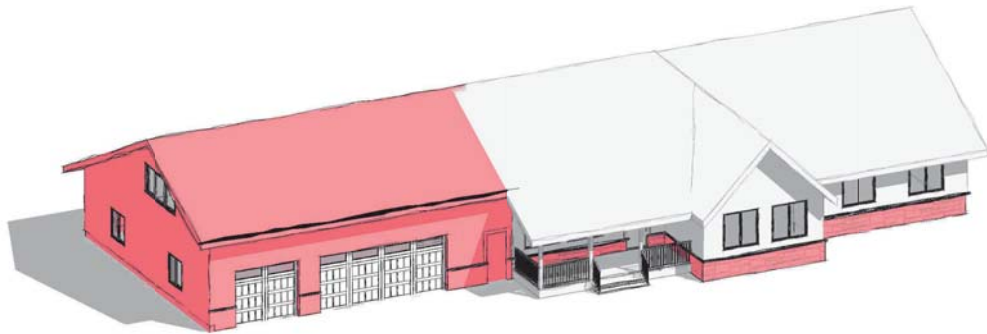
Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.

Green Garage Addition

1040 Chris Ln, McCall, ID 83638

Parcel # RP002800000950



Sheet Description

G100	Title
G101	General
G102	General Continued
A100	Site Plan
AD 101	Demo
A101	1st Floor Plan
A102	Bonus Floor Plan
A103	1st Floor Electrical Plan
A104	Bonus Electrical Plan
A201	Elevations
A301	Section
A401	Details
A105	Mechanical

Green Garage Addition
1040 Chris Ln, McCall, ID 83638
Parcel # RP002800000950



Design
Development

2/13/2021

Title

G100

Design Criteria

Building Codes

McCall Title 2 Building Codes	Building Codes, Regulations, and Permits
2018 International Building Code	
2018 International Residential Code	Including Parts V & VI
2018 International Engery Concervation Code	Residential portion ammended
2017 National Electric Code	
2017 Idaho State Plumbing Code	
2018 International Fuel Gas Code	
2015 International Fire Code	

Mcall Fire District	Sub-district 1
Zonning	R1
Elevation:	5,030
Seismic Category:	D
Weathering:	Severe
Wind:	115 MPH, exposure C (Per IRC and Valley County) 90 MPH, exposure B (Per McCall City) 169 MPH with topographical effects ENGINEER TO CONFIRM
Frost Depth:	24"
Termite:	Slight/moderate
Winter Design Temp:	8976 HDD
Flood Hazard:	None
Mean Annual Precipitation:	26"
Cliimate Zone:	5a
Square Feet:	
Existing Residence (excluded in remodeled)	2,270 sq ft
Existing Residence (included in remodeled)	130 sq ft
Existing Patio and Deck	360 sq ft
Bonus Floor	630 sq ft 485 sq ft (unconditioned attic storage)
Garage	1,490 sq ft
Driveway	5,825 sqft
Structural:	
Snow Load	120 to 150 lbs ENGINEER TO CONFIRM
See structural sheets	

General

- Contractors and Sub-contractors shall perform their trades and duties in a manner conforming to the procedures and requirements of the adopted codes and authority(ies) having jurisdiction, described under design criteria.
- Garage walls and ceilings shall be finished with not less than 1/2" gypsum board. If space above garage is habitable 5/8", type X gypsum board shall be used on ceiling.
- Stairs with accessible space underneath of them shall be protected underneath with not less than 1/2" gypsum board.
- Attic access hatches and doors shall have insulation greater to or equal to the adjcent assembly.
- Ventilation of existing crawl space and attic should not be reduced. Relocate vents as needed to maintain.
- All heating fixtures and equipment shall be installed according to manufactures instructions.



AD DESIGN
Jason
Allred

Green Garage Addition
1040 Chris Ln, McCall, ID 83638
Parcel # RP00280000950

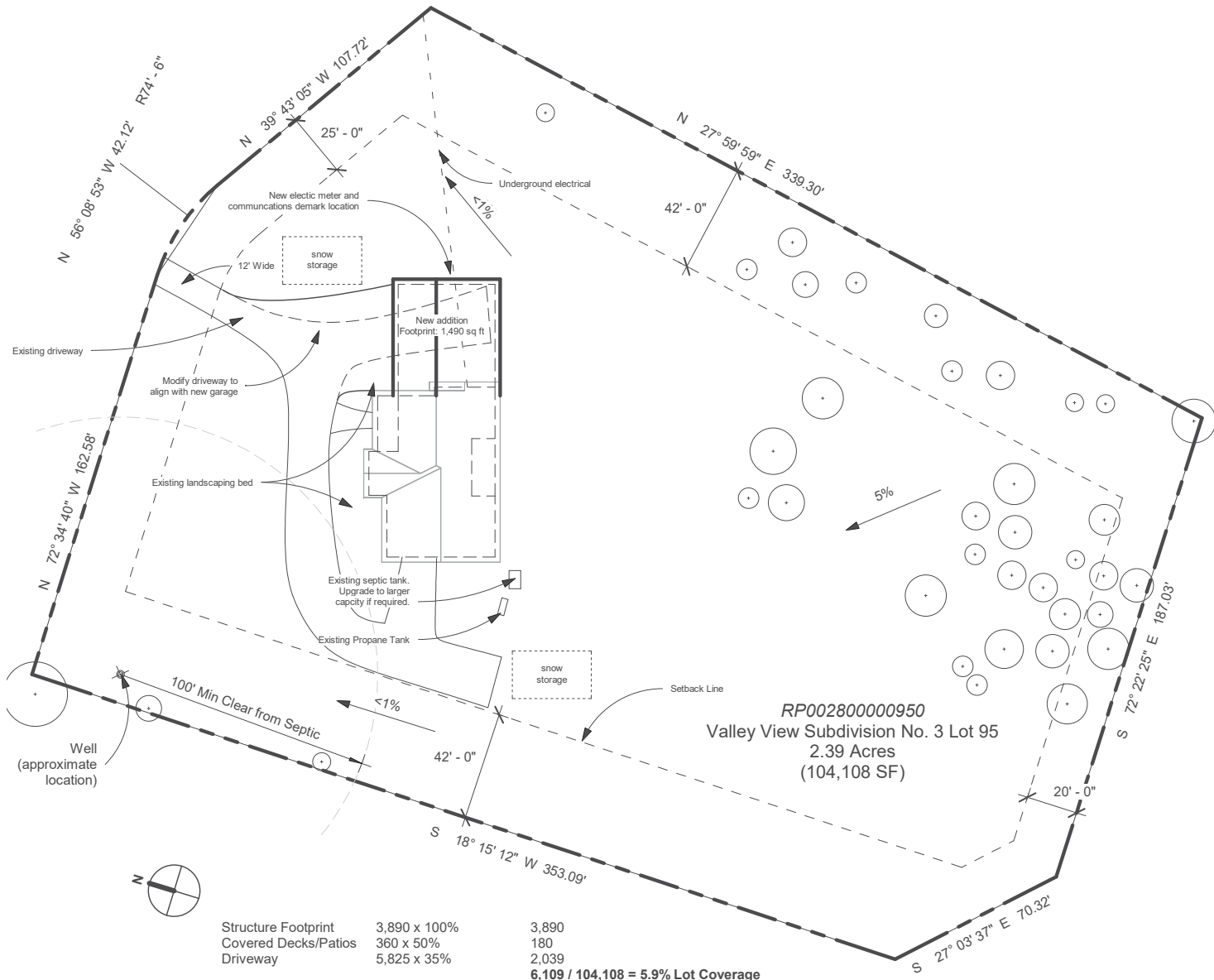
Design
Development

2/13/2021

General

G101

1. Provide manufacture manuals for mechanical and water heating systems.
2. All flues and combustion air intakes for wood burning fireplaces shall have tight fitting dampers.
3. Mechanical ventilation systems shall have gravity dampers. Those not a part of tested and listed HVAC equipment shall meet efficiency and air flow limits.
 - A. Kitchen exhaust fan(s) shall have an exhaust rate of 100 cfm intermittent or 25 cfm continuous.
 - B. Bathroom-Toilet room fan(s) shall have an exhaust rate of 50 cfm intermittent or 20 cfm continuous.
4. Building cavities shall not be used as ducts or plenums.
5. HVAC piping conveying fluids above 105 °F shall be insulated with no less than R-3 insulation.
6. Heating and cooling equipment shall be sized per ACCA Manual S based on loads calculated per ACCA Manual J or other methods approved by the code official.
7. Air handler leakage shall be designated by manufacture to be no more then 2% of design air flow.
8. Protect all insulation on HVAC piping.
9. The heating system shall be controlled by a thermostat with the ability to program temperature set points for differing days and times, initially be programmed by manufacture.
10. Hot water pipe shall be insulated with no less than R-3 insulation.
11. 75% of lamps in permanent fixtures shall be high-efficiency OR 75% of fixtures which only use high-efficiency lamps shall be used.
12. Fuel gas lighting systems shall not use a continuous pilot light.
13. Venting of fireplaces, water heaters/boilers, and other fuel burning appliances shall be sized and installed according to the Idaho Plumbing Code (509 &510)
14. Recessed lights, outlets, and other penetrations of the air tight envelope will be gasketed, sealed, or otherwise constructed to restrict the leaking of conditioned air.
15. Maintain 6" min. in front of combustion openings and a 30" x 30" clear area with doors open on the service side of appliances
16. Supply or return ducts outside of the building thermal envelope shall be insulated;
 - A. In attics, ducts greater than 3 inches in diameter shall be insulated with no less than R-8 insulation and ducts less than 3 inches in diameter shall be insulated with no less than R-6 insulation.
 - B. In other areas, ducts greater than 3 inches in diameter shall be insulated with no less than R-6 insulation and ducts less than 3 inches in diameter shall be insulated with no less than R-4.2 insulation
17. For ducts and air handlers not located entirely within the building thermal envelop air leakage shall be tested at 25 pa either at rough-in or post construction.
 - A. At rough-in, leakage shall be less than or equal to 4 cfm per 100 sf of conditioned floor area with an installed air handler OR 3 cfm per 100 sf of conditioned floor area without an installed air handler.
 - B. At post construction, leakage shall be less than or equal to 4 cfm per 100 sf of conditioned floor area.



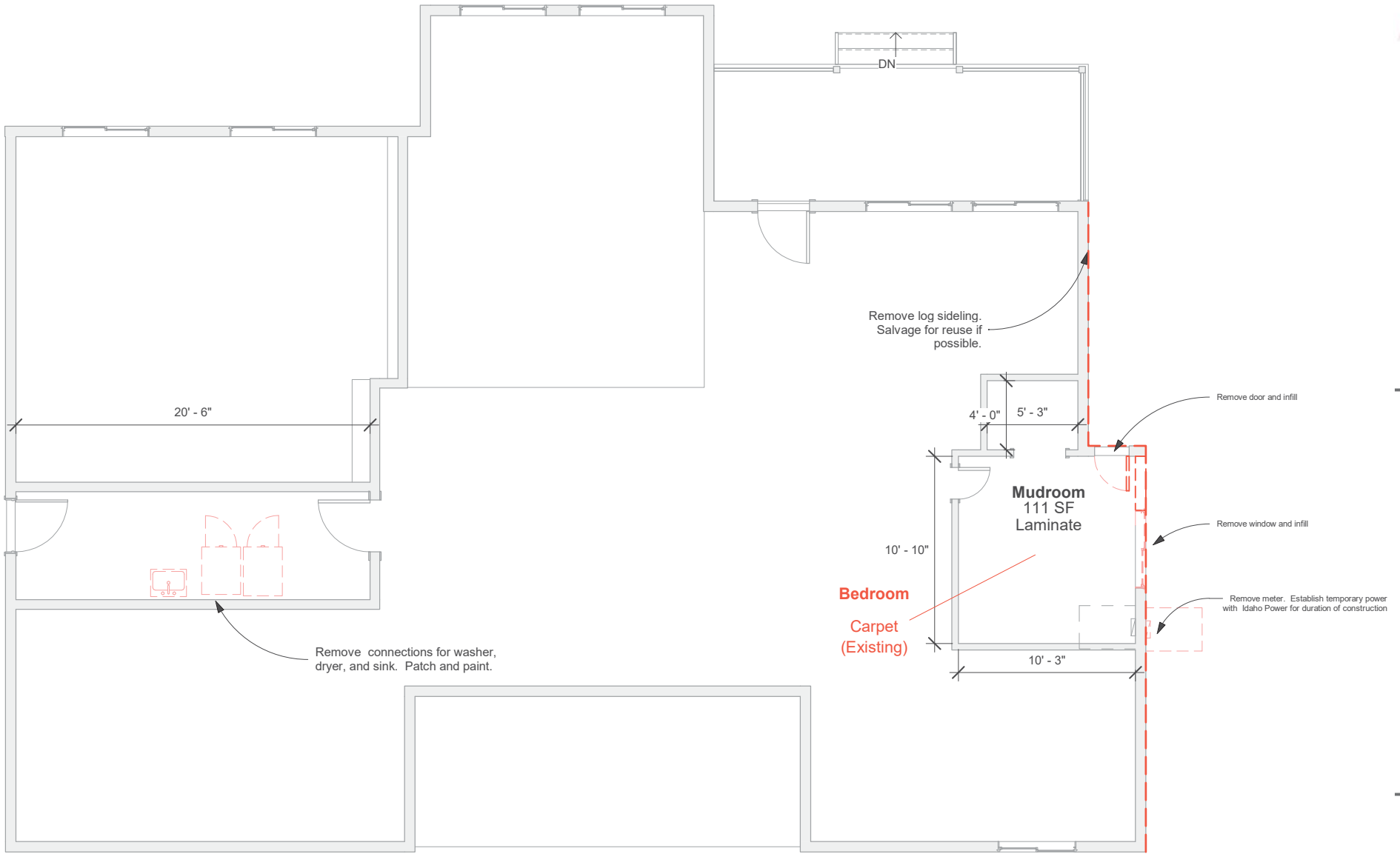
Structure Footprint	3,890 x 100%	3,890
Covered Decks/Patios	360 x 50%	180
Driveway	5,825 x 35%	2,039
		6,109 / 104,108 = 5.9% Lot Coverage

① Site Plan
1" = 40'-0"



Jason Allred

Green Garage Addition
1040 Chris Ln, McCall, ID 83638
Parcel # RP002800000950



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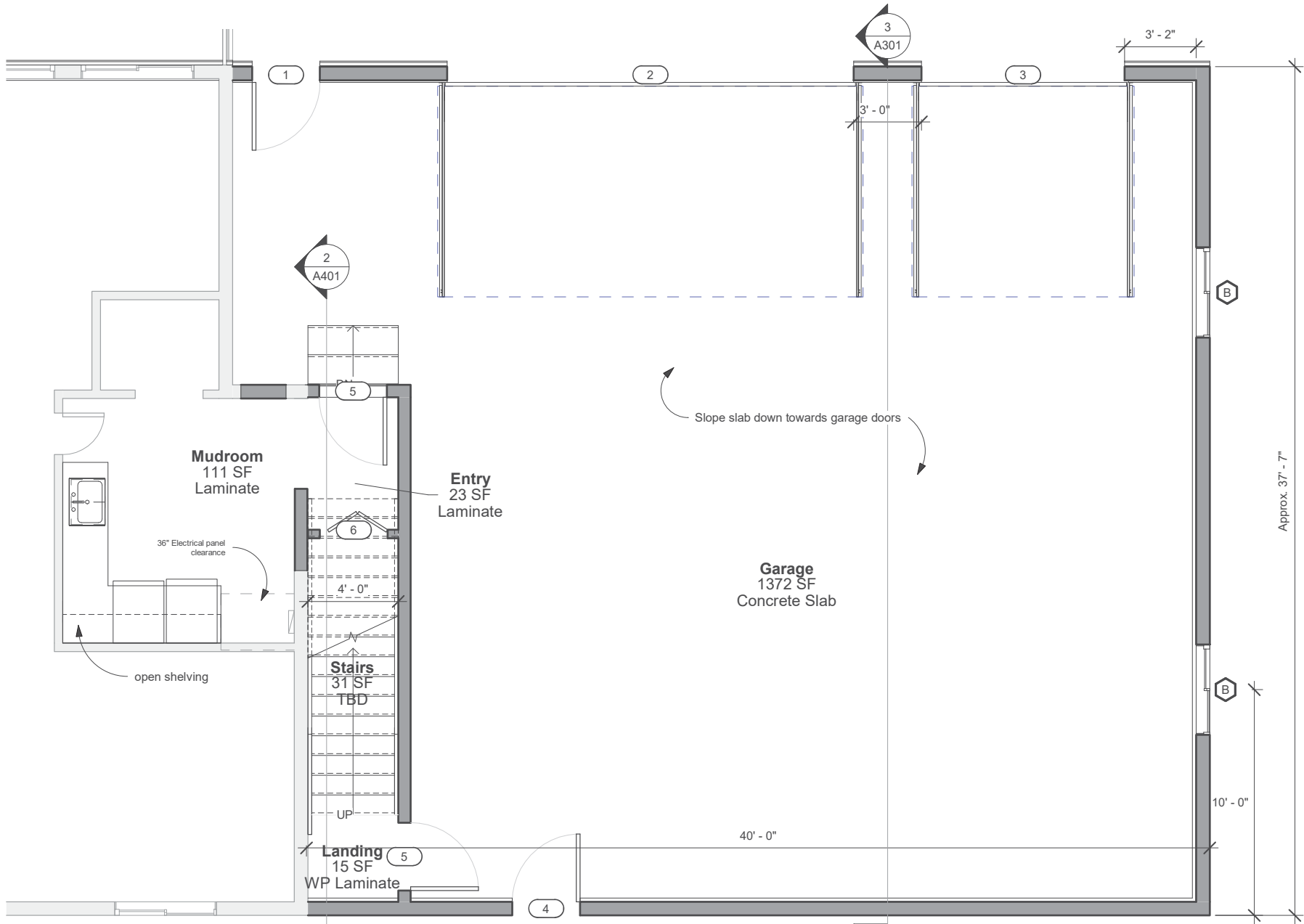
① 1st Floor Floor Plan Demo
3/16" = 1'-0"

Design
Development

2/13/2021

Demo

AD
101



① 1st Floor New Floor Plan
1/4" = 1'-0"

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Green Garage Addition
1040 Chris Ln, McCall, ID 83638
Parcel # RP002800000950

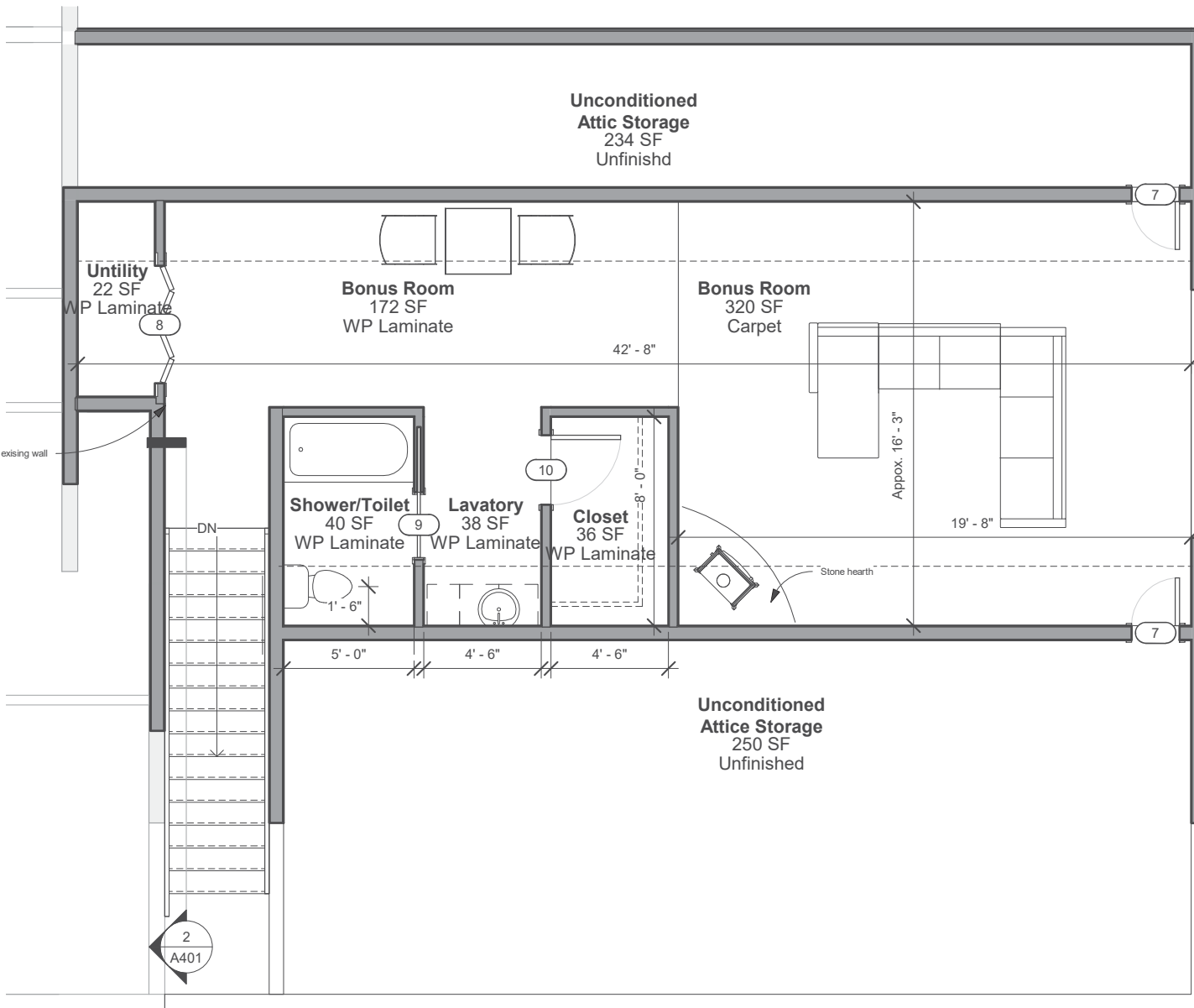


Design
Development

2/13/2021

1st Floor Plan

A101



① Bonus Floor
1/4" = 1'-0"



APT DESIGN
Jason
Allred

Green Garage Addition
1040 Chris Ln, McCall, ID 83638
Parcel # RP002800000950

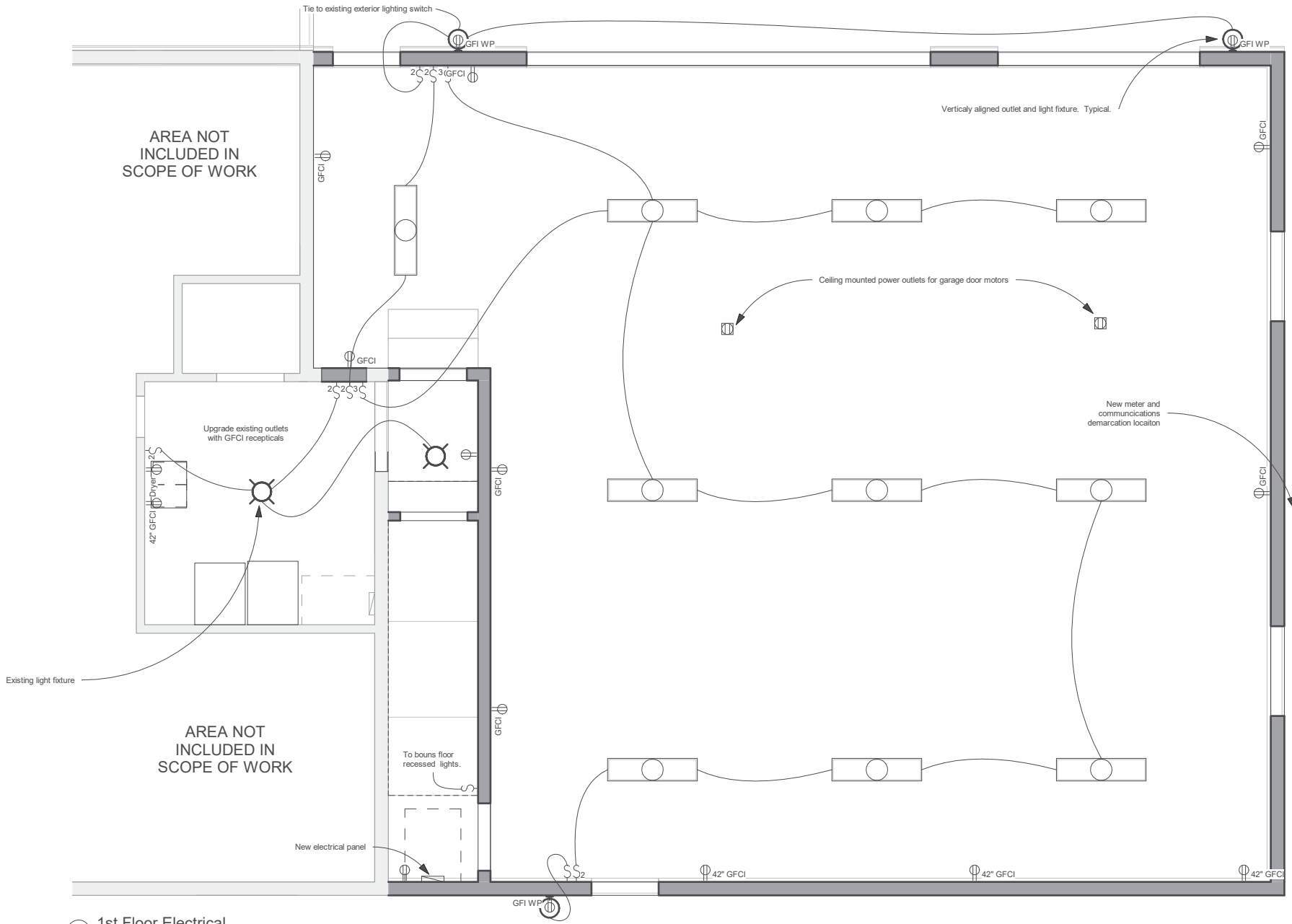


Design
Development

2/13/2021

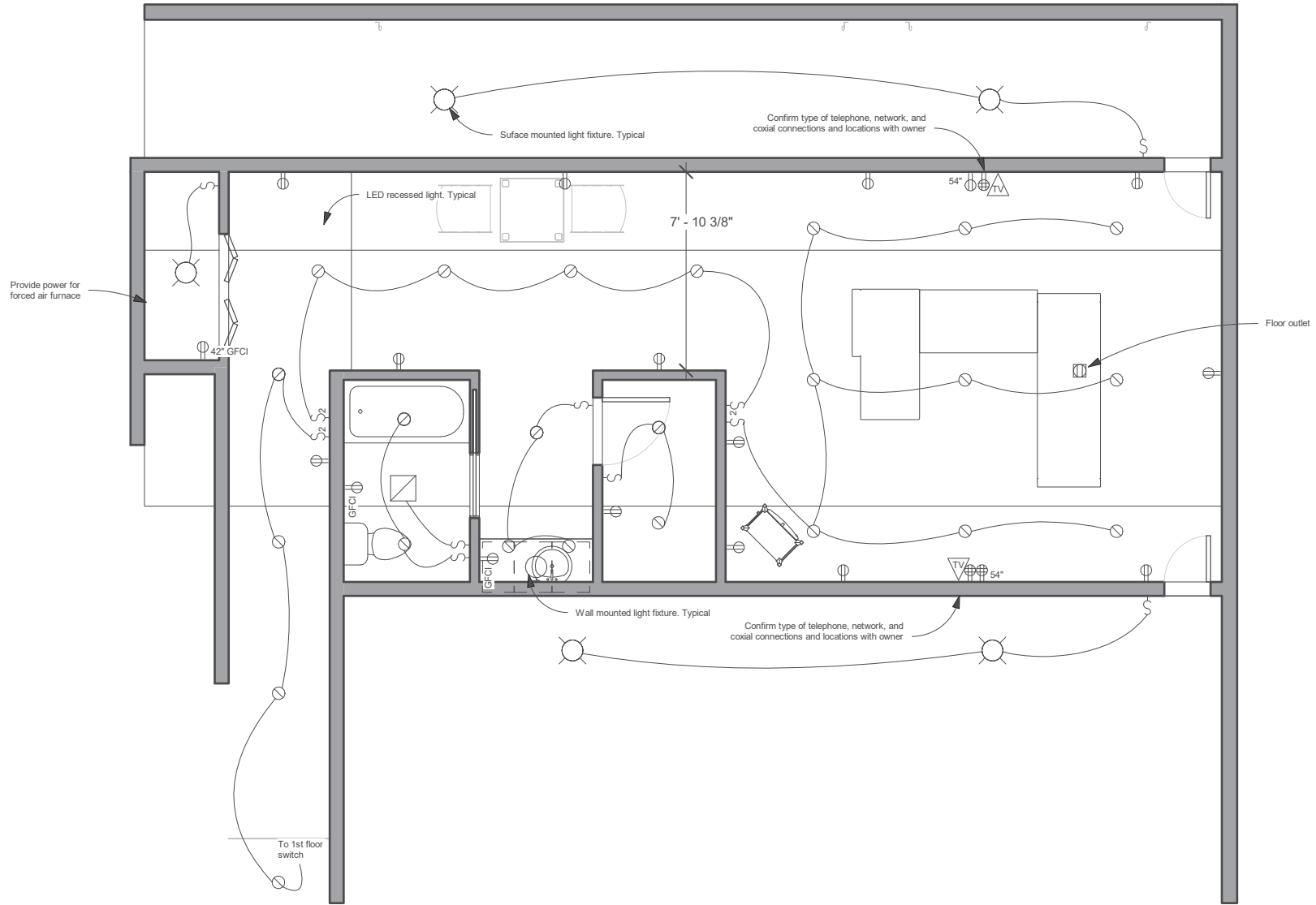
1st Floor
Electrical Plan

A103



① 1st Floor Electrical
1/4" = 1'-0"

2/13/2021 4:24:08 PM



① Bonus Floor Electrical
 1/4" = 1'-0"

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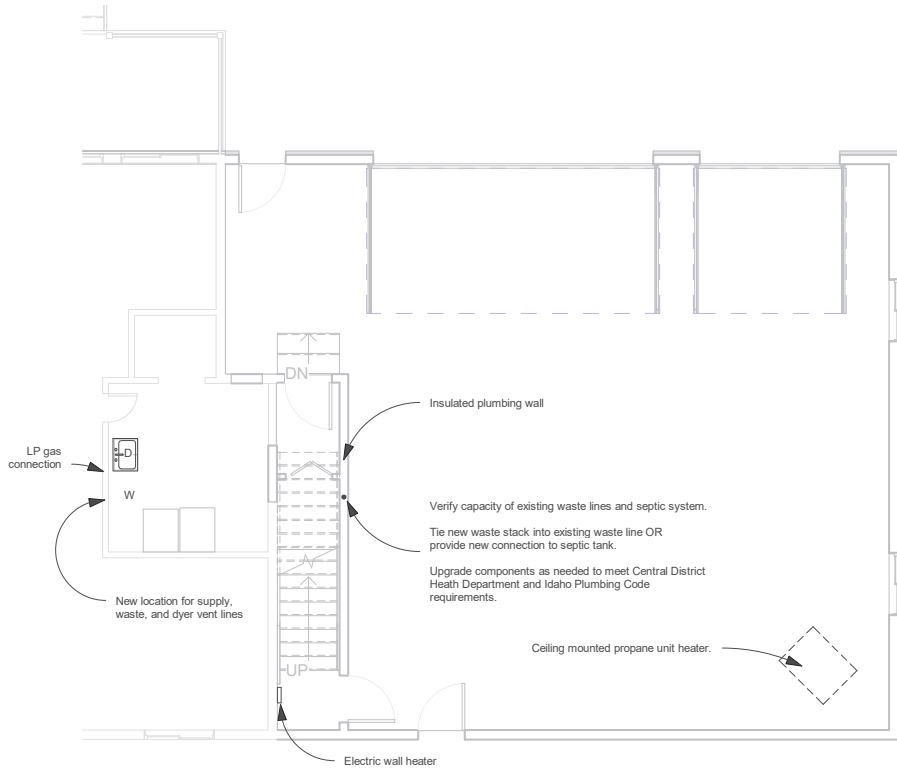


Design
 Development

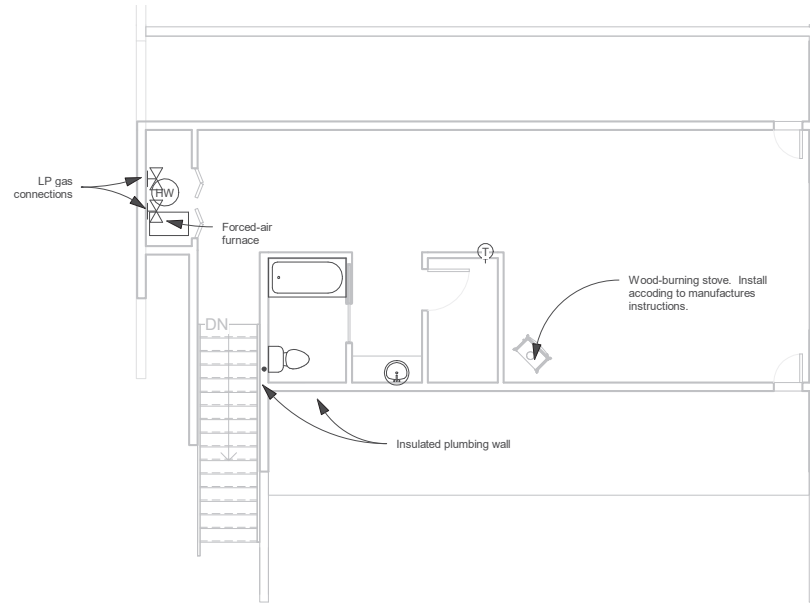
2/13/2021

Bonus
 Electrical Plan

A104

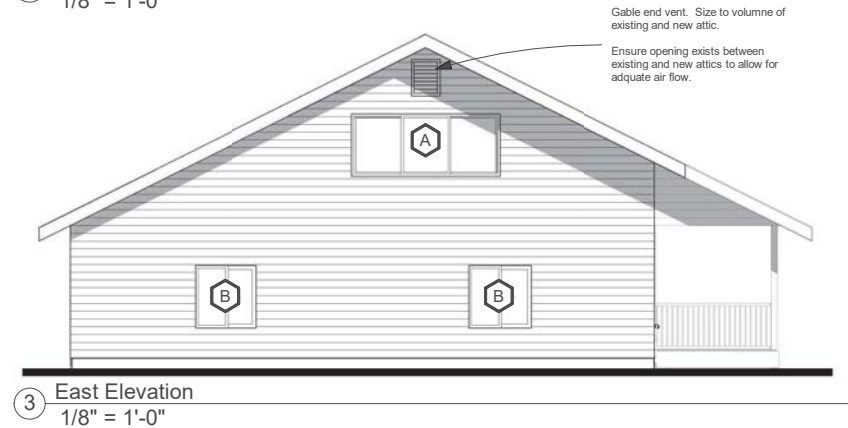
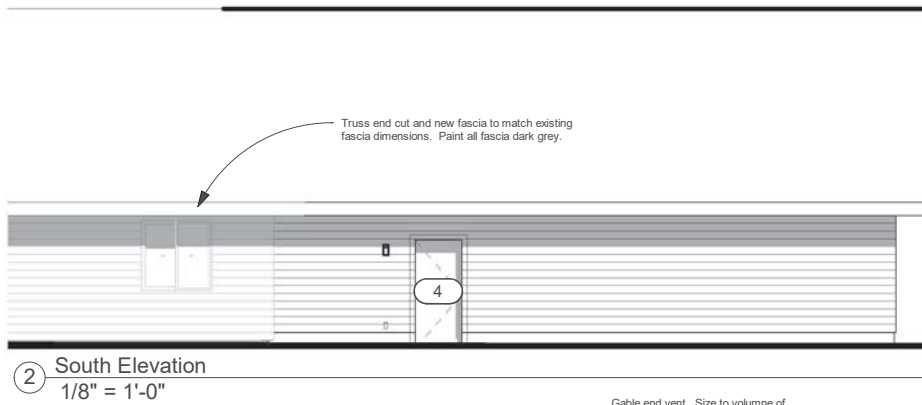
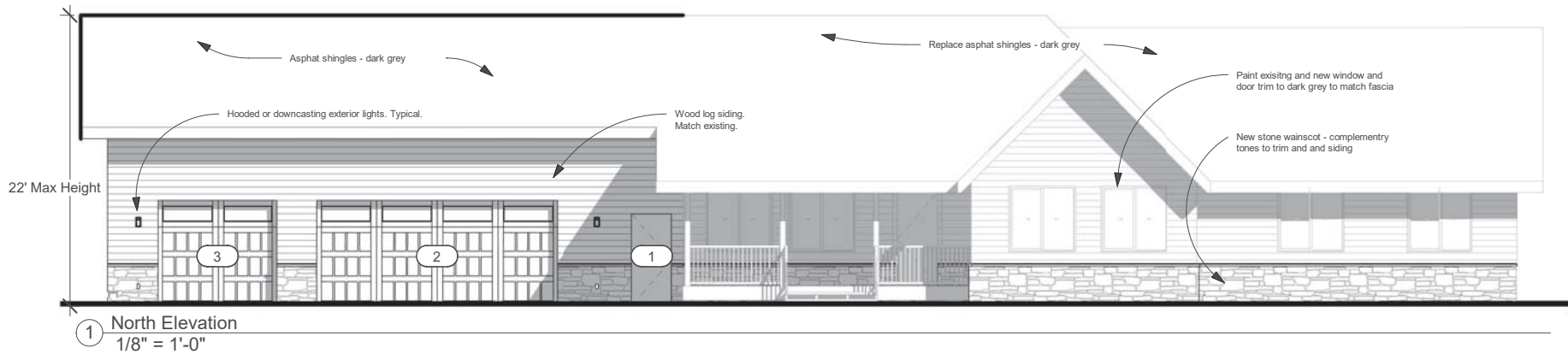


① 1st Floor Mechanical
 1/8" = 1'-0"



② Bonus Floor Mechanical
 1/8" = 1'-0"

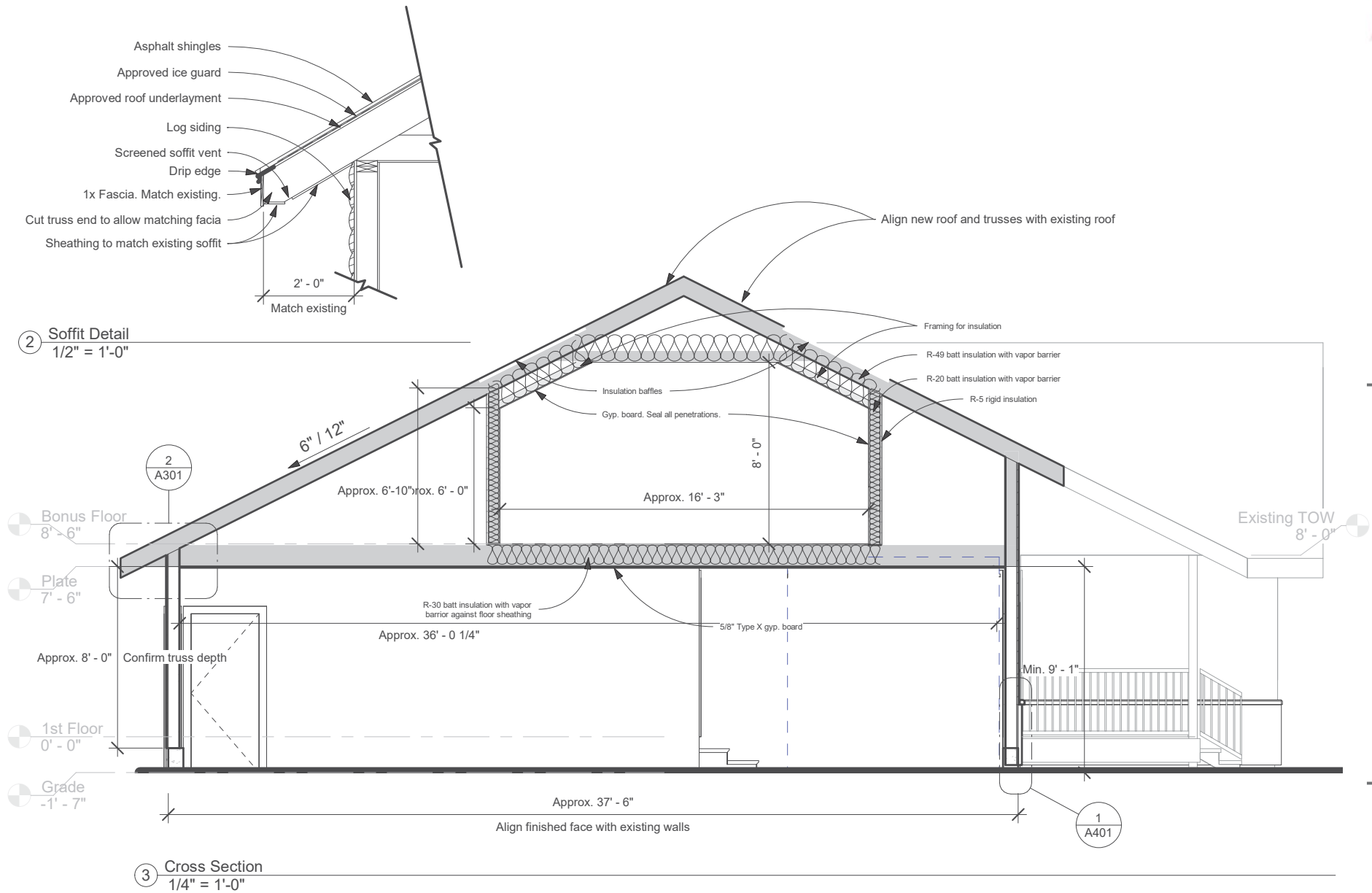
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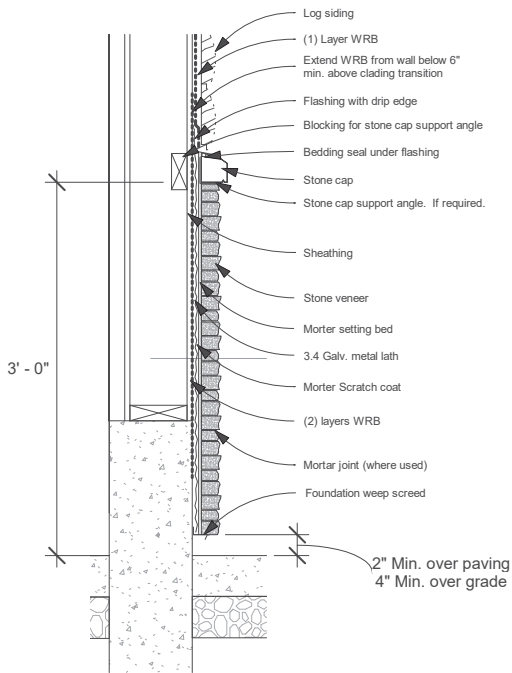
Door Schedule				
Mark	Width	Height	Head Height	Comments
1	3' - 0"	7' - 0"	7' - 0"	Exterior. Match style and finishes of existing front door
2	0' - 0"	0' - 0"	0' - 0"	Limited head room, insulated panels, articulated design
3	0' - 0"	0' - 0"	0' - 0"	Limited head room, insulated panels, articulated design
4	3' - 0"	7' - 0"	7' - 0"	Exterior
5	3' - 0"	7' - 0"	7' - 0"	Exterior. Self closing fire-rated door
6	3' - 0"	5' - 0"	5' - 0"	Single bi-fold door
7	1' - 10"	5' - 6"	5' - 6"	Insulated attic access door
8	4' - 6"	6' - 8"	6' - 8"	Vented double bi-fold doors
9	2' - 6"	6' - 8"	6' - 8"	Pocket door
10	2' - 8"	6' - 8"	6' - 8"	

Window Schedule					
Mark	Height	Width	Sill Height	Head Height	Comments
A	4' - 0"	10' - 0"	2' - 8"	6' - 8"	Match existing slider windows and trim
B	4' - 0"	4' - 0"	3' - 0"	7' - 0"	Match existing slider windows and trim

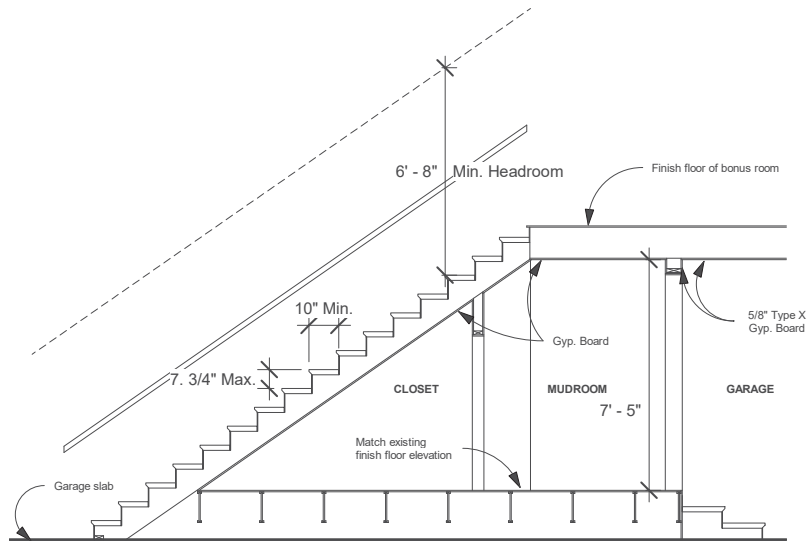
See structural sheets for structural construction



See structural sheets for structural construction



① Stone Wainscot Detail
 1" = 1'-0"



② Stair Section
 1/4" = 1'-0"

2/13/2021 4:24:13 PM

Meredith Todd

From: Morgan Stroud
Sent: Monday, May 10, 2021 10:03 AM
To: matthewcarnold@yahoo.com
Cc: Diane Kushlan; John Powell
Subject: DR21-11 - 1040 Chris Ln - Final Engineering Approval

Good morning,

I would like to add to the comments below that this project has Final Engineering Approval based on the materials that were reviewed in March.

Thank you,
Morgan S.

From: Morgan Stroud
Sent: Wednesday, March 24, 2021 8:47 AM
To: matthewcarnold@yahoo.com
Cc: Diane Kushlan <dkushlan@mccall.id.us>
Subject: DR21-11 - 1040 Chris Ln - Engineering Review #1

Good morning,

The City of McCall Public Works department has reviewed the materials that were provided for this proposal and has the following comments and requirements:

1. It is not anticipated that the stormwater drainage will alter much or affect neighboring properties taking into account the size of this project in relation to the size of the parcel. A formal stormwater management application, plan, and report will not be needed with this project. Reevaluation of these requirements will need to occur if the size and location of the proposed project on the parcel change.
2. The City's Building Department will monitor and inspect the temporary on-site stormwater controls.

Please let me know if you have any questions about the comments above.

Thank you,
Morgan Stroud, E.I. | Staff Engineer
216 E Park Street | McCall | ID 83638
Direct: 208.634.3458 | Cell: 208.315.2299



McCall Area Planning and Zoning Commission Staff Report

DR-21-36, SR-21-18, SH-21-12

111 West Lake Street

December 7, 2021

Applicant: Todd and Christi Ketlinski
Representative: Luke Vannoy - McCall Design and Planning
Application: Design Review, Scenic Route Review, Shoreline Review
Zoning District: R4 – Low Density Residential
Jurisdictional Area: City Limits

Description

An Application for Design, Shoreline, and Scenic Route Review for a new, single-family residence on the site of the property's existing ADU. The new 6,296 square foot home will include 4 bedrooms, 5.5 bathrooms and an attached 3-car garage. No changes to landscaping on the lake side of the property are proposed.

Staff Analysis

Project Information

Zoning District: R4 – Low Density Residential

Comprehensive Plan Designation: Low Density Residential

Project Acreage: 0.7-acres (30,492 square-feet)

Proposed Use: Single-family residence

Dimensional Standards:

	Proposed	Required
Front Setback	37-feet, 9-inches	25-feet
Rear Setback	59-feet, 6-inches	20-feet*
East Side Yard Setbacks	10-feet, 3-inches	10-feet
West Side Yard Setbacks	12-feet	10-feet
Lot Coverage	5,257 square-feet (80% of allowed)	6,606 square feet (22% of total)
Snow Storage	1,109 square-feet	Greater than 803 square-feet
Building Height	34-feet, 6-inches	35-feet

* Except where the 50-foot shoreline setback extends beyond the required rear setback.

Code Sections of Interest

- McCall City Code Section 3.2.02: Meanings of Terms or Words

BLANK WALL: A street facade that is characterized by a lack of transparency into which the pedestrian can see.

FENCE: A hedge, structure, or partition, erected for the purpose of enclosing or delineating a piece of land, or to divide a piece of land into distinct portions or ownerships.

WALL: A constructed solid barrier of concrete, boulders, bricks, tiles or similar types of material that limit visibility and the flow of air and light.

- McCall City Code Section 3.3.03: Residential Zone General Development Standards:

Table 3.3.03 states that the minimum distance between points of building elements in opposition is 10-feet in the R4 zoning district.

- McCall City Code Section 3.3.09(C): Residential Zone Design Review Standards”

Residential Design Standards:

1. Building Scale: The building shall be in scale with the site conditions and surrounding structures. Appropriate scale can be accomplished through the following:

a. The principal structure on the site is the dominant element.

b. Building massing and scale is responsive to the site conditions, including views through and of the site, and existing vegetation. Development that is designed to complement rather than dominate the natural landscape.

c. Variations in wall planes and roof lines are incorporated to minimize the apparent scale of the building.

d. Porches, balconies, decks and terraces that strengthen the residential scale and character of the building.

2. Building Design: Building designs shall enhance and/or continue the styles found in the McCall area with allowance for new interpretations of historic details. Local natural building materials, roof overhangs, covered porches and entrances, simplicity of design, and segmented windows are elements of the McCall style. Building designs that are in keeping with the McCall style include the following:

a. Materials:

i. Natural looking exterior materials looking siding and decking materials that resist heat and flames, including: composite decking, cement, plaster, stucco, masonry (such as stone brick or blocks) and fiber-cement siding, soffit, and trim products.

ii. Use of stones that convey the appearance of a structural element rather than a veneer facing.

iii. Metal when used in combination with natural materials.

b. Roofs:

i. Roofs that compliment and respond to the mountain setting and heavy snow environment.

ii. Hipped, pitched, shed and gabled roofs.

iii. Roofs designed to prevent snow or ice from shedding directly onto a pedestrian walkway, access or adjacent property.

iv. Class A fire rated roof assemblies.

c. Exterior Color:

i. Exterior wall colors that tend toward earthy warm hues, with accent colors kept to a minimum of two (2) and used to highlight entrances.

ii. No harshly contrasted color combinations, brilliant, luminescent or day-glow colors on exterior finishes.

d. No blank walls shall face street frontages, including blank or unarticulated garage doors.

e. Decks and balconies shall be designed to handle snow and drift loads including snow shedding from roof overhangs above.

- McCall City Code Section 3.7.023(B): Requirements for Development:

(B) Permit Criteria: No conditional use or building permit shall be issued, nor is any development, grading, or alteration of any land within this zone permitted, unless the applicant establishes to the satisfaction of the commission and council in the case of a conditional use, or of the administrator in the case of a building permit, that:

1. The proposed development meets all applicable requirements of this title and title IX of this code.

2. The plans accurately identify the water pool shore contours and high water marks, which, in the case of river environs, shall mean the limits of the area of special flood hazard.

3. A letter is on file from a specialist certified by the United States army corps of engineers wetlands expert that certifies that no wetlands related issues or issues related to fill of navigable waters issues were presented by the proposed development; or that a section 404 permit has been issued or is forthcoming by the corps of engineers, whichever is appropriate, city approval(s) under this title and title IX of this code are contingent upon all applicable section 404 permit requirements being met; if a permit requirement is not met, the city may revoke its approval(s) under this title and title IX of this code.

4. The requirements of the underlying zone are met.

5. The fifty foot (50') building setback line is met per subsection (C)3(c) of this section.

6. Proof of stormwater certification training has been provided by the individual applying for the building permit.

- McCall City Code Section 3.7.031: Lands Included

The scenic route zone includes:

(A) One hundred fifty foot (150') wide strips of land bounded by the right of way lines of the following named streets, and by lines parallel to and one hundred fifty feet (150') away from such right of way lines, together with any right of way not part of the roadway:

1. State Highway 55, from the southern boundary to the northern boundary of the impact area (Third Street and Lake Street).

(B) Any structure or a portion thereof which may be visible when traveling along the roadways designated above and any part of which is located within one hundred fifty feet (150') of the nearest right of way line of a road named in this section or designated pursuant to this section; such a structure is subject to design review and approval under chapter 16, "Design Review", of this title, notwithstanding that portions of the structure are not on land that is within this zone.

- McCall City Code Section 3.7.032(B): Requirements for Development:
The commission shall ascertain whether the proposed development, improvement or use will:
 1. Block or disrupt the visibility of significant views or features.
 2. Be compatible (in terms of setback, bulk, height, design, finish materials, signing and landscaping) with its immediate surroundings and the desired visual quality of the scenic route.

- McCall City Code Section 3.7.032(I): Requirements for Development:
Landscaping: In addition to the requirements set forth in section 3.8.13 “Landscaping, Screening And Buffering,” development within the scenic route zones shall provide the following landscaping:
 1. One (1) native tree including evergreen and deciduous and three (3) native shrubs each ten feet (10') of the linear street frontage along the scenic route. Not less than fifty percent (50%) shall be evergreen trees.
 2. The required landscaping shall be located with a twenty-five feet (25') setback from the scenic route. Landscaping shall be arranged within groups or clustered allowing spacing for natural maturity. Linear designed landscaping is not acceptable.
 3. If conflicts arise between this section and section 3.8.04 “Fire Mitigation Standards,” or in the use of any snow storage or utility easements, a recommended resolution will be the responsibility of the McCall Fire Protection District or the Administrator.

- McCall City Code Section 3.8.10(A)-(C): Fencing and Walls:
 - (A) Purpose: To ensure that the scale, materials and design of fences and walls blend with the site and building, that the placement of walls and fences respect and do not dominate the existing built or natural environment including the movement of wildlife.
 - (B) Fencing Standards:
 1. Fencing which encloses a property shall be primarily constructed of natural materials, such as log poles or split rails.
 2. In the CC, CBD, I and BP zones, fencing is not permitted except as a screen abutting residential properties and when approved by the Administrator to provide security for hazardous materials or operations.
 3. The use of plastic or synthetic materials in fencing shall have the appearance or mimic natural materials and colors, and be approved by the Administrator.
 4. The use of chain link fencing in any residential zone, CBD and CC zones is prohibited. In all other zones, approval by the Administrator is required.
 5. Fences located within twenty feet (20') of a property line facing a public right of way, shall not exceed four feet (4') in height.

6. No fence shall be installed on a dedicated easement in a manner that blocks or inhibits the use of such easement.

7. All other fences in all zones, except in the Industrial (I) zone shall not exceed six feet (6') in height.

(C) Walls: Walls shall be compatible in form, scale, and materials with the architectural details and materials of nearby buildings, and add, not detract, from the appearance of the site.

1. Walls may not be faced with any material disallowed for buildings and shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials, rather than constructed with a solid or flat surface.

2. Rock facing on walls shall be applied in a manner that makes the rock appear as a structural element rather than a veneer. Textured, specially formed and sand blasted concrete are suggested wall materials.

3. Walls over twenty-four inches (24") high may require railings or planting buffers for safety. Low retaining walls may be used for seating if capped with a surface of at least twelve to sixteen inches (12" to 16") wide.

4. In residential districts, walls in excess of thirty inches (30 in.) in height must adhere to the property setbacks for structures and shall be no higher than four feet (4') or terraced with a five foot (5') separation of walls.

5. Walls, where visible to the public or the primary building shall be no higher than four feet (4') or terraced with a five foot (5') horizontal separation of walls to accommodate landscaping.

6. Multiple retaining walls with terraces between shall be the minimum necessary for reasonable development of the site as determined by the Administrator.

- McCall City Code Section 3.8.19: Main Entrances in R4 Through R16 Zones:

(A) Purpose: These standards:

1. Together with the garage standards, ensure that there is a physical and visual connection between the living area of the residence and the street.

2. Enhance public safety for residents and visitors and provide opportunities for community interaction.

3. Ensure that the pedestrian entrance is visible or clearly identifiable from the street by its orientation or articulation.

(B) Applicability:

1. The standards of subsection (C) of this section apply to single-family and two-family dwellings, and townhouse units in the R4 through R16 zones.
2. Where a proposal is for an alteration or addition to existing development, the standards of this section apply only to the portion being altered or added.
3. On sites with more than one street frontage, the applicant may choose on which frontage to meet the standards.
4. Development on flag lots or on lots that slope up or down from the street with an average slope of twenty percent (20%) or more is exempt from these standards.
5. Main entrances for developments which have secured a building permit prior to March 16, 2006, are exempt from these provisions.

(C) Standards: At least one main entrance for each structure must:

1. Be within eight feet (8') of the longest street facing wall of the dwelling unit; and
2. One of the following:
 - (a) Face the street. See figure 3.8.19(A) of this section.
 - (b) Be at an angle of up to forty five degrees (45°) from the street.
 - (c) Open onto a porch. See figure 3.8.19(B) of this section. The porch must:
 - (1) Be at least twenty five (25) square feet in area.
 - (2) Have at least one entrance facing the street.
 - (3) Have a roof that is:

A. No more than twelve feet (12') above the floor of the porch.

B. At least thirty percent (30%) solid. This standard may be met by having thirty percent (30%) of the porch area covered with a solid roof, or by having the entire area covered with a trellis or other open material if no more than seventy percent (70%) of the area of the material is open.

FIGURE 3.8.19(A)

MAIN ENTRANCE FACING STREET

FIGURE 3.8.19(A)
MAIN ENTRANCE FACING STREET

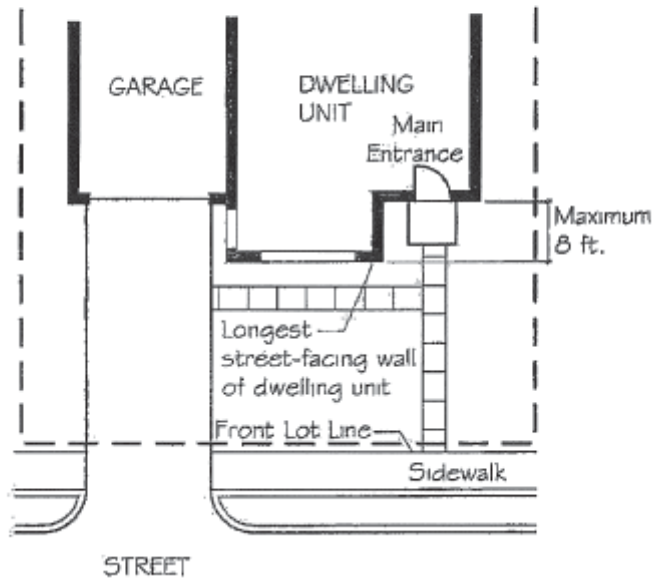
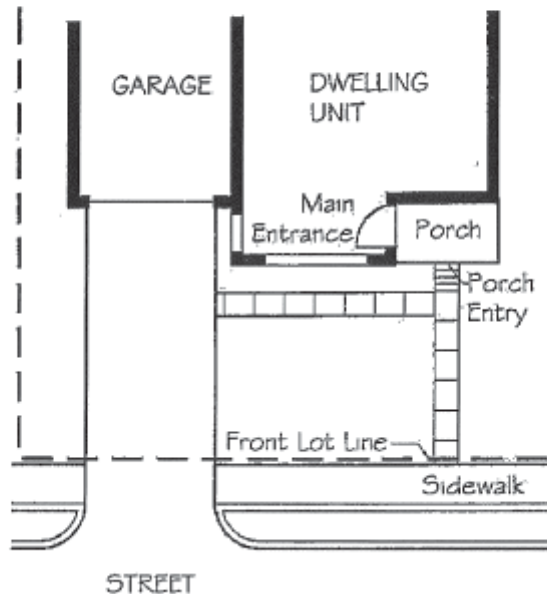


FIGURE 3.8.19(B)

MAIN ENTRANCE OPENING ONTO PORCH

FIGURE 3.8.19(B)
MAIN ENTRANCE OPENING ONTO PORCH



- McCall City Code Section 3.11.07(A): Exceptions:
Shoreline Development: If the nonconforming structure or portion thereof is located within the fifty foot (50') shoreline setback pursuant to chapter 7 of this title, then:
 1. A single one-time only addition, less than twenty five percent (25%) of the total square footage of the existing nonconforming structure is allowed so long as the addition to the structure conforms to all the requirements of the zone.
 2. Additions or enlargements to the structure that equal more than twenty five percent (25%) of the total square footage of the existing nonconforming structure shall require the entire nonconforming structure, or portion thereof which causes the structure to be nonconforming, to be removed and conform to all the requirements of the zone.
- McCall City Code Section 3.16.07: Design Review Criteria:
The commission or administrator shall determine the following before approval is given:
 - (A) The project is in general conformance with the comprehensive plan.
 - (B) The project does not jeopardize the health, safety or welfare of the public.
 - (C) The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance as enumerated in section 3.16.01 of this chapter.
 - (D) The project will have no substantial impact on adjacent properties or on the community at large.

(E) If applicable, a subdivision design review document has been reviewed and approved by the commission in lieu of the design guidelines.

(F) For projects in the shoreline and river environs zone, the project will not have an unreasonable and adverse impact on the visual quality of its setting or the water quality.

(G) For projects in the scenic route zone, the project will preserve and enhance the scenic quality of the street or highway.

Comprehensive Plan Sections of Interest

- Vision in Motion – Our Vision:

McCall is a diverse, small town united to maintain a safe, clean, healthy, and attractive environment. It is a friendly, progressive community that is affordable and sustainable.

- Deep Dive – Future Land Use Designations – Low Density Residential:

This land use designation is intended to provide for the development of low-density single-family residential neighborhoods. These neighborhoods consist of larger home sites.

Design Guidelines of Interest

- McCall Design Guidelines Section 4.1(C): Blank Walls:

A blank wall is a street facade that is characterized by a lack of transparency into which the pedestrian can see. A blank wall:

1. Does not have glass on a high percentage of the facade, OR
2. Does not have glass that is transparent, OR
3. Does not have glass that is maintained (spaced) across the entire facade, OR
4. Does not have glass that is placed at pedestrian eye-level.

Characteristics

- Garage doors are included as blank walls.
- Fences are included as blank walls.
- Glass display cases and display windows less than three feet deep are included as blank walls when they do not allow the pedestrian to view the interior of a street facade.
- Tinting of a percentage too high to allow transparency constitutes a blank wall.
- Regardless of architectural details, landscaping, or signage; a blank wall remains a blank wall.

Guideline

Avoid blank walls on all street facades. A street facade is defined here as any wall abutting a dedicated public street.

- McCall Design Guideline 29: Fences and Walls:

The design of fences and walls should harmonize with the site and the buildings on it in scale as well as in materials. Walls and fencing may be required elements in a site design for privacy, property line delineations, or screening. Low walls for seating are also encouraged as an amenity in pedestrian areas; these should be capped with a surface at least 12 - 16 inches wide. Low walls with seating, detailing and landscaping are especially appropriate as an edge to off-street

parking areas. The placement of walls and fences should respect existing land forms and follow existing contours and fit into existing land massing rather than arbitrarily following site boundary lines. Fencing should not dominate the buildings of the landscape. Planting may often be integrated with fencing scheme in order to soften the visual impact. The tops of fences should generally be maintained horizontal. If the ground slopes the fence should be stepped. Fences intended to provide private areas should be kept close to building so as not to adversely impact common open area. Fencing which is away from buildings should be of a more open character than fences intended to provide privacy close to houses. Fencing materials should be compatible with the materials and color of the surrounding or the prevailing building materials and color in adjacent developments. The use of natural materials is preferred, as opposed to chain link or plastic or other synthetic materials.

Staff Discussion

- The applicant is proposing to construct a six-foot (6') tall stone veneer wall between West Lake Street and the front of the house. McCall City Code Section 3.3.09(C)(2)(d) along with the McCall Design Guidelines, prohibit blank walls adjacent to street frontages. Prior to the issuance of a building permit, the applicant should provide a revised site plan with no fencing or walls between the proposed residence and West Lake Street.
- A lawn and patio area exist within the fifty-foot (50') shoreline setback area. As no modifications are being proposed to the landscaping between the proposed residence and Payette Lake and no structures are being modified within the shoreline setback area, the existing nonconforming lawn and patio may remain. Any future modifications to the landscaping within the shoreline setback area should require the removal of the lawn and patio.
- The landscape plan, date stamped by the City on October 26, 2021, proposes the following plantings within the area between West Lake Street and the proposed wall:
 - Two (2) existing ponderosa pine trees
 - Two (2) flame maple trees
 - Two (2) quaking aspen trees
 - Three (3) spruce trees
 - Three (3) feather reed grass bushes
 - Five (5) dwarf burning bushes
 - Six (6) native rose bushes

McCall City Code Section 3.7.032(I)(1) requires one (1) native tree and three (3) native shrubs per ten-feet (10') of street frontage. The subject property has one-hundred feet (100') of street frontage, which would require ten (10) native trees and thirty (30) native shrubs. McCall City Code Section 3.7.032(I)(2) requires the landscaping to be located within twenty-five feet (25') of the front property line. However, it is staff's opinion that the proposed location of the landscaping will facilitate better snow removal along West Lake Street, enhance the prominence of the existing mature ponderosa pine trees, and is an acceptable location. Prior to the issuance of a building permit, the applicant should be required to provide a revised landscaping plan showing a minimum of ten (10) native trees and thirty (30) native shrubs located between West Lake Street and the front lawn area. A minimum of five (5) of the trees should be evergreen trees.

Comments

Agency –

McCall Public Works

In an email dated November 19, 2021, the City of McCall Staff Engineer had the following comments:

The Public Works Department has reviewed the documents submitted for review for **DR21-35** and have the following comments and concerns.

1. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Please explain the thought process behind the detention pond and its location. It seems that most of the runoff from the property would accumulate north of where the detention pond is located.
 - b. Some variation of infiltration device should be used north of the proposed home to infiltrate stormwater runoff prior to it entering Payette Lake.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan and appropriate hydrographs and calculations.
2. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
3. The landscaping plan and stormwater management plan do not seem to coincide regarding the side yard swales that are proposed and large trees that are being proposed.
4. Please show the water line location on your plans and identify if any changes to the service size will occur.
5. Walls and structures over 4 feet in height are required to be designed by a licensed professional engineer.
6. An access permit from ITD will be needed for alterations to the access point onto Lake Street prior to any construction commencing.

Comments 1 through 5, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance.

City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I would like to request that the landscape plan incorporate a few new plantings between the new structure and the lake to soften and breakup the building facade from the lake looking back to shore. A reduction in existing grass or at the very least a buffering strip of plantings to reduce fertilizer runoff to the lake are also recommended.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

See attached letter dated November 16, 2021.

Big Payette Lake Water Quality Council (BPLWQC)

In an email dated November 4, 2021, the BPLWQC had the following comments:

The home appears to have a very gentle slope down to Payette Lake which is good but the grass between the home and lake (unless there is zero plan to fertilize this lawn) is harmful to lake water quality from any runoff of rain, snow or storm event. The hardscape path leading from the house to the existing patio does not lend itself to the absorption of stormwater runoff. Native plants and grasses are great and should be used as one of the best erosion breaks toward the lake. Please follow the Lake-A-Syst packets when preparing the site for construction. All measures need to be taken to protect erosion into Payette Lake during the entire construction process.

Public –

No public comment has been received.

Staff Recommendation

Staff recommends approval of this application with the Conditions of Approval listed below.

Conditions of Approval

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. Prior to the issuance of a building permit, the applicant shall provide a revised site plan with no fencing or walls between the proposed residence and West Lake Street.
3. The existing nonconforming lawn and patio may remain. Any future modifications to the landscaping within the shoreline setback area shall require the removal of the lawn and patio.
4. Prior to the issuance of a building permit, the applicant shall provide a revised landscape plan showing a minimum of ten (10) native trees and thirty (30) native shrubs located between West Lake Street and the front lawn area. A minimum of five (5) of the trees shall be evergreen trees.
5. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

IN RE:)
)
KETLINSKI RESIDENCE)
Design Review, Shoreline)
Environs Overlay Review, and)
Scenic Route Overlay Review)
)
Application Number:)
DR-21-36, SR-21-18, SH-21-12)

**McCALL AREA PLANNING AND ZONING COMMISSION
FINDINGS OF FACT, CONCLUSIONS OF LAW, AND
DECISION**

FINDINGS OF FACTS

Applicant: Todd and Christi Ketlinski

Representative: Luke Vannoy - McCall Design and Planning

Application: An Application for Design, Shoreline, and Scenic Route Review for a new, single-family residence on the site of the property’s existing ADU. The new 6,296 square foot home will include 4 bedrooms, 5.5 bathrooms and an attached 3-car garage. No changes to landscaping on the lake side of the property are proposed.

Address: 111 West Lake Street

Location: Lot 1 of the South Shore Subdivision, situated in the NE ¼ of the NE ¼ of the W ½ of Section 8, T18N, R3E, B.M. City of McCall, Valley County, Idaho.

Public Notice: Newspaper: The Notice of Hearing was published in the *Star News* on November 18, 2021.
Mailing: The Notice of Hearing was mailed by the applicant to property owners within 300 feet on November 18, 2021.
Posting: The Notice of Hearing was posted by the applicant on the subject property on November 18, 2021.

Zoning: R4 – Low Density Residential

Property Size: 0.7-acres (30,492 square-feet)

Lot Coverage: 5,257 square-feet (80% of allowed)

Building Height: The maximum height of the proposed residence is 34.5-feet.

Setbacks:

	Proposed	Required
Front Setback	37-feet, 9-inches	25-feet
Rear Setback	59-feet, 6-inches	20-feet*
East Side Yard Setbacks	10-feet, 3-inches	10-feet
West Side Yard Setbacks	12-feet	10-feet

Parking Spaces:

Provided: 2+ spaces

Required: 2 spaces per MCC 3.8.062

APPROVAL STANDARDS

MCC 3.7.02 Shoreline and River Environs Zone

No conditional use or building permit shall be issued, nor is any development, grading, or alteration of any land within this zone permitted, unless the applicant establishes to the satisfaction of the commission and council in the case of a conditional use, or of the administrator in the case of a building permit, that:

1. **The proposed development meets all applicable requirements of this title and title IX of this code.** The proposed site plan and structure meet the applicable requirements of MCC Title 3, Chapter 7, Chapter 8, and Title 9.
2. **The plans accurately identify the water pool shore contours and high water marks, which, in the case of river environs, shall mean the limits of the area of special flood hazard. The site**

plan indicates the Water Pool Shore Contour elevation. The water pool contour line is indicated on the site plan.

3. **A letter is on file from a specialist certified by the United States army corps of engineers wetlands expert that certifies that no wetlands related issues or issues related to fill of navigable waters issues were presented by the proposed development; or that a section 404 permit has been issued or is forthcoming by the corps of engineers, whichever is appropriate, city approval(s) under this title and title IX of this code are contingent upon all applicable section 404 permit requirements being met.** It appears that a 404 permit will not be necessary although the City reserves the right to require this at a later date if it becomes necessary.
4. **The requirements of the underlying zone are met.** The proposed residence meet the requirements of the Shoreline Zone and R4-Low Density Residential Zone.
5. **The fifty-foot (50') building setback line is met per subsection (C)3(c) of this section.** The fifty foot (50') building setback for properties within the Shoreline and River Environs Zone is indicated on the submitted site plan. The proposed residence is located outside of this setback.
6. **Proof of stormwater certification training has been provided by the individual applying for the building permit.** Proof of stormwater certification is required prior to issuance of a building permit.
7. **The Commission determined whether the proposed development, improvement or use:**
 - a. **Dominates the vistas of the water to the extent that it has an unreasonable adverse impact on the visual quality of its setting.** The proposed residence is large, but is consistent with other residences in the neighborhood.
 - b. **Is compatible in form, line, color and texture with its surroundings.** The proposed residence is compatible in form, line, color and texture because it utilizes natural colors and materials and is designed similarly to other residences in the neighborhood.

- c. **Significantly differs in scale or contrast from its surroundings to the extent that it has an unreasonable adverse impact on the visual quality of its setting.** The proposed residence meets the dimensional standards of the zone and is designed to minimize the difference between the site’s topography and the built form. Therefore, the proposed residence does not differ in scale or contrast from its surroundings.
- d. **Creates a wall of structures as viewed from the water.** The proposed residence will be set back approximately 60-feet from the lake. The proposed residence is largest when viewed from the lake and there is minimal landscaping between the lake and the residence. Therefore, the proposed residence may create the appearance of a wall of structures from the water.

MCC 3.7.032 Scenic Route Requirements for Development

Scenic Route Zone

The Commission shall determine whether the proposed development, improvement, or use:

1. **The project preserves and enhances the scenic quality of the street or highway.**

The proposal will maintain the scenic quality of West Lake Street through the native vegetation that will be planted along the scenic route and quality architectural design of the proposed residence.

2. **Is compatible (in terms of setback, bulk, height, design, finish materials, and signing with its immediate surroundings and the desired visual quality of the scenic route.**

The proposed site coverage of 5,257 square-feet (80% of allowed) on a 30,492 square-foot site is in scale with the surrounding area. Compatibility with the desired visual quality of the scenic route is provided by: (1) building setbacks that are more than the minimum requirements; (2) the proposed landscaping that will buffer views of the new structures; and (3) the compatible exterior building design, materials, and colors.

3. **Building Clustering** N/A
4. **Landscaping:** The applicant is required to provide landscaping adjacent to West Lake Street in conformance with McCall City Code Section 3.7.032(I).

MCC 3.16: Design Review

The commission has determined that:

1. The project is in general conformance with the comprehensive plan.
2. The project does not jeopardize the health, safety or welfare of the public.
3. The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance, adopted by the city of McCall.
4. The project will have no substantial impact on adjacent properties or the community at large.

Please see review of the Design Guidelines below.

MCC 3.3.09: DESIGN Requirements

General Objectives for Residential Design

1. **Support a residential character and is compatible with its surrounding neighborhood:** The structure is residential in character and is compatible with the surrounding neighborhood.
2. **Preserves natural features:** The proposed residence is approximately 60-feet from the lake, and therefore will preserve the natural features within the 50 ft. shoreline setback.
3. **Promotes active and safe streetscapes:** The applicant is required to remove the proposed wall shown on the submitted plans, improve the landscaping adjacent to West Lake Street, and include an entrance in the street facing façade. These modifications will promote an active and safe streetscape.

Residential Review Standards

1. **Building Scale:** The proposed residence is similar in design and scale to other homes along the lake.

2. Building Design

- a. **Materials:** The proposed residence is to use wood, stone veneers, and other materials with a generally natural appearance.
- b. **Roof:** The proposed residence will utilize a low pitched metal roof.
- c. **Exterior Color:** Exterior colors are of natural hues.
- d. **Blank Walls:** Blank walls are avoided by including glass on all facades and removing the proposed wall from the front yard area.
- e. **Snow Loads:** Snow loads will be verified as part of the building permit process.

MCC 3.8 General Development Standards

1. **Tree Removal:** No significant trees are proposed to be removed.
2. **Fire Hazard Mitigation:** The proposed landscaping is consistent with the fire hazard mitigation standards of McCall City Code Section 3.8.04.
3. **Off Street Parking:** Adequate parking has been provided for the residential use.
4. **Bicycle Parking:** N/A
5. **Driveways:** The property will be accessed from West Lake Street. A driveway permit from the Idaho Transportation Department will be required.
6. **Fencing And Walls:** N/A
7. **Accessory Use, Buildings And Structures:** N/A
8. **Accessory Dwelling Units:** N/A
9. **Corner Vision:** N/A
10. **Landscaping And Buffering:** The proposed landscaping meets the requirements of McCall City Code, subject to the required modifications conditioned herein.
11. **Snow Storage And Drainage:** Adequate snow storage has been identified on site. Final engineering approval of the grading and drainage plan will be required prior to issuance of a building permit.

12. **Main Entrances In R4 Through R16 Zones:** The applicant is required to modify their plans to include an entrance within eight-feet (8') of the longest street-facing façade.

13. **Special Standards For Garages:** Due to the size of the lot, the residence is exempt from garage design standards.

14. **Local Housing Density Bonus Program:** N/A

15. **Seasonal Dwelling Units:** N/A

16. **Site Design**

- a. Preserve natural resources – The plans submitted preserve natural resources.
- b. Reduce land and water modifications – a grading and drainage plan will be required to receive final approval by the City Engineer prior to issuance of a building permit. The plan shall demonstrate that the project can meet the City's drainage management guidelines.
- c. Location to preserve prominent skylines - the location will not impact any significant views of skylines
- d. Underground utilities – all utilities will be installed underground

Architecture

1. **Enhance McCall Classic Styles:** The proposed addition is compatible with classic McCall styles.
2. **Minimize Scale:** The building design uses architectural details to minimize its apparent scale.
3. **Building Additions:** N/A
4. **Roof Lines:** The roof line low-pitched roof helps to minimize the scale.
5. **Mechanical Equipment:** No exterior mechanical equipment is proposed.
6. **Multi-Unit Structures:** N/A
7. **Balconies and Porches:** The proposed residence includes balconies and porches.
8. **Exterior Doorways:** The applicant is required to modify their plans to include an entrance within eight-feet (8') of the longest street-facing façade.
9. **Wall Materials:** Exterior wall materials include wood, metal, and stone veneers.
10. **Shop Front Design:** N/A
11. **Wall Colors:** Proposed wall colors are of natural hues.

Landscaping and Site Design

1. **Light Fixtures:** Light fixtures are in compliance with McCall’s Outdoor Lighting Ordinance.
2. **Fences and Walls:** N/A
3. **Retaining Walls:** N/A
4. **Paving and Streetscapes:** The proposed driveway is in conformance with McCall City Code
5. **Landscaping Plan:** The proposed landscaping is in conformance with McCall City Code.
6. **Site Conditions for Landscaping:** N/A
7. **Lawn Area:** All areas disturbed with construction shall be re-seeded with native shrubs or grasses.
8. **Plants as Screening:** The site has several existing trees and additional plantings are proposed to screen the site when viewed from West Lake Street.
9. **Utility Installations:** All new utilities are required to be undergrounded.
10. **Snow Storage:** Adequate snow storage has been identified.
11. **Screen Parking Lots:** N/A
12. **Irrigation System Required:** N/A
13. **Retain Existing Vegetation:** Existing vegetation will be retained as much as possible, all disturbed areas will be reseeded with native seed prior to issuance of a certificate of occupancy.
14. **Preserve Existing Trees:** The applicant is not proposing to remove any significant trees.
15. **Grading and Drainage:** Final approval of a grading and drainage plan will be required prior to issuance of a building permit.
16. **Maintenance:** Landscaping maintenance is the responsibility of the property owner.
17. **Sidewalks:** N/A
18. **Bike Paths:** N/A

Residential Districts

1. **Preserve historic residences:** No historic residences are impacted by this application.
2. **Preserve human scale in residential character:** The proposed residence steps down with the topography of the site to minimize building massing and is adequately landscaped.
3. **Preserve compatibility with surrounding neighborhoods:** The structure is compatible with the surrounding neighborhood.
4. **Preserve natural features of the immediate landscape and environment:** The proposed residence utilizes natural colors and materials, and is required to be adequately landscaped.
5. **Provide for community, or affordable, housing as needed:** The project does not provide for community or affordable housing; however, it is not required by code. The applicant is proposing to donate the existing residential structure to the city to provide community housing elsewhere in the city.
6. **Provide open spaces to enhance and maintain the rural character:** The proposed residence does not maximize the allowable lot coverage, and therefore provides more open space than is required.
7. **Provide living and moving space for native animals:** The proposed residence does not maximize the allowable lot coverage, and therefore provides more living and moving space for animals than is required.
8. **Promote active and safe streetscapes in residential neighborhoods that are conducive to walking and biking:** The applicant is required to remove the proposed wall shown on the submitted plans, improve the landscaping adjacent to West Lake Street, and include an entrance in the street facing façade. These modifications will promote an active and safe streetscape.

DEPARTMENT/AGENCY COMMENTS

Agency –

McCall Public Works

In an email dated November 19, 2021, the City of McCall Staff Engineer had the following comments:

The Public Works Department has reviewed the documents submitted for review for **DR21-35** and have the following comments and concerns.

1. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Please explain the thought process behind the detention pond and its location. It seems that most of the runoff from the property would accumulate north of where the detention pond is located.
 - b. Some variation of infiltration device should be used north of the proposed home to infiltrate stormwater runoff prior to it entering Payette Lake.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan and appropriate hydrographs and calculations.
2. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
3. The landscaping plan and stormwater management plan do not seem to coincide regarding the side yard swales that are proposed and large trees that are being proposed.
4. Please show the water line location on your plans and identify if any changes to the service size will occur.
5. Walls and structures over 4 feet in height are required to be designed by a licensed professional engineer.

6. An access permit from ITD will be needed for alterations to the access point onto Lake Street prior to any construction commencing.

Comments 1 through 5, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance.

City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I would like to request that the landscape plan incorporate a few new plantings between the new structure and the lake to soften and breakup the building facade from the lake looking back to shore. A reduction in existing grass or at the very least a buffering strip of plantings to reduce fertilizer runoff to the lake are also recommended.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

See attached letter dated November 16, 2021.

Big Payette Lake Water Quality Council (BPLWQC)

In an email dated November 4, 2021, the BPLWQC had the following comments:

The home appears to have a very gentle slope down to Payette Lake which is good but the grass between the home and lake (unless there is zero plan to fertilize this lawn) is harmful to lake water quality from any runoff of rain, snow or storm event. The hardscape path leading from the house to the existing patio does not lend itself to the absorption of stormwater runoff. Native plants and grasses are great and should be used as one of the best erosion breaks toward the lake. Please follow the Lake-A-Syst packets when preparing the site for construction. All measures need to be taken to protect erosion into Payette Lake during the entire construction process.

Public –

No public comment has been received.

CONCLUSIONS OF LAW

1. The City of McCall has provided for the processing of Design Review applications, pursuant to Title 3, Chapter 16 of McCall City Code.
2. Adequate notice of the December 7, 2021 public hearing was provided, pursuant to Section 67-6512, Idaho Code and Title 3, Chapter 15 of McCall City Code.
3. Upon compliance with the conditions noted below, the application meets the Design Review Standards set forth in Title 3, Chapter 16 of McCall City Code.

DECISION

THEREFORE, the McCall Area Planning and Zoning Commission hereby **approves** this Design Review application, provided that the following conditions are met:

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. Prior to the issuance of a building permit, the applicant shall provide a revised site plan with no fencing or walls between the proposed residence and West Lake Street.
3. The existing nonconforming lawn and patio may remain. Any future modifications to the landscaping within the shoreline setback area shall require the removal of the lawn and patio.
4. Prior to the issuance of a building permit, the applicant shall be required to provide a revised landscaping plan showing a minimum of ten (10) native trees and thirty (30) native shrubs located between West Lake Street and the front lawn area. A minimum of five (5) of the trees shall be evergreen trees.

5. Prior to the issuance of a building permit, the applicant shall provide a revised set of plans with a main entrance located within eight-feet (8') of the longest street facing wall of the proposed residence.
6. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

Findings of Fact **adopted** this 7th day of DECEMBER, 2021.

Robert Lyons, Chair
McCall Area Planning and Zoning Commission

Attest:

Brian Parker, City Planner
City of McCall

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received: _____

Fees Paid: _____

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # \$475 Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # \$300 Scenic Route (SR) - \$300
- # \$300 Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: Todd and Christi Ketlinski Email: _____

Mailing Address: _____

Property Owner 2 (If Applicable): _____ Email: _____

Mailing Address: _____ Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: _____ Email: luke@mccalldp.com

Mailing Address: PO Box 729, McCall, ID 83638 Phone: (208) 634-5707

PROPERTY INFORMATION

Address(es) of Property: 111 W Lake Street, McCall, ID 83638

Legal Description of Property: South Shore Subdivision Lot 1

Zoning District of Property: R4 Project Sq. Footage (If Applicable): 6,296 sf

Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: *(please attach supplemental information if needed)*

SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

The submittal items identified in the application info sheets are the minimum required materials for the City to accept applications. The City Planner may require additional information based on the specific circumstances of each proposal.

Property Owner 1

Signature

Property Owner 2 *(If Applicable)*

Signature

Signature

FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

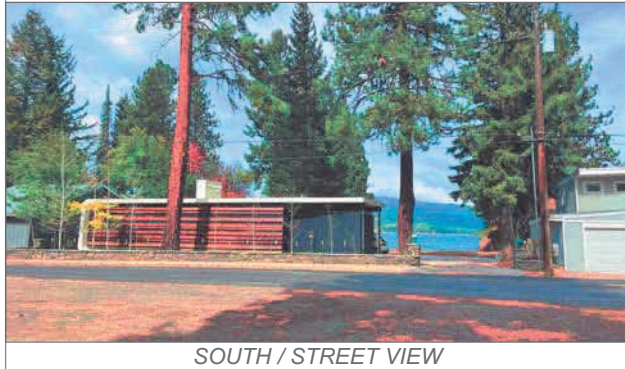
I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.



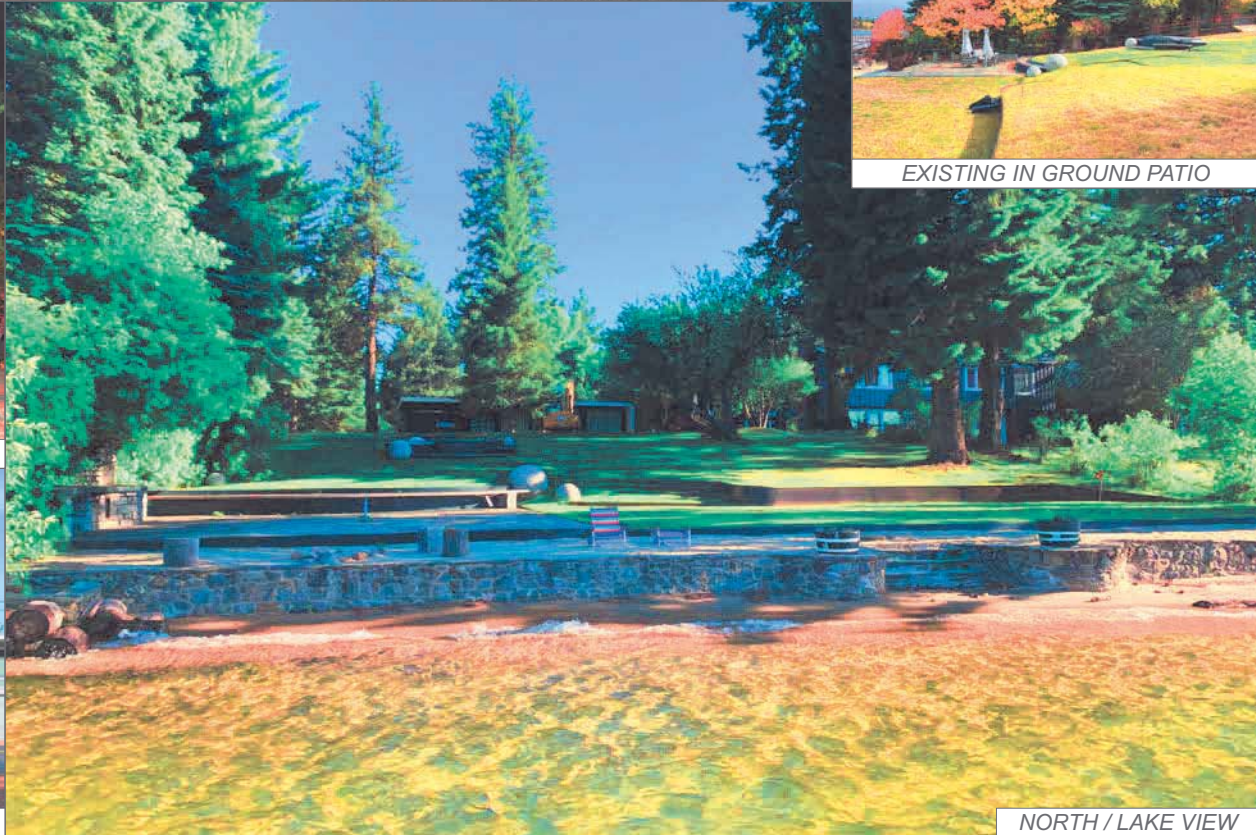
WEST VIEW



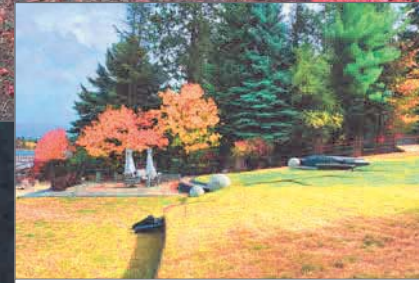
SOUTHEAST VIEW




SOUTH / STREET VIEW



NORTH / LAKE VIEW



EXISTING IN GROUND PATIO



 mb
 ARCHITECTURE
 INTERIOR AND CLIMATE SPECIFIC

© 2021 MDPi
 SUSEMIHL - VANNOY

McCALL DESIGN & PLANNING®
 PO Box 729
 McCALL, IDAHO 83638
 208-634-5707

DATE: 11/29/21

DRAWN BY: LAV, HAS


 PRELIMINARY-
 NOT FOR
 CONSTRUCTION

KETLINSKI
 111 LAKE STREET
 McCALL, ID 83638

DR2



Drive Entry
NOT TO SCALE



Lake View
NOT TO SCALE



Home Entry
NOT TO SCALE



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PO Box 729

MCCALL, IDAHO 83638

208-634-5707

SUSEMIHL - VANNOY

DATE: 11/29/21

DRAWN BY: LAV, HAS

PRELIMINARY - NOT FOR CONSTRUCTION

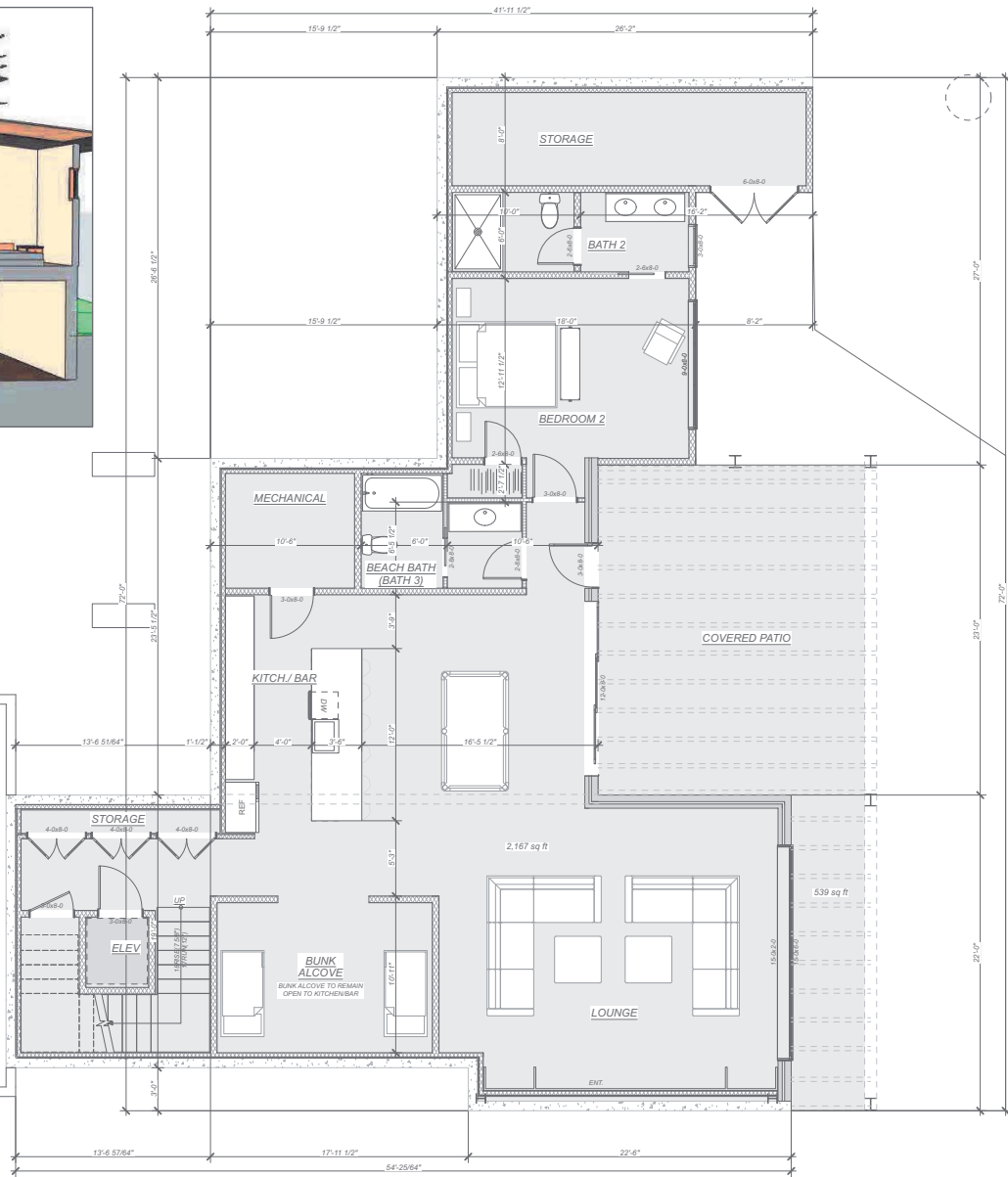
KETLINSKI
111 LAKE STREET
MCCALL, ID 83638

L2


© 2021 MDP/PC ARCHITECTURE



SECTION FROM LAKE VIEW LOOKING SOUTH
NOT TO SCALE



LOWER FLOOR PLAN
SCALE: 1/4" = 1'-0"


 MBE AND CLIMATE SENSITIVE ARCHITECTURE
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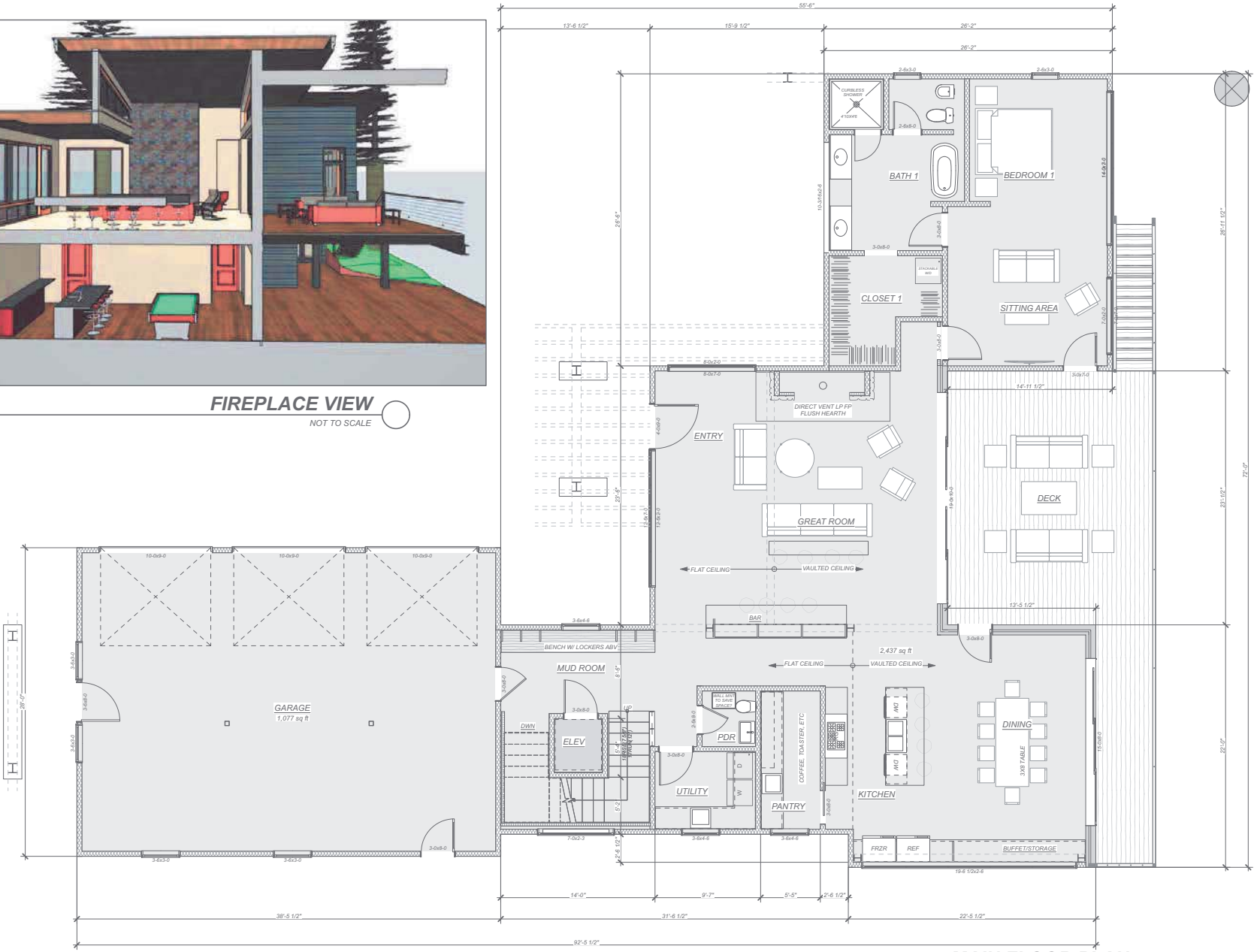
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 PRELIMINARY - NOT FOR CONSTRUCTION


KETLINSKI
 111 LAKE STREET
 MCCALL, ID 83638

DR3



FIREPLACE VIEW
NOT TO SCALE




 SITE AND CLIMATE SPECIFIC ARCHITECTURE
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DATE: 11/29/21
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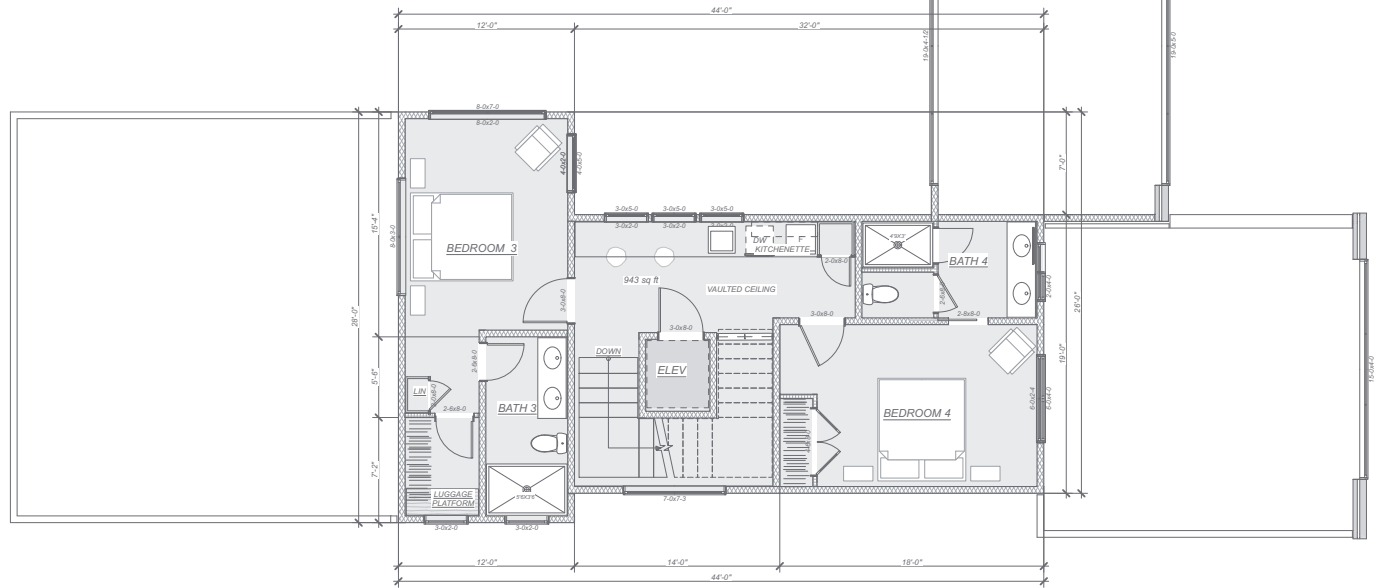
KETLINSKI
 111 LAKE STREET
 McCALL, ID 83638

DR4



VIEW NE FROM ENTRY

NOT TO SCALE



UPPER FLOOR PLAN

SCALE: 1/4" = 1'-0"





NE VIEW
NOT TO SCALE



NW VIEW
NOT TO SCALE



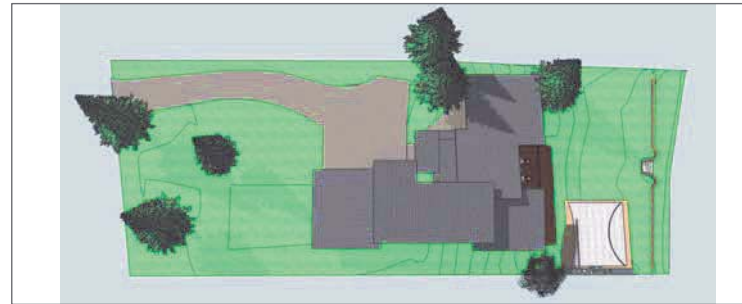
SW VIEW
NOT TO SCALE



LAKE VIEW
NOT TO SCALE



NE HIGH VIEW
NOT TO SCALE



AERIAL
NOT TO SCALE



BRIDGER STEEL BOX RIB 7.2 STRUCTURAL SIDING
COLOR DARK GRAY OR SIM.



KOLBE VISTALUXE AL LINE WINDOWS
COLOR "ONYX"



SEE SYMBOL ON
SITE PLAN ON DR1
FOR EXTERIOR
LIGHTING LOCATIONS

WEST ELM METAL CYLINDER LED SCONCE
OR SIM. COLOR "BLACK" @ ALL EXTERIOR
LIGHTING LOCATIONS



PIONEER MILLWORKS FIR TOASTED 2
SHIPLAP SIDING



STEPPED STEEL FASCIA
COLOR BLACK OR SIM.



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ARCHITECTURE
MCCALL DESIGN & PLANNING®

PO Box 729
MCCALL, IDAHO 83638
208-634-5707

DATE: 11/29/21

DRAWN BY: LAV, HAS



PRELIMINARY-
NOT FOR
CONSTRUCTION

KETLINSKI
111 LAKE STREET
MCCALL, ID 83638

DR7

RECEIVED

By Meredith Todd at 1:39 pm, Oct 26, 2021

STORMWATER APPLICATION
City of McCall

Fill in all information. Submit one copy of signed application and three copies of Stormwater Management Plan/Report to the City Engineer.

1. Project Name: Ketlinski Residence
Location: 111 W. Lake Street McCall, ID 83638

2. Owner's Name: Todd Ketlinski
Street: 1714 W. Clearvue Street City: Eagle
State: ID Zip Code: 83616 Phone: (208) 863-2543

3. Project Description: The proposed project consists of the demolition of the existing residence, existing cabin, concrete driveway, stone walk/patio around the existing residence, and stone wall near the entrance. The existing hot tub will be removed/ relocated and the existing patio near the lake will remain in place. A new stone wall will be constructed behind the 25' frontage setback line and the new asphalt driveway will be constructed to access the proposed 4,132 S.F. two-story residence and attached garage.

a. Total property area, in acres. 0.59 Acres
b. Proposed impervious surface (asphalt, rooftop, concrete, sidewalk, etc.) in square feet. There will be approximately 0.18 acres (8,612.2 S.F.) of impervious surface area at build out. There is currently 0.24 acres (10,666.1 S.F.) of impervious surface on-site.

c. Describe existing vegetation present on site. The existing vegetation on-site is a mix of small to large diameter coniferous trees with a few deciduous trees/shrubs in landscaped areas and grassy open space leading down to the shoreline.

d. Start date of construction. Spring/Summer 2022
e. Estimated length of time to complete improvements. 6-12 Months

4. Stormwater Management Plan/Report attached? Yes No

5. Circle the section of the Stormwater Management Plan/Report Checklist which are applicable to project.
A B C D E F

6. Party responsible for operation and maintenance of project, including maintenance of temporary and permanent Best Management Practices:

Michael Falash Contractor [Signature] 10.26.2021
Name Title Signature Date
308 N. 15th Street Boise, ID 83702 (208) 288-2178 (208) 631-8460
Address Daytime Phone After Hours Phone

Do not write below this line.

This Stormwater Management Plan/Report is:

Approved: _____

Not Approved: _____

Approved, with conditions: _____

By The City of McCall

Representative Title Signature Date



CRESTLINE ENGINEERS, INC.
 CIVIL ENGINEERING CONSULTANTS
 323 DEINHARD LANE, SUITE C
 PO BOX 2330
 McCALL, IDAHO 83638
 208.634.4140 · 208.634-4146 FAX

PROJECT: Ketlinski Residence

CLIENT: McCall Design & Planning

JOB NO.: 21056 **DATE:** October 22, 2021

BY: RFP

REVISION DATE: _____

RE: Ketlinski Residence - Stormwater Calculations

Drainage Area Calculations

Drainage Areas	(ft²)	(Acres)
Total Property Area/Boundary	25,588.3	0.59
Development Area	25,588.3	0.59

Pre-Development: Development Area Surfaces	(ft²)	(Acres)	(%)
Building Roofs (CN = 98)	6,527.2	0.15	25.51%
Concrete Driveway (CN = 98)	1,700.0	0.04	6.64%
Concrete Walk (CN = 98)	460.7	0.01	1.80%
Grouted Stone Patio/Walks/Fence (CN = 98)	1,978.2	0.05	7.73%
Open Space (CN = 79)	14,922.2	0.34	58.32%
Hot Tub (CN = N/A)	164.7	0.00	0.64%
	25,588.3	0.59	100.00%

Total Impervious Surface Area = 10,666.1 0.24 41.68%

Post Development: Development Area Surfaces (At Build-out)	(ft²)	(Acres)	(%)
Building Roofs (CN = 98)	4,889.3	0.11	19.11%
Asphalt Driveway (CN = 98)	3,111.8	0.07	12.16%
Grouted Stone Patio/Walks/Fence (CN = 98)	559.8	0.01	2.19%
Steel Stairs (CN = 98)	51.3	0.00	0.20%
Wood Decks (CN = 89)	314.1	0.01	1.23%
Open Space (CN = 79)	16,662.0	0.38	65.12%
	25,588.3	0.59	100.00%

Total Impervious Surface Area = 8,612.2 0.20 33.66%

Drainage Area Flow Paths

	Length	Elevation Change	Slope
Pre-Development Flow Path:	(ft)	(ft)	(%)
1. Sheet Flow (n = 0.24, Grass - Dense Grasses)	101.2	13.3	13.09%

Total Length/Average Slope = 101.2 13.3 13.09%

	Length	Elevation Change	Slope
Post Development Flow Path:	(ft)	(ft)	(%)
1. Sheet Flow (n = 0.24, Grass - Dense Grasses)	5.3	0.1	2.01%
2. Channel Flow (n = 0.24, Grass - Dense Grass)	86.9	0.4	0.51%

Total Length/Average Slope = 92.2 0.5 0.50%



PAYETTE
LAKE

PROJECT
LOCATION



NORTH
SCALE: 1" = 2000'



323 DEINHARD LANE, SUITE C · PO BOX 2330
McCALL, IDAHO 83638
208.634.4140 · 208.634.4146 FAX

KETLINSKI RESIDENCE
VICINITY MAP

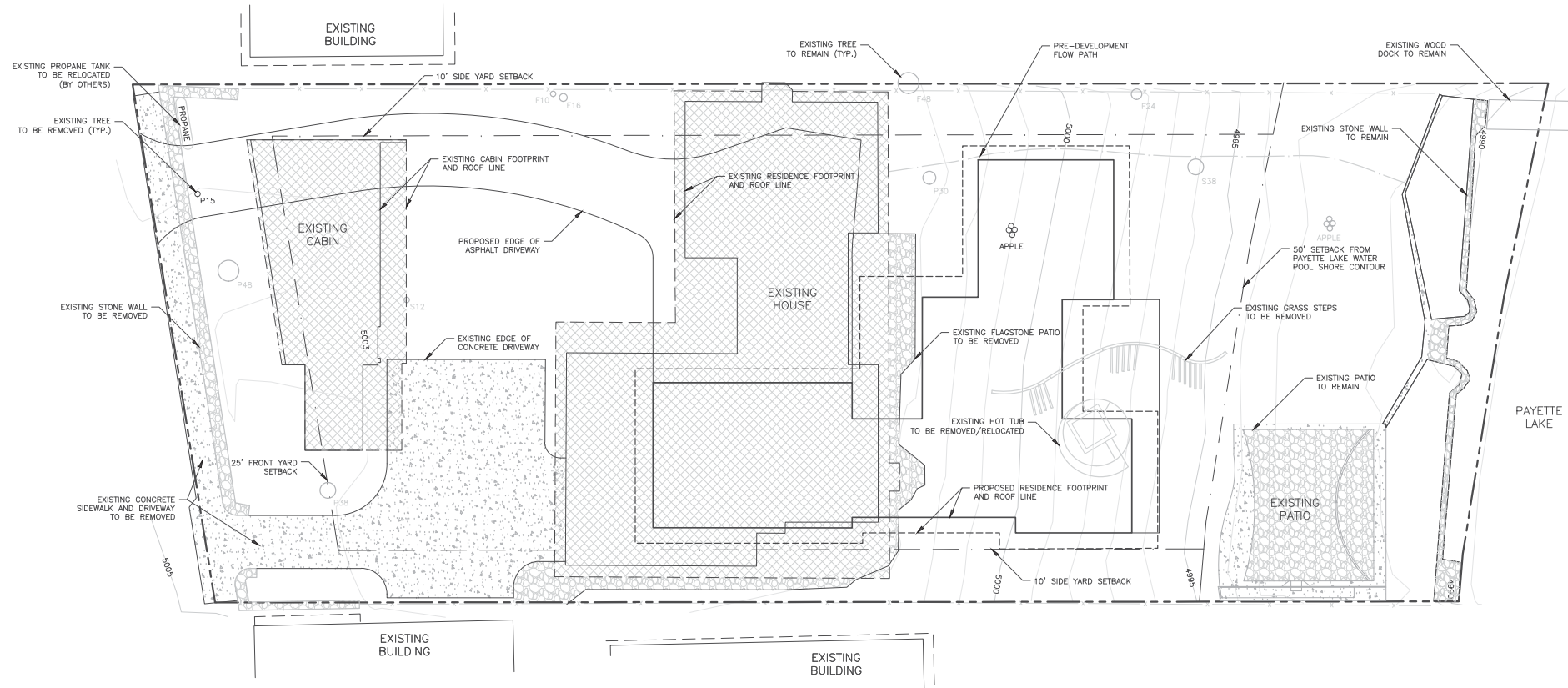
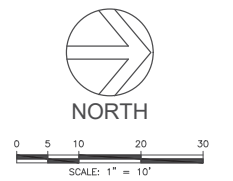
PROJECT	21056	DRAWN	FIGURE NO.
DATE	10/22/2021	RFP	1 OF 1

NOTES:

- EXISTING TOPOGRAPHY AND PROPERTY BOUNDARIES AS SHOWN ON THIS PLAN ARE BASED UPON SURVEY DATA PROVIDED BY DROULARD LAND SURVEYING, INC. PROPOSED IMPROVEMENTS AS SHOWN ON THE PLAN ARE BASED UPON DRAWINGS PROVIDED BY McCALL DESIGN & PLANNING, INC.
- THE EXISTING SITE INFORMATION IS PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR'S CONSTRUCTION SURVEY PRIOR TO THE START OF ANY PROJECT CONSTRUCTION. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF THE EXISTENCE OF OBJECTS OR UTILITIES ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THESE DRAWINGS.

LEGEND:

- PROPERTY BOUNDARY
- ▨ EXISTING CONTOUR
- ▨ EXISTING ROOF AREA TO BE REMOVED
- ▨ EXISTING CONCRETE DRIVEWAY TO BE REMOVED
- F24 EXISTING TREE W/DIA. IN INCHES TO REMAIN
- D24 EXISTING TREE W/DIA. IN INCHES TO BE REMOVED



NO.	REVISION	BY	DATE	DESIGN
				GTT/RFP
				DRAWN
				RFP
				CHECKED
				GTT
				APPROVED
				RFP

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

CRESTLINE
ENGINEERS
323 DEINHARD LANE, SUITE C · PO BOX 2330
McCALL, IDAHO 83638
208.634.4140 · 208.634.4146 FAX

KETLINSKI RESIDENCE
McCALL, IDAHO
EXISTING CONDITIONS WITH SITE PLAN

VERIFY SCALE	
BAR IS ONE INCH ON FULL SIZE DRAWING 1"	
PROJECT	21056
DATE	10/22/2021
DRAWING NO.	SHEET NO.
C-1	1 OF 2

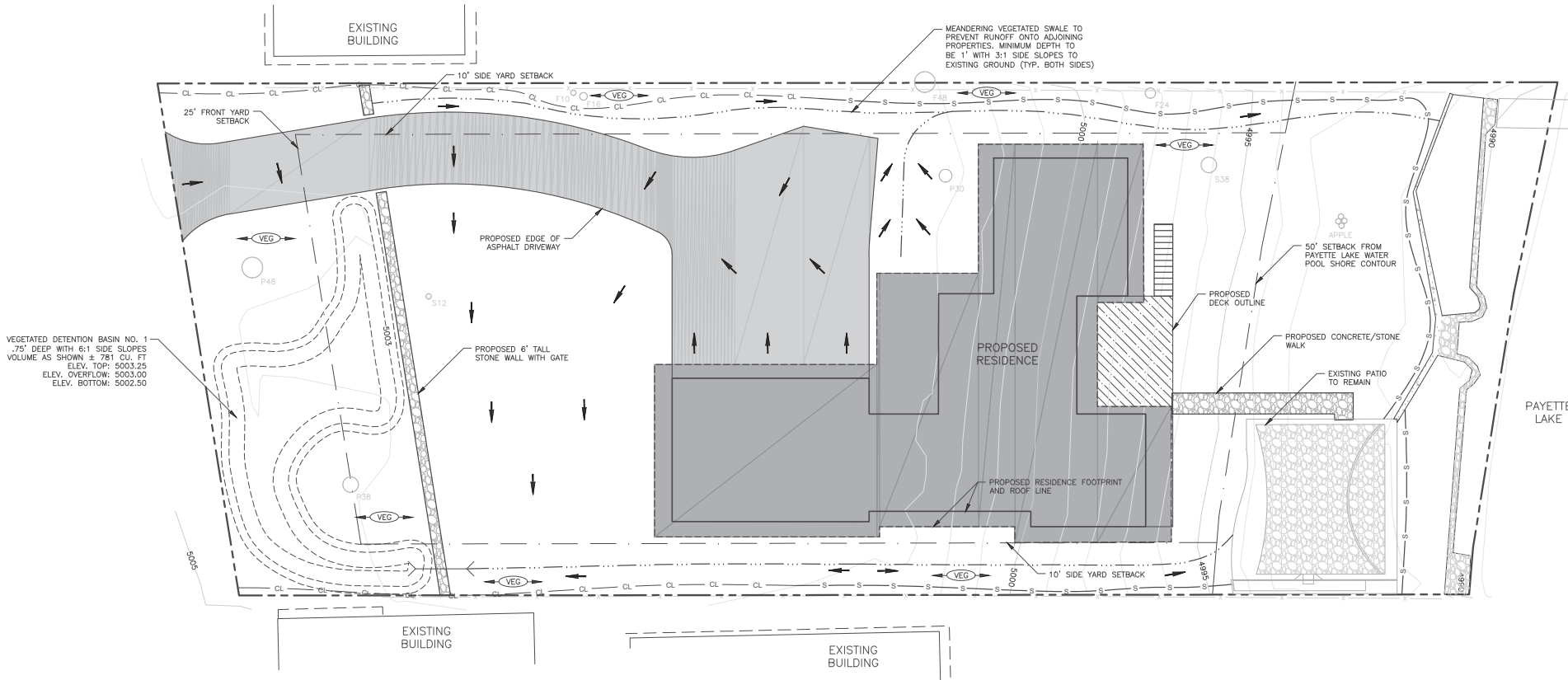
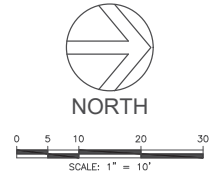
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- REFER TO THE "STATE OF IDAHO, CATALOG OF STORMWATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES" FOR FURTHER DETAILS ON BMP IMPLEMENTATION AND INSTALLATION.
- ALL EROSION AND SEDIMENT CONTROL BMP'S SHALL BE INSTALLED PRIOR TO THE START OF ANY PROJECT CONSTRUCTION OR EARTH DISTURBING ACTIVITIES AND SHOULD REMAIN IN PLACE UNTIL ALL DISTURBED/EXPOSED AREAS HAVE BEEN STABILIZED AND/OR REVEGETATED.
- THE OWNER AND/OR THEIR SELECTED CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL BMP'S IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REQUIREMENTS.
- THE IMPLEMENTATION OF THESE EROSION AND SEDIMENT CONTROL MEASURES INCLUDING INSTALLATION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THIS PLAN IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL PROJECT CONSTRUCTION IS COMPLETED AND APPROVED BY THE OWNER. THE OWNER SHALL BE RESPONSIBLE FOR ALL MAINTENANCE AFTER THE PROJECT IS APPROVED.
- WATTLES MAY BE USED IN PLACE OF SILT FENCE WHERE DETERMINED APPROPRIATE. SILT FENCE HAS BEEN SHOWN ON THE PROPERTY LINES IN SOME AREAS TO PREVENT ENCRoACHMENT ONTO NEIGHBORING PROPERTIES.
- WORK ACTIVITIES SHALL TAKE PLACE WITHIN THE CLEARING LIMITS AS SHOWN ON THIS PLAN. CONTRACTOR SHALL PRESERVE NATURAL VEGETATION OUTSIDE OF CLEARING LIMITS.
- STAGING AREA(S) TO BE LOCATED BY CONTRACTOR ALONG WITH PORTABLE TOILETS, GARBAGE RECEPTACLES, CONCRETE WASHOUT, AND ALL OTHER CONTRACTOR FACILITIES.
- ALL SITE GRADING ADJACENT TO THE NEW RESIDENCE SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING AT A MINIMUM OF 1.5% IN HARDSCAPE AREAS AND 4% IN LANDSCAPE AREAS.
- DRIVEWAY GRADES SHALL BE SLOPED AWAY FROM THE GARAGE AT A MINIMUM SLOPE OF 2% AND A MAXIMUM SLOPE OF 6% FOR A DISTANCE OF NO LESS THAN TEN (10) FEET. GRADING OF THE DRIVEWAY SHALL BE IN ACCORDANCE WITH THE DIRECTION OF THE DRAINAGE FLOW DIRECTION ARROWS AS SPECIFIED IN THE STORMWATER MANAGEMENT PLAN.
- AREAS BETWEEN NEW RESIDENCE AND PROPERTY BOUNDARIES SHALL BE SLOPED TO INSURE RUNOFF IS KEPT ON-SITE. SWALES SHALL BE CONSTRUCTED ADJACENT TO/NEAR SIDE PROPERTY LINES TO TO PREVENT RUNOFF FROM FLOWING ONTO ADJOINING PROPERTIES. THESE SWALES ARE INTENDED TO BE FIELD FIT AND MEANDERED AROUND EXISTING VEGETATION AND SITE FEATURES AS NECESSARY.
- REVEGETATION AND STABILIZATION OF ALL DISTURBED PROJECT AREAS SHALL BE IN ACCORDANCE WITH THE PROJECT'S LANDSCAPE DESIGN. IF A LANDSCAPE DESIGN/PLAN IS NOT AVAILABLE, DISTURBED AREAS SHALL BE REVEGETATED WITH A GRASS MIXTURE NATIVE TO THAT AREA.

LEGEND:

- PROPERTY BOUNDARY
- EXISTING CONTOUR
- PROPOSED ROOF AREA
- PROPOSED ASPHALT DRIVEWAY
- PROPOSED STONE WALL/WALK
- PROPOSED DECK AREA
- PROPOSED DRAINAGE FLOW DIRECTION ARROW
- PROPOSED DRAINAGE SWALE
- S --- SILT FENCE
- CL --- CONSTRUCTION/CLEARING LIMITS
- VEG --- PRESERVE EXISTING VEGETATION
- P24 --- EXISTING TREE W/DIA. IN INCHES



VEGETATED DETENTION BASIN NO. 1
 .75' DEEP WITH 6:1 SIDE SLOPES
 VOLUME AS SHOWN ± 781 CU. FT
 ELEV. TOP: 5003.25
 ELEV. OVERFLOW: 5003.00
 ELEV. BOTTOM: 5002.50

MEANDERING VEGETATED SWALE TO PREVENT RUNOFF ONTO ADJOINING PROPERTIES. MINIMUM DEPTH TO BE 1' WITH 3:1 SIDE SLOPES TO EXISTING GROUND (TYP. BOTH SIDES)

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

CRESTLINE ENGINEERS
 323 DEINHARD LANE, SUITE C · PO BOX 2330
 McCALL, IDAHO 83638
 208.634.4140 · 208.634.4146 FAX

KETLINSKI RESIDENCE
 McCALL, IDAHO
 PRELIMINARY STORMWATER MANAGEMENT PLAN

VERIFY SCALE	
BAR IS ONE INCH ON FULL SIZE DRAWING 1"	
PROJECT	21056
DATE	10/23/2021
DRAWING NO.	SHEET NO.
C-2	2 OF 2

Path: M:\01\WAC\Design\Projects\21056\21056-DWG\DWG\21056_PreliminaryStormwaterManagementPlan.dwg File Name: 21056_StormwaterManagementPlan.dwg Plot Date: 10/22/2021 1:23:21 PM

From: [Laura Shealy](#)
To: [Brian Parker](#)
Subject: Re: City of McCall Request for Comment - DR-21-36 - 111 West Lake Street
Date: Thursday, November 4, 2021 8:09:22 PM
Attachments: [image001.png](#)
[image002.png](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Brian,

The home appears to have a very gentle slope down to Payette Lake which is good but the grass between the home and lake (unless there is zero plan to fertilize this lawn) is harmful to lake water quality from any runoff of rain, snow or storm event. The hardscape path leading from the house to the existing patio does not lend itself to the absorption of stormwater runoff. Native plants and grasses are great and should be used as one of the best erosion breaks toward the lake. Please follow the Lake-A-Syst packets when preparing the site for construction. All measures need to be taken to protect erosion into Payette Lake during the entire construction process. Thanks Laura Shealy Big Payette Lake Water Quality Council

On Thu, Nov 4, 2021 at 2:47 PM Brian Parker <bparker@mccall.id.us> wrote:

All,

Please provide comment on the attached application by November 18, 2021.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us

Blog: mccallcitysource.com

Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



Keeping citizens informed.

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PAYETTE LAKES RECREATIONAL WATER AND SEWER DISTRICT

201 Jacob Street • McCall, Idaho 83638 • office 208-634-4111 • fax 208-634-7613

November 16, 2021

Brian Parker
City of McCall
216 East Park Street
McCall, Idaho 83638

RE: DR-21-36, 111 West Lake Street

Brian:

Payette Lakes Recreational Water and Sewer District (District) has reviewed the proposed project at the above referenced address, and has the following comments.

1. There is a sewer service connection for the existing structure in the area of construction, the sewer service connection shall be protected from damage during demolition of the existing structures and construction of the proposed structure to prevent excessive water, or debris from entering the sewer system. Failure to do so will result in the owner/contractor being responsible for any cost incurred by the District for cleaning or removing debris that enters the sewer system.
2. Inflows of surface water and excessive infiltration are prohibited. Such prohibited sources of inflow shall include, but not be limited to, the following: Heating, cooling, or water system discharges in excess of one thousand gallons per day, storm water connections, sub-water drains, floor drains located within garages, foundation drains, roof drains, swimming pools, street drains, basement drains, sump pumps and abandoned sewer lines.
3. The owner/contractor shall notify the District two (2) business days before re-connection to the sewer stub.

Please contact me should you have any questions.

Best Regards,

A handwritten signature in blue ink that reads "Dale Caza".

Dale Caza
Manager, Payette Lakes Recreational Water and Sewer District
Email: dcaza@plrwsd.org

From: [Morgan Stroud](#)
To: trketlinski@gmail.com
Cc: [Brian Parker](#); [Meredith Todd](#)
Subject: DR21-36 - 111 W Lake St - Engineering Review #1
Date: Friday, November 19, 2021 10:25:27 AM
Attachments: [image001.png](#)

The Public Works Department has reviewed the documents submitted for review for **DR21-35** and have the following comments and concerns.

1. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Please explain the thought process behind the detention pond and its location. It seems that most of the runoff from the property would accumulate north of where the detention pond is located.
 - b. Some variation of infiltration device should be used north of the proposed home to infiltrate stormwater runoff prior to it entering Payette Lake.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan and appropriate hydrographs and calculations.
2. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
3. The landscaping plan and stormwater management plan do not seem to coincide regarding the side yard swales that are proposed and large trees that are being proposed.
4. Please show the water line location on your plans and identify if any changes to the service size will occur.
5. Walls and structures over 4 feet in height are required to be designed by a licensed professional engineer.
6. An access permit from ITD will be needed for alterations to the access point onto Lake Street prior to any construction commencing.

Comments 1 through 5, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance. Please let me know if you have any questions or concerns.

Thank you,
Morgan Stroud, E.I.T. | Staff Engineer
216 E Park Street | McCall | ID 83638
Direct: 208.634.3458 | Cell: 208.315.2299



**McCall Area Planning and Zoning Commission
Staff Report**

DR-21-39, SR-21-20, SH-21-14

1415 Warren Wagon Road

December 7, 2021

Applicant: Len Jordan
Representative: Todd Allen Construction
Application: Design Review, Scenic Route Review, Shoreline Review
Zoning District: R4 – Low Density Residential
Jurisdictional Area: Area of Impact

Description

A design review, scenic route review, and shoreline environs review application to relocate an existing single-family residence, to construct an uncovered deck attached to relocated residence, and to construct a new 6,105 square-foot single-family residence.

Staff Analysis

Project Information

Zoning District: R4 – Low Density Residential

Comprehensive Plan Designation: Low Density Residential

Project Acreage: 1.26-acres (54,886 square-feet)

Proposed Use: Single-family residence

Dimensional Standards:

Primary Residence

	Proposed	Required
Front Setback	52-feet	25-feet
Rear Setback	58-feet	20-feet*
North Side Yard Setbacks	15-feet	18-feet, 1-inch
South Side Yard Setbacks	15-feet	26-feet, 5-inches
Lot Coverage	6,858 square-feet (99% of allowed)	6,899 square feet (21% of total)
Snow Storage	Greater than 768 square-feet	Greater than 768 square-feet
Building Height	30-feet	35-feet

* Except where the 50-foot shoreline setback extends beyond the required rear setback.

Relocated Cabin

	Proposed	Required
Front Setback	25-feet	25-feet
Rear Setback	58-feet	20-feet*
North Side Yard Setbacks	11 -feet	8-feet, 8-inches
South Side Yard Setbacks	11-feet	8-feet, 8-inches
Lot Coverage	3,821 square-feet (91% of allowed)	4,221 square feet (26% of total)
Snow Storage	Greater than 848 square-feet	Greater than 848 square-feet
Building Height	21-feet, 6-inches	35-feet

* Except where the 50-foot shoreline setback extends beyond the required rear setback.

Code Sections of Interest

- McCall City Code Section 3.2.02: Meanings of Terms or Words
 LOT COVERAGE: The building footprint plus other surfaces such as driveways, decks, patios, and walkways expressed as a percentage of the total lot area. See section 3.3.06 of this title.

- McCall City Code Section 3.3.06: Residential Zone Lot Coverage of Improvements:
The maximum permitted lot coverage varies with lot size. [Figure 3.3.06](#) of this section establishes the maximum permitted lot coverage for all uses in all residential zones.

To determine the lot coverage for a proposed development, calculate the following values and add to determine the total coverage. (Ord. 821, 2-23-2006, eff. 3-16-2006)

Building/house footprint	100 percent of footprint area = A
Decks, patios, walkways, plazas, etc.	50 percent of covered area = B
Driveways, surface parking, etc.	35 percent of covered area = C
	Total = A+B+C

(Ord. 864, 2-12-2009)

Determine the maximum permitted lot coverage (A+B+C) for home sites by using [figure 3.3.06](#) of this section. For example:

Home sites equal to or less than 5,000 square feet	50 percent
Home sites of 8,000 square feet	33 percent
Home sites of 10,000 square feet	30 percent
Home sites of 20,000 square feet	24 percent
Home sites of 90,000 square feet	15 percent

Home sites of 200,000 square feet and above	10 percent
---	---------------

Some examples:

(A) Lot Size = 7,000 square feet.

House plus garage footprint = 1,800 square feet x 1.00 = 1,800 (A).

Patio = 200 square feet x 0.50 = 100 (B).

Driveway and walk = 300 square feet x 0.35 = 105 (C).

A + B + C = 1,800 + 100 + 105 = 2005.

Lot size = 7,000; from [figure 3.3.06](#) of this section, maximum value for A + B + C = 36%, or 2,520.

So this example meets the coverage requirement.

(B) Lot Size = 15,000 square feet.

House plus garage footprint = 3,500 square feet x 1.00 = 3,500 (A).

Patio and deck = 800 square feet x 0.50 = 400 (B).

Driveway and walk = 600 square feet x 0.35 = 210 (C).

A + B + C = 3,500 + 400 + 210 = 4,110.

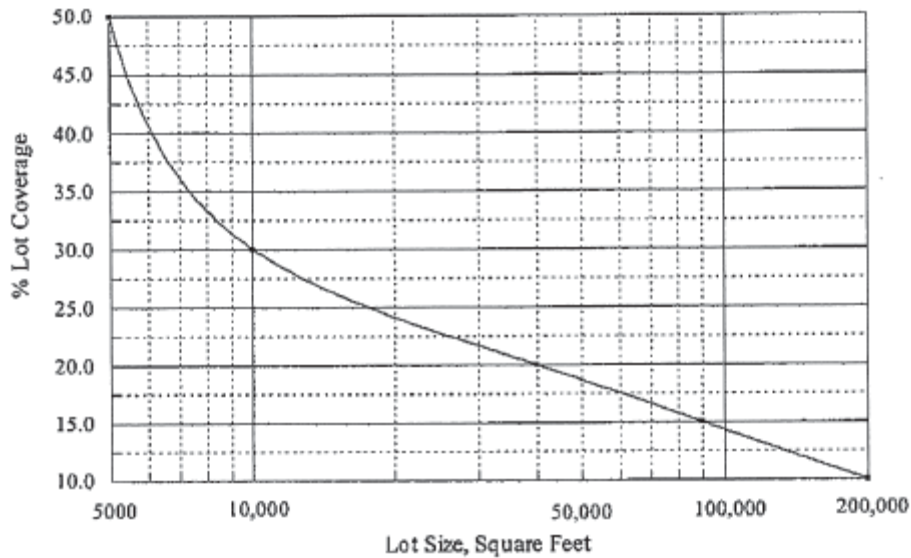
Lot size = 15,000; from [figure 3.3.06](#) of this section, maximum value for A + B + C = 26%, or 3,900.

So in this example, something will have to be reduced to bring the total to 3,900.

Figure 3.3.06

Figure 3.3.06

Residential Lot Coverage vs. Lot Size



- McCall City Code Section 3.3.09(C): Residential Zone Design Review Standards”

Residential Design Standards:

1. Building Scale: The building shall be in scale with the site conditions and surrounding structures. Appropriate scale can be accomplished through the following:

- a. The principal structure on the site is the dominant element.
- b. Building massing and scale is responsive to the site conditions, including views through and of the site, and existing vegetation. Development that is designed to complement rather than dominate the natural landscape.
- c. Variations in wall planes and roof lines are incorporated to minimize the apparent scale of the building.
- d. Porches, balconies, decks and terraces that strengthen the residential scale and character of the building.

2. Building Design: Building designs shall enhance and/or continue the styles found in the McCall area with allowance for new interpretations of historic details. Local natural building materials, roof overhangs, covered porches and entrances, simplicity of design, and segmented windows are elements of the McCall style. Building designs that are in keeping with the McCall style include the following:

- a. Materials:

i. Natural looking exterior materials looking siding and decking materials that resist heat and flames, including: composite decking, cement, plaster, stucco, masonry (such as stone brick or blocks) and fiber-cement siding, soffit, and trim products.

ii. Use of stones that convey the appearance of a structural element rather than a veneer facing.

iii. Metal when used in combination with natural materials.

b. Roofs:

i. Roofs that compliment and respond to the mountain setting and heavy snow environment.

ii. Hipped, pitched, shed and gabled roofs.

iii. Roofs designed to prevent snow or ice from shedding directly onto a pedestrian walkway, access or adjacent property.

iv. Class A fire rated roof assemblies.

c. Exterior Color:

i. Exterior wall colors that tend toward earthy warm hues, with accent colors kept to a minimum of two (2) and used to highlight entrances.

ii. No harshly contrasted color combinations, brilliant, luminescent or day-glow colors on exterior finishes.

d. No blank walls shall face street frontages, including blank or unarticulated garage doors.

e. Decks and balconies shall be designed to handle snow and drift loads including snow shedding from roof overhangs above.

• McCall City Code Section 3.7.023(B): Requirements for Development:

(B) Permit Criteria: No conditional use or building permit shall be issued, nor is any development, grading, or alteration of any land within this zone permitted, unless the applicant establishes to the satisfaction of the commission and council in the case of a conditional use, or of the administrator in the case of a building permit, that:

1. The proposed development meets all applicable requirements of this title and title IX of this code.

2. The plans accurately identify the water pool shore contours and high water marks, which, in the case of river environs, shall mean the limits of the area of special flood hazard.

3. A letter is on file from a specialist certified by the United States army corps of engineers wetlands expert that certifies that no wetlands related issues or issues related to fill of navigable waters issues were presented by the proposed development; or that a section 404 permit has been issued or is forthcoming by the corps of engineers, whichever is appropriate, city approval(s) under this title and title IX of this code are contingent upon all applicable section 404 permit requirements being met; if a permit requirement is not met, the city may revoke its approval(s) under this title and title IX of this code.

4. The requirements of the underlying zone are met.

5. The fifty foot (50') building setback line is met per subsection (C)3(c) of this section.

6. Proof of stormwater certification training has been provided by the individual applying for the building permit.

- McCall City Code Section 3.7.031: Lands Included

The scenic route zone includes:

(A) One hundred fifty foot (150') wide strips of land bounded by the right of way lines of the following named streets, and by lines parallel to and one hundred fifty feet (150') away from such right of way lines, together with any right of way not part of the roadway:

3. Warren Wagon Road to the northern boundary of the area of city impact.

(B) Any structure or a portion thereof which may be visible when traveling along the roadways designated above and any part of which is located within one hundred fifty feet (150') of the nearest right of way line of a road named in this section or designated pursuant to this section; such a structure is subject to design review and approval under chapter 16, "Design Review", of this title, notwithstanding that portions of the structure are not on land that is within this zone.

- McCall City Code Section 3.7.032(B): Requirements for Development:

The commission shall ascertain whether the proposed development, improvement or use will:

1. Block or disrupt the visibility of significant views or features.
2. Be compatible (in terms of setback, bulk, height, design, finish materials, signing and landscaping) with its immediate surroundings and the desired visual quality of the scenic route.

- McCall City Code Section 3.7.032(I): Requirements for Development:

Landscaping: In addition to the requirements set forth in section 3.8.13 "Landscaping, Screening And Buffering," development within the scenic route zones shall provide the following landscaping:

1. One (1) native tree including evergreen and deciduous and three (3) native shrubs each ten feet (10') of the linear street frontage along the scenic route. Not less than fifty percent (50%) shall be evergreen trees.

2. The required landscaping shall be located with a twenty-five feet (25') setback from the scenic route. Landscaping shall be arranged within groups or clustered allowing spacing for natural maturity. Linear designed landscaping is not acceptable.

3. If conflicts arise between this section and section 3.8.04 "Fire Mitigation Standards," or in the use of any snow storage or utility easements, a recommended resolution will be the responsibility of the McCall Fire Protection District or the Administrator.

- McCall City Code Section 3.16.07: Design Review Criteria:

The commission or administrator shall determine the following before approval is given:

- (A) The project is in general conformance with the comprehensive plan.
- (B) The project does not jeopardize the health, safety or welfare of the public.
- (C) The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance as enumerated in section 3.16.01 of this chapter.
- (D) The project will have no substantial impact on adjacent properties or on the community at large.
- (E) If applicable, a subdivision design review document has been reviewed and approved by the commission in lieu of the design guidelines.
- (F) For projects in the shoreline and river environs zone, the project will not have an unreasonable and adverse impact on the visual quality of its setting or the water quality.
- (G) For projects in the scenic route zone, the project will preserve and enhance the scenic quality of the street or highway.

Comprehensive Plan Sections of Interest

- Vision in Motion – Our Vision:

McCall is a diverse, small town united to maintain a safe, clean, healthy, and attractive environment. It is a friendly, progressive community that is affordable and sustainable.

- Deep Dive – Future Land Use Designations – Low Density Residential:

This land use designation is intended to provide for the development of low-density single-family residential neighborhoods. These neighborhoods consist of larger home sites.

Staff Discussion

- The applicant is proposing to construct a 6,105 square foot home, move an existing cabin to the north side of the property boundary that the cabin currently straddles, and construct a 1,232 square-foot garage on the northern parcel. The two parcels were the subject of ROS-21-18, approved by the McCall Area Planning & Zoning Commission on November 2, 2021. The record of survey adjusting the property boundaries between the two parcels has not been recorded to date. No building permits for either parcel should be issued prior to recordation of the record of survey associated with ROS-21-18.
- The applicant is proposing to construct a single shared driveway between the two parcels. Prior to the issuance of a building permit, the applicant should provide a copy of the recorded shared driveway agreement between the two parcels.
- The applicant is proposing to construct large stone walkways with gaps between the stones to allow for mosses and grasses to grow. The applicant is requesting that these walkways do not count toward the total lot coverage calculation as they would be over the maximum allowable

lot coverage. As this type of walkway allows for greenery to grow in the path and for drainage between the stones, staff is supportive of this interpretation.

Comments

Agency –

McCall Public Works

In an email dated November 22, 2021, the City of McCall Staff Engineer had the following comments:

The Public Works Department has reviewed the documents submitted for review for **DR21-39** and have the following comments and concerns.

1. There is currently a Record of Survey for these two parcels that was never finalized. The finalization of that process will need to be completed before the site work and building can occur.
2. An access easement should be provided to the garage and relocated cabin through the new parcel that the new home will be located on.
3. 12 foot snow storage and utility easement should be declared along Warren Wagon Road in-lieu of additional right-of-way dedication because Warren Wagon Road is identified to only have around 45.5 feet of existing right-of-way and the standard width for a major collector road is 70 feet.
4. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Identifying the upstream flows and volumes which are tributary to the project site and more specifically to the storm drain that is being proposed to be relocated with this project. These values should be used to help determine that the pipe sizing is sufficient and will not cause overflow issues upstream in the storm drain that is located within the Warren Wagon right-of-way.
 - b. It appears that there is an infiltration trench proposed parallel to the lake, however, the preliminary grading does not show that the runoff from the swales will enter it. Adjusting the grading and extending the trench to the south may be needed to capture runoff from the front of the home.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan, detention basin areas, pipe sizing of the storm drain, and appropriate hydrographs and calculations.
5. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
6. There will need to be two to three water meters for this project. The third meter is dependent upon if there will be habitable space provided in the new standalone three car garage. There is required to be a minimum of one water meter for each lot. Please show on your site plan where the meters will be located and please use the City of McCall's Standard Details.
7. Depending on the water demands, upsizing of the service line may be required. Providing information of the proposed water usage for each lot and each habitable building will be key in determining if upsizing will be required.

8. One of the site plans should include the location of the watermain Warren Wagon Road and detail how the new water meter(s) are being connected and detail if any service lines will need to be abandoned due to upsizing requirements.
9. A Public Works Permit will need to be obtained prior to building permit issuance for the driveway onto **Warren Wagon Road**. More information about the Public Works Permit can be found at this link: <https://www.mccall.id.us/public-works-permit#:~:text=City%20of%20McCall%20Public%20Works,the%20city's%20water%20meter%20pits>, or contacting the Streets Superintendent, Cris Malvich at 208.634.8946.

Comments 1 through 8, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance.

McCall City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I visited this site with the design team last summer and given the significant amount of trees between Warren Wagon Road and the new structures and driveway. I would be happy to make further consultations as the project takes shape and once the outlined tree protection measures are in place. At this time, I have no concerns and would be happy to consult as the project takes shape.

Big Payette Lake Water Quality Council (BPLWQC)

In an email dated November 8, 2021, the BPLWQC had the following comments:

The Jordan residence slopes gradually but directly down to Payette Lake. It is good that they are planning for native plants in the landscaping. Fertilizers should be prohibited in this waterfront environ due to potential contamination of the lake. A large construction site like this home needs all erosion control planning approved ahead of construction. The Lake-A-Syst is an excellent guide for homeowners and builders and covers all aspects of erosion mitigation techniques. The City of McCall has a copy of the Lake-A-Syst.

Public –

No public comment has been received.

Staff Recommendation

Staff recommends approval of this application with the Conditions of Approval listed below.

Conditions of Approval

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. No building permits for either parcel shall be issued prior to recordation of the record of survey associated with ROS-21-18.
3. Prior to the issuance of a building permit, the applicant should provide a copy of the recorded shared driveway agreement between the two parcels.

4. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

IN RE:)
)
Jordan RESIDENCE)
Design Review, Shoreline)
Environs Overlay Review, and)
Scenic Route Overlay Review)
)
Application Number:)
DR-21-36, SR-21-18, SH-21-12)

**McCALL AREA PLANNING AND ZONING COMMISSION
FINDINGS OF FACT, CONCLUSIONS OF LAW, AND
DECISION**

FINDINGS OF FACTS

Applicant: Len Jordan

Representative: Todd Allen Construction

Application: A design review, scenic route review, and shoreline environs review application to relocate an existing single-family residence, to construct an uncovered deck attached to relocated residence, and to construct a new 6,105 square-foot single-family residence.

Address: 1415 Warren Wagon Road

Location: Tax Lot No. 23, situate in Government Lot 4 of Section 5 & Lot 1 of Block 1, Recorder’s Plat, situate in the NE ¼ of Section 8, T18N, R3E, B.M., Valley County, Idaho.

Public Notice: Newspaper: The Notice of Hearing was published in the *Star News* on November 18, 2021.
Mailing: The Notice of Hearing was mailed by the applicant to property owners within 300 feet on November 16, 2021.
Posting: The Notice of Hearing was posted by the applicant on the subject property on November 17, 2021.

Zoning: R4 – Low Density Residential

Property Size: 1.26-acres (54,886 square-feet)

Lot Coverage: 6,858 square-feet (99% of allowed)

Building Height: The maximum height of the proposed residence is 30-feet.

Setbacks:

	Proposed	Required
Front Setback	52-feet	25-feet
Rear Setback	58-feet	20-feet*
North Side Yard Setbacks	15-feet	18-feet, 1-inch
South Side Yard Setbacks	15-feet	26-feet, 5-inches

Parking Spaces: Provided: 2+ spaces

Required: 2 spaces per MCC 3.8.062

APPROVAL STANDARDS

MCC 3.7.02 Shoreline and River Environs Zone

No conditional use or building permit shall be issued, nor is any development, grading, or alteration of any land within this zone permitted, unless the applicant establishes to the satisfaction of the commission and council in the case of a conditional use, or of the administrator in the case of a building permit, that:

1. **The proposed development meets all applicable requirements of this title and title IX of this code.** The proposed site plan and structure meet the applicable requirements of MCC Title 3, Chapter 7, Chapter 8, and Title 9.
2. **The plans accurately identify the water pool shore contours and high water marks, which, in the case of river environs, shall mean the limits of the area of special flood hazard. The site**

plan indicates the Water Pool Shore Contour elevation. The water pool contour line is indicated on the site plan.

3. **A letter is on file from a specialist certified by the United States army corps of engineers wetlands expert that certifies that no wetlands related issues or issues related to fill of navigable waters issues were presented by the proposed development; or that a section 404 permit has been issued or is forthcoming by the corps of engineers, whichever is appropriate, city approval(s) under this title and title IX of this code are contingent upon all applicable section 404 permit requirements being met.** It appears that a 404 permit will not be necessary although the City reserves the right to require this at a later date if it becomes necessary.
4. **The requirements of the underlying zone are met.** The proposed residence meet the requirements of the Shoreline Zone and R4-Low Density Residential Zone.
5. **The fifty-foot (50') building setback line is met per subsection (C)3(c) of this section.** The fifty foot (50') building setback for properties within the Shoreline and River Environs Zone is indicated on the submitted site plan. The proposed residence is located outside of this setback.
6. **Proof of stormwater certification training has been provided by the individual applying for the building permit.** Proof of stormwater certification is required prior to issuance of a building permit.
7. **The Commission determined whether the proposed development, improvement or use:**
 - a. **Dominates the vistas of the water to the extent that it has an unreasonable adverse impact on the visual quality of its setting.** The proposed residence is large but is consistent with other residences in the neighborhood.
 - b. **Is compatible in form, line, color and texture with its surroundings.** The proposed residence is compatible in form, line, color and texture because it utilizes natural colors and materials and is designed similarly to other residences in the neighborhood.

- c. **Significantly differs in scale or contrast from its surroundings to the extent that it has an unreasonable adverse impact on the visual quality of its setting.** The proposed residence meets the dimensional standards of the zone and is designed to minimize the difference between the site’s topography and the built form. Therefore, the proposed residence does not differ in scale or contrast from its surroundings.
- d. **Creates a wall of structures as viewed from the water.** The proposed residence will be set back approximately 60-feet from the lake. The proposed residence is well landscaped from the lake and will not create the appearance of a wall of structures.

MCC 3.7.032 Scenic Route Requirements for Development

Scenic Route Zone

The Commission shall determine whether the proposed development, improvement, or use:

1. **The project preserves and enhances the scenic quality of the street or highway.**

The proposal will maintain the scenic quality of Warren Wagon Road by minimizing the impacts of existing vegetation, filling in between existing vegetation with native trees, and quality architectural design of the proposed residence.

2. **Is compatible (in terms of setback, bulk, height, design, finish materials, and signing with its immediate surroundings and the desired visual quality of the scenic route.**

The proposed residence is at the maximum allowable lot coverage, but is consistent with other residences in the area. Compatibility with the desired visual quality of the scenic route is provided by: (1) building setbacks that are more than the minimum requirements; (2) the proposed landscaping that will buffer views of the new structures; and (3) the compatible exterior building design, materials, and colors.

3. **Building Clustering** N/A

4. **Landscaping:** The applicant is providing landscaping adjacent to Warren Wagon Road in conformance with McCall City Code Section 3.7.032(l).

MCC 3.16: Design Review

The commission has determined that:

1. The project is in general conformance with the comprehensive plan.
2. The project does not jeopardize the health, safety or welfare of the public.
3. The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance, adopted by the city of McCall.
4. The project will have no substantial impact on adjacent properties or the community at large.

Please see review of the Design Guidelines below.

MCC 3.3.09: DESIGN Requirements

General Objectives for Residential Design

1. **Support a residential character and is compatible with its surrounding neighborhood:** The structure is residential in character and is compatible with the surrounding neighborhood.
2. **Preserves natural features:** The proposed residence is approximately 60-feet from the lake, and therefore will preserve the natural features within the 50 ft. shoreline setback.
3. **Promotes active and safe streetscapes:** The proposed residence does little to promote an active or safe streetscape.

Residential Review Standards

1. **Building Scale:** The proposed residence is similar in design and scale to other homes along the lake.
2. **Building Design**
 - a. **Materials:** The proposed residence is to use wood, stone, and other materials with a generally natural appearance.

- b. **Roof:** The proposed residence will utilize synthetic cedar shake shingles and metal roofing.
- c. **Exterior Color:** Exterior colors are of natural hues.
- d. **Blank Walls:** Blank walls are avoided by including glass on all facades and undulating the street facade.
- e. **Snow Loads:** Snow loads will be verified as part of the building permit process.

MCC 3.8 General Development Standards

1. **Tree Removal:** The applicant has worked with the City Arborist to minimize the number of significant trees to be removed.
2. **Fire Hazard Mitigation:** The proposed landscaping is consistent with the fire hazard mitigation standards of McCall City Code Section 3.8.04.
3. **Off Street Parking:** Adequate parking has been provided for the residential use.
4. **Bicycle Parking:** N/A
5. **Driveways:** The property will be accessed from Warren Wagon Road. A driveway permit from Public Works and shared driveway agreement will be required.
6. **Fencing And Walls:** N/A
7. **Accessory Use, Buildings And Structures:** The proposed accessory garage located on the northern parcel is consistent with the requirements of 3.8.11.
8. **Accessory Dwelling Units:** N/A
9. **Corner Vision:** N/A
10. **Landscaping And Buffering:** The proposed landscaping meets the requirements of McCall City Code.
11. **Snow Storage And Drainage:** Adequate snow storage has been identified on site. Final engineering approval of the grading and drainage plan will be required prior to issuance of a building permit.

12. **Main Entrances In R4 Through R16 Zones:** The plans are consistent with McCall City Code Section 3.8.19.

13. **Special Standards For Garages:** Due to the size of the lot, the residence is exempt from garage design standards.

14. **Local Housing Density Bonus Program:** N/A

15. **Seasonal Dwelling Units:** N/A

16. **Site Design**

- a. Preserve natural resources – The plans submitted preserve natural resources.
- b. Reduce land and water modifications – a grading and drainage plan will be required to receive final approval by the City Engineer prior to issuance of a building permit. The plan shall demonstrate that the project can meet the City’s drainage management guidelines.
- c. Location to preserve prominent skylines - the location will not impact any significant views of skylines
- d. Underground utilities – all utilities will be installed underground

Architecture

1. **Enhance McCall Classic Styles:** The proposed addition is compatible with classic McCall styles.
2. **Minimize Scale:** The building design uses architectural details to minimize its apparent scale.
3. **Building Additions:** N/A
4. **Roof Lines:** The roof line is varied to minimize the scale.
5. **Mechanical Equipment:** No exterior mechanical equipment is proposed.
6. **Multi-Unit Structures:** N/A
7. **Balconies and Porches:** The proposed residence includes balconies and porches.
8. **Exterior Doorways:** The proposed residence has an exterior doorway on the longest street facing façade.
9. **Wall Materials:** Exterior wall materials include wood, metal, and stone.
10. **Shop Front Design:** N/A
11. **Wall Colors:** Proposed wall colors are of natural hues.

Landscaping and Site Design

1. **Light Fixtures:** Light fixtures are in compliance with McCall's Outdoor Lighting Ordinance.
2. **Fences and Walls:** N/A
3. **Retaining Walls:** N/A
4. **Paving and Streetscapes:** The proposed driveway is in conformance with McCall City Code
5. **Landscaping Plan:** The proposed landscaping is in conformance with McCall City Code.
6. **Site Conditions for Landscaping:** N/A
7. **Lawn Area:** All areas disturbed with construction shall be re-seeded with native shrubs or grasses.
8. **Plants as Screening:** The site has several existing trees and additional plantings are proposed to screen the site when viewed from Warren Wagon Road.
9. **Utility Installations:** All new utilities are required to be undergrounded.
10. **Snow Storage:** Adequate snow storage has been identified.
11. **Screen Parking Lots:** N/A
12. **Irrigation System Required:** N/A
13. **Retain Existing Vegetation:** Existing vegetation will be retained as much as possible, all disturbed areas will be reseeded with native seed prior to issuance of a certificate of occupancy.
14. **Preserve Existing Trees:** The applicant has worked with the City Arborist to preserve existing trees.
15. **Grading and Drainage:** Final approval of a grading and drainage plan will be required prior to issuance of a building permit.
16. **Maintenance:** Landscaping maintenance is the responsibility of the property owner.
17. **Sidewalks:** N/A
18. **Bike Paths:** N/A

Residential Districts

1. **Preserve historic residences:** No historic residences are impacted by this application.
2. **Preserve human scale in residential character:** The proposed residence steps down with the topography of the site to minimize building massing and is adequately landscaped.
3. **Preserve compatibility with surrounding neighborhoods:** The structure is compatible with the surrounding neighborhood.
4. **Preserve natural features of the immediate landscape and environment:** The proposed residence utilizes natural colors and materials, and is required to be adequately landscaped.
5. **Provide for community, or affordable, housing as needed:** The project does not provide for community or affordable housing; however, it is not required by code.
6. **Provide open spaces to enhance and maintain the rural character:** The proposed residence maximizes the allowable lot coverage.
7. **Provide living and moving space for native animals:** The proposed residence meets setback requirements, and therefore provides more living and moving space for animals than is required.
8. **Promote active and safe streetscapes in residential neighborhoods that are conducive to walking and biking:** The proposed residence does not promote an active or safe streetscape.

DEPARTMENT/AGENCY COMMENTS

Agency –

McCall Public Works

In an email dated November 22, 2021, the City of McCall Staff Engineer had the following comments: The Public Works Department has reviewed the documents submitted for review for **DR21-39** and have the following comments and concerns.

1. There is currently a Record of Survey for these two parcels that was never finalized. The finalization of that process will need to be completed before the site work and building can occur.

2. An access easement should be provided to the garage and relocated cabin through the new parcel that the new home will be located on.
3. 12 foot snow storage and utility easement should be declared along Warren Wagon Road in-lieu of additional right-of-way dedication because Warren Wagon Road is identified to only have around 45.5 feet of existing right-of-way and the standard width for a major collector road is 70 feet.
4. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Identifying the upstream flows and volumes which are tributary to the project site and more specifically to the storm drain that is being proposed to be relocated with this project. These values should be used to help determine that the pipe sizing is sufficient and will not cause overflow issues upstream in the storm drain that is located within the Warren Wagon right-of-way.
 - b. It appears that there is an infiltration trench proposed parallel to the lake, however, the preliminary grading does not show that the runoff from the swales will enter it. Adjusting the grading and extending the trench to the south may be needed to capture runoff from the front of the home.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan, detention basin areas, pipe sizing of the storm drain, and appropriate hydrographs and calculations.
5. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
6. There will need to be two to three water meters for this project. The third meter is dependent upon if there will be habitable space provided in the new standalone three car garage. There is required to be a minimum of one water meter for each lot. Please show on your site plan where the meters will be located and please use the City of McCall's Standard Details.
7. Depending on the water demands, upsizing of the service line may be required. Providing information of the proposed water usage for each lot and each habitable building will be key in determining if upsizing will be required.
8. One of the site plans should include the location of the watermain Warren Wagon Road and detail how the new water meter(s) are being connected and detail if any service lines will need to be abandoned due to upsizing requirements.
9. A Public Works Permit will need to be obtained prior to building permit issuance for the driveway onto **Warren Wagon Road**. More information about the Public Works Permit can be found at this link: <https://www.mccall.id.us/public-works-permit#:~:text=City%20of%20McCall%20Public%20Works,the%20city's%20water%20meter%20pits>, or contacting the Streets Superintendent, Cris Malvich at 208.634.8946.

Comments 1 through 8, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance.

McCall City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I visited this site with the design team last summer and given the significant amount of trees between Warren Wagon Road and the new structures and driveway. I would be happy to make further consultations as the project takes shape and once the outlined tree protection measures are in place. At this time, I have no concerns and would be happy to consult as the project takes shape.

Big Payette Lake Water Quality Council (BPLWQC)

In an email dated November 8, 2021, the BPLWQC had the following comments:

The Jordan residence slopes gradually but directly down to Payette Lake. It is good that they are planning for native plants in the landscaping. Fertilizers should be prohibited in this waterfront environ due to potential contamination of the lake. A large construction site like this home needs all erosion control planning approved ahead of construction. The Lake-A-Syst is an excellent guide for homeowners and builders and covers all aspects of erosion mitigation techniques. The City of McCall has a copy of the Lake-A-Syst.

Public –

No public comment has been received.

CONCLUSIONS OF LAW

1. The City of McCall has provided for the processing of Design Review applications, pursuant to Title 3, Chapter 16 of McCall City Code.
2. Adequate notice of the December 7, 2021 public hearing was provided, pursuant to Section 67-6512, Idaho Code and Title 3, Chapter 15 of McCall City Code.
3. Upon compliance with the conditions noted below, the application meets the Design Review Standards set forth in Title 3, Chapter 16 of McCall City Code.

DECISION

THEREFORE, the McCall Area Planning and Zoning Commission hereby **approves** this Design Review application, provided that the following conditions are met:

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. No building permits for either parcel shall be issued prior to recordation of the record of survey associated with ROS-21-18.
3. Prior to the issuance of a building permit, the applicant should provide a copy of the recorded shared driveway agreement between the two parcels.
4. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

Findings of Fact **adopted** this 7th day of DECEMBER, 2021.

Robert Lyons, Chair
McCall Area Planning and Zoning Commission

Attest:

Brian Parker, City Planner
City of McCall

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received:

11/16/2021

Fees Paid:

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # \$475 Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # \$300 Scenic Route (SR) - \$300
- # \$300 Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: Len Jordan Email: _____

Mailing Address: _____

Property Owner 2 (If Applicable): _____ Email: _____

Mailing Address: _____ Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: Todd Allen Construction Email: toddallenconstruction@gmail.com

Mailing Address: 335 Deinhard Ln McCall ID 83638 Phone: (208) 315-0473

PROPERTY INFORMATION

Address(es) of Property: 1415 Warren Wagon Road McCall ID

Legal Description of Property: Tax no. 23 Gov lot 4, section 5 and a portion of Lot 1 Block 1

Zoning District of Property: R4 Project Sq. Footage (If Applicable): 6,625 SF

Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: *(please attach supplemental information if needed)*

New single family residence with landscaping and terracing. New uncovered deck at relocated Cabin.

SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

The submittal items identified in the application info sheets are the minimum required materials for the City to accept applications. The City Planner may require additional information based on the specific circumstances of each proposal.

LEN K. JORDAN

Property Owner 1


Signature

Property Owner 2 *(If Applicable)*

Signature

Agent/Authorized Representative

Signature

FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.



1 PLAN: SITE PLAN
SCALE: 1" = 10'

- A - SCENCE
HINLEY ATLANTIS (MEDIUM)
- B - SCENCE
HINLEY ATLANTIS (LARGE)
- C - SCENCE
HINLEY COLLECTION DARK SKY
- D - STEP
WAC LED™ STEP LIGHT
- E - PATH
HINLEY ATLANTIS
- F - BARN LIGHTS
AT GUEST LIGHT
BY VINTAGE BARN

NOTE:
ALL EXTERIOR LIGHTING TO BE DARK
SKY COMPLIANT



LOCATION OF EXISTING
STUDIO
(SURVEYED JULY 2021)

DRAWN: _____
DATE: 2021.10.26
FILE: DESIGN REVIEW SET

NO.	DATE	DESCRIPTION
1	11.11.2021	GARAGE PLANS

PRINT DATE: Friday, November 12, 2021

ALLOWABLE LOT COVERAGE CALCULATOR

Lot Area in square feet: — Input lot area here

Lot Coverage % Allowed:

Lot Coverage Allowed: **6,899**

Proposed Lot Coverage

Footprint of existing structures: — Input if into boxes

Footprint of proposed structures:

Total structure size: 4774 at 100% of actual = 4774 sf

Area of Existing Uncovered Decks, Patios, Walkways, Plazas, etc.:

Area of New Uncovered Decks, Patios, Walkways, Plazas, etc.:

Total decks, etc. size: 2552 at 50% of actual = 1276 sf

Existing Driveways, surface parking, etc.:

New Driveways, surface parking, etc.:

Total driveway size: 2304 at 35% of actual = 806.4 sf

Total Proposed Lot Coverage: 6858.4 sf

Proposed Building Size

Occupied space (existing): — Input if into boxes

Unoccupied space (existing garage):

Covered exterior space (existing):

Occupied space (new):

Unoccupied space (new garage):

Covered exterior space (new):

Total Proposed Building Size: 6105 sf

Project Requires Design Review. Contact City Planner.

SIDEYARD SETBACK CALCULATOR

Lot Width: — Input Lot Width here

Minimum Side Setback: **15** **15'-0"**

Both side setbacks must add up to: **44.6** **44'-6"**

Note: Building may be no closer to side lot line than 1/2 the height of any building element. See city code.

LOT COVERAGE CALCULATIONS, SET BACK, AND SQUARE FOOTAGE INFORMATION

ARCHITECT



ENGINEER

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DRAWN: _____

DATE: 2021.10.26

FILE: DESIGN REVIEW SET

REVISIONS

NO.	DATE	DESCRIPTION

JORDAN RESIDENCE
1415 WARREN WAGON ROAD

ARCHITECT



ENGINEER

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DRAWN

DATE 2021.10.26

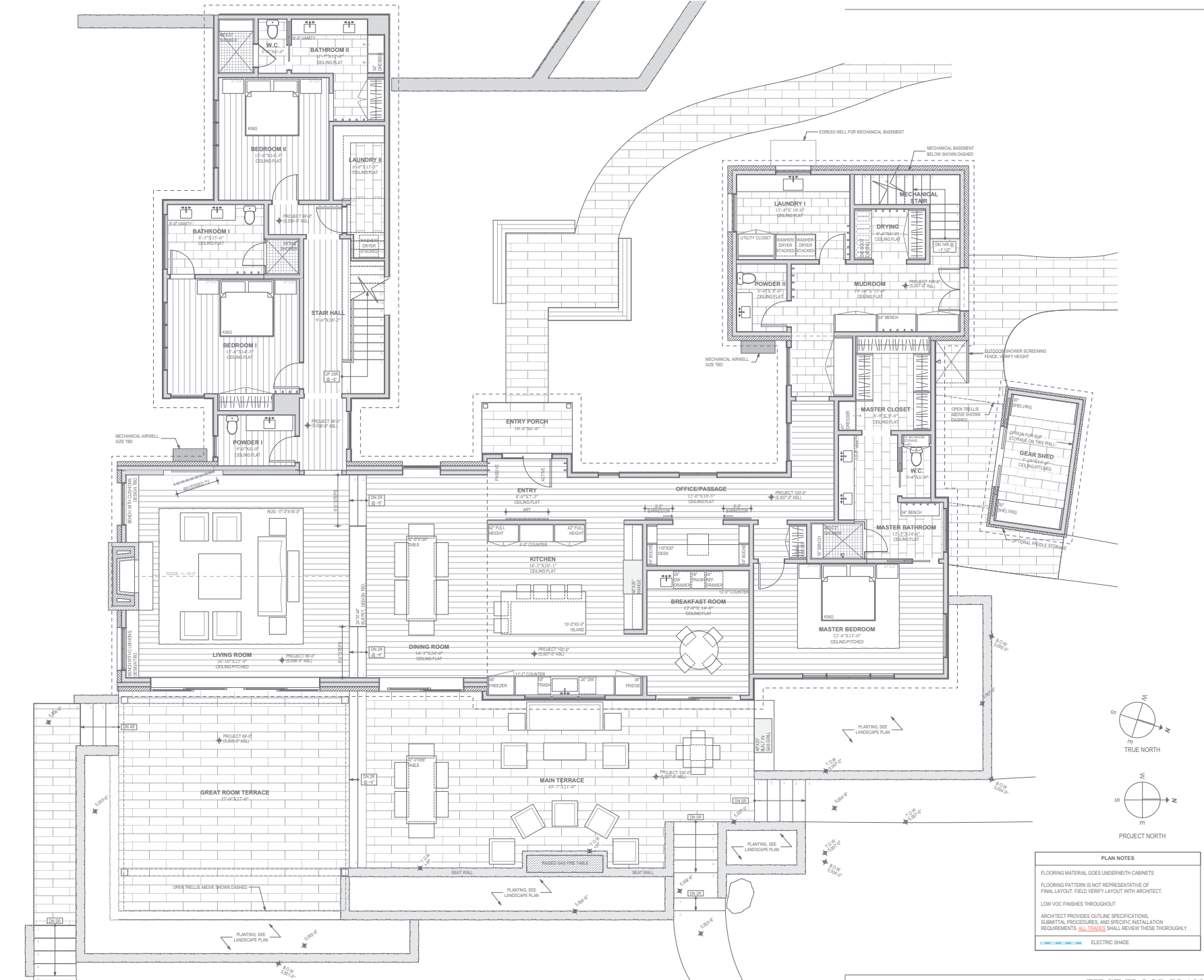
FILE DESIGN REVIEW SET

REVISIONS

NO. DATE DESCRIPTION

NO.	DATE	DESCRIPTION

PRINT DATE: Tuesday, October 26, 2021



1 PLAN: FIRST FLOOR
SCALE: 1/4" = 1'-0"

FIRST FLOOR PLAN

PLAN NOTES

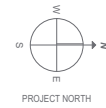
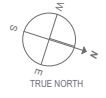
FLOORING MATERIAL GOES UNDERNEATH CABINETS

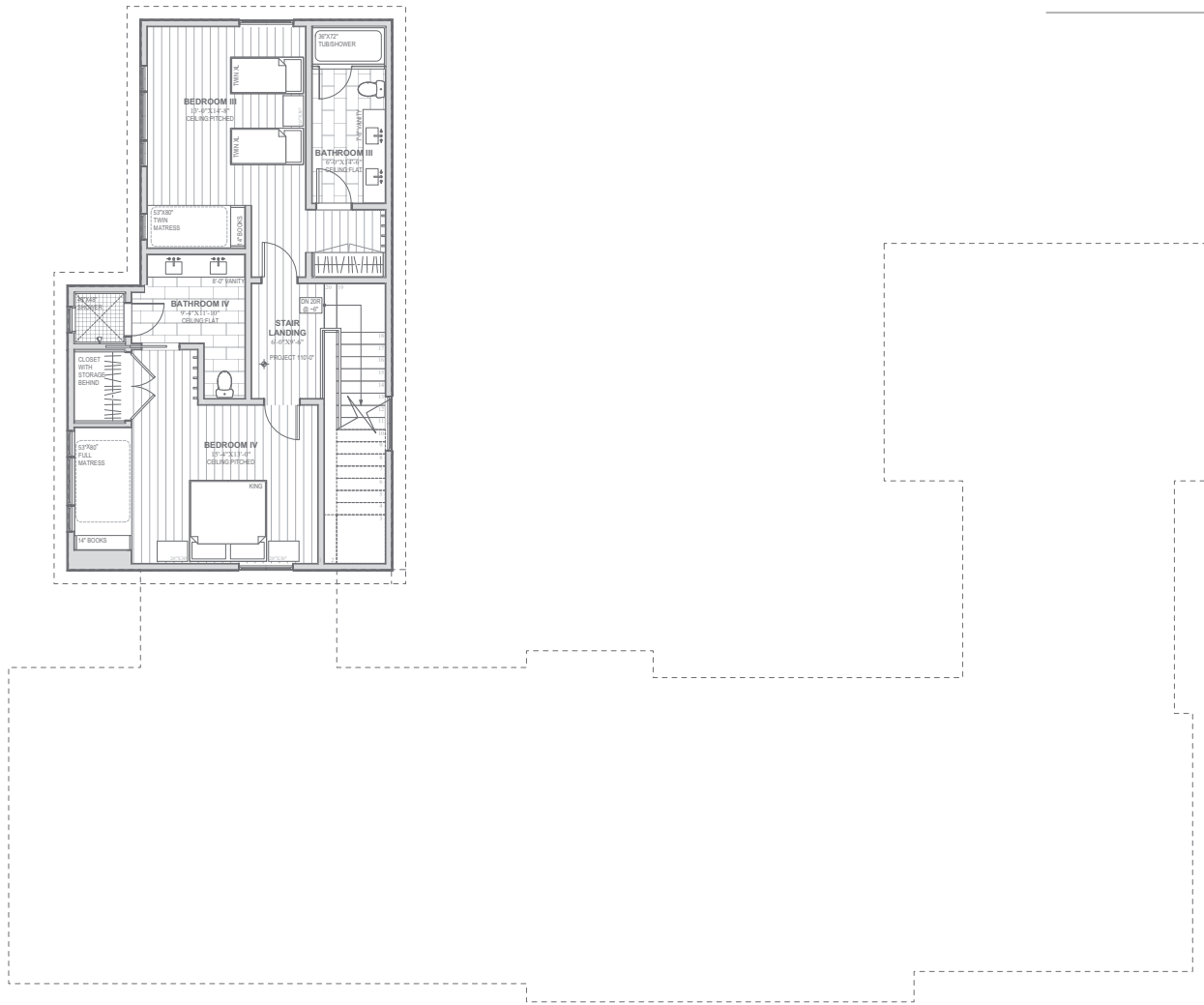
FLOORING PATTERN IS NOT REPRESENTATIVE OF FINAL LAYOUT. FIELD VERIFY LAYOUT WITH ARCHITECT.

LOW VOC FINISHES THROUGHOUT

ARCHITECT PROVIDES OUTLINE SPECIFICATIONS, SUBMITTAL PROCEDURES, AND SPECIFIC INSTALLATION REQUIREMENTS. **ALL TRADES** SHALL REVIEW THESE THOROUGHLY.

ELECTRIC SHADE

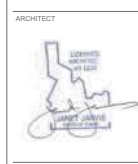




1 PLAN: SECOND FLOOR
SCALE: 1/4" = 1'-0"



PLAN NOTES	
FLOORING MATERIAL GOES UNDERNEATH CABINETS	
FLOORING PATTERN IS NOT REPRESENTATIVE OF FINAL LAYOUT. FIELD VERIFY LAYOUT WITH ARCHITECT.	
LOW VOC FINISHES THROUGHOUT	
ARCHITECT PROVIDES OUTLINE SPECIFICATIONS. SUBMITTAL PROCEDURES AND SPECIFIC INSTALLATION REQUIREMENTS. ALL TRADES SHALL REVIEW THESE THOROUGHLY.	
	ELECTRIC SHADE



ARCHITECT

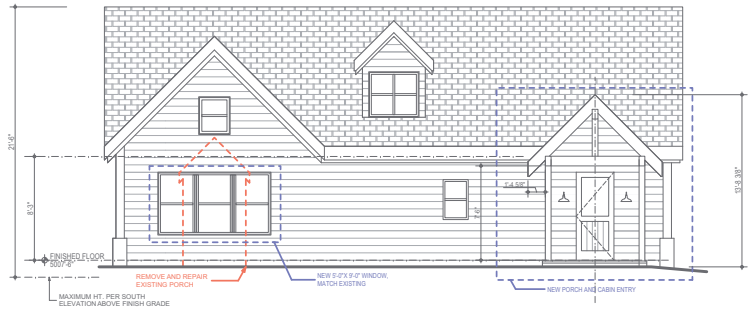
ENGINEER

DATE: 2021.10.26

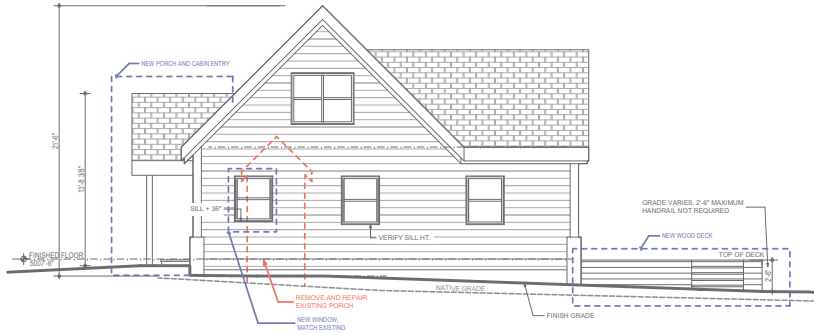
FILE: DESIGN REVIEW SET

REVISIONS

NO.	DATE	DESCRIPTION



1 ELEVATION: CABIN WEST
SCALE: 1/4" = 1'-0"



2 ELEVATION: CABIN SOUTH
SCALE: 1/4" = 1'-0"

ALLOWABLE LOT COVERAGE CALCULATOR
City of McCall

Lot Area in square feet: 5330 — Input lot area here
 Lot Coverage % Allowed: 75.83
 Lot Coverage Allowed: **4,221**

Proposed Lot Coverage

Footprint of existing structures: 1153 — Input if into boxes
 Footprint of proposed structures: 1263
 Total structure size: 2416 at 100% of actual = 2416 sf

Area of Existing Uncovered Decks, Patios, Walkways, Plaza, etc.: 218
 Area of New Uncovered Decks, Patios, Walkways, Plaza, etc.: 803
 Total decks, etc. size: 1026 at 50% of actual = 513 sf

Existing Driveways, surface parking, etc.: 0
 New Driveways, surface parking, etc.: 2543
 Total driveway size: 2543 at 20% of actual = 890.05 sf

Total Proposed Lot Coverage: 3821.05 sf

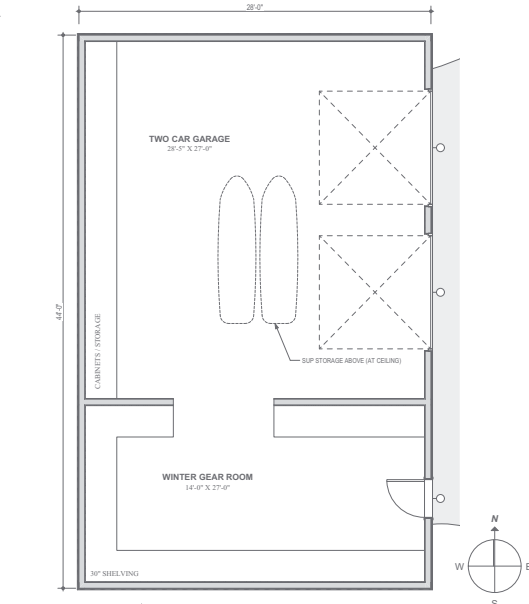
Proposed Building Size

Occupied space (existing): — Input if into boxes
 Unoccupied space (existing garage): —
 Covered exterior space (existing): —
 Occupied space (new): —
 Unoccupied space (new garage): 1212
 Covered exterior space (new): 14
 Total Proposed Building Size: 1268 sf

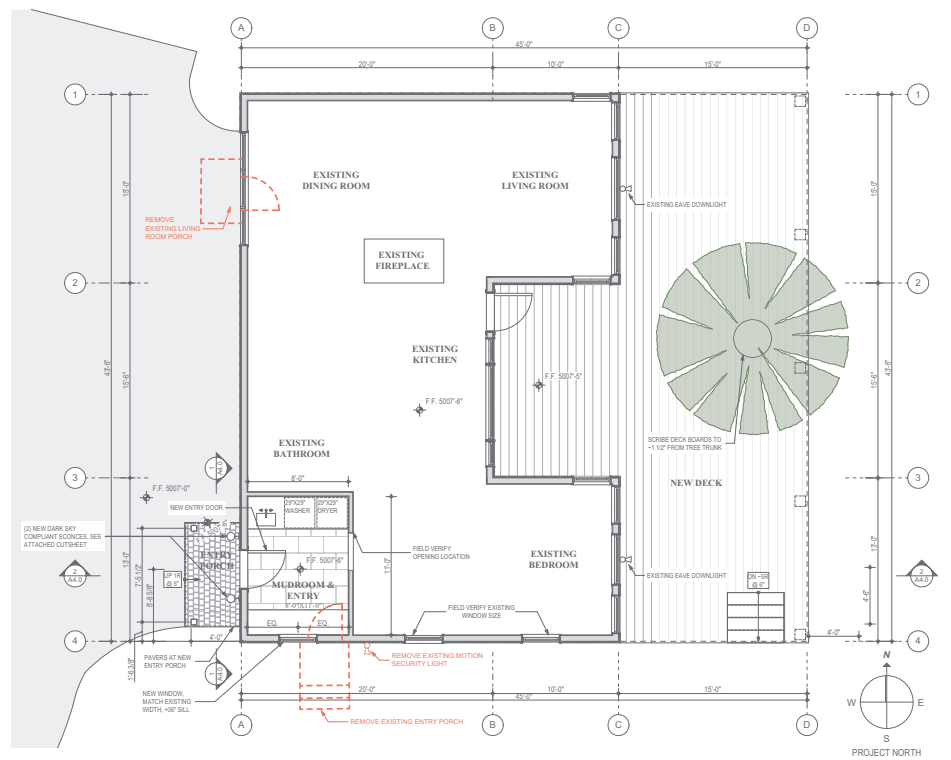
SIDEYARD SETBACK CALCULATOR
City of McCall

Lot Width: 31 — Input Lot Width here
 Minimum Side Setback: 8.65 8'-8"
 Both side setbacks must add up to: 18.2 18'-2"

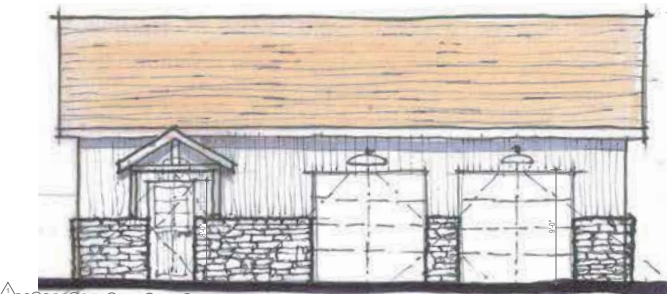
Note: Building may be no closer to side lot line than 1/2 the height of any building element. See city code.



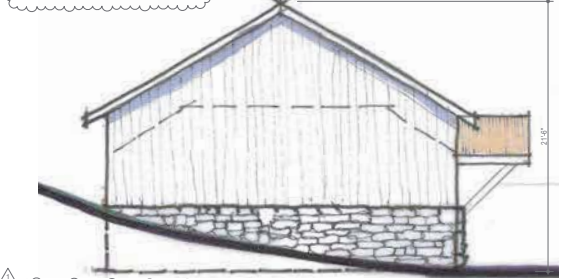
4 PLAN: GARAGE
SCALE: 1/4" = 1'-0"



3 PLAN: GUEST HOUSE
SCALE: 1/4" = 1'-0"



5 ELEVATION: GARAGE EAST
SCALE: 1/4" = 1'-0"



6 ELEVATION: GARAGE SOUTH
SCALE: 1/4" = 1'-0"

GUEST HOUSE & GARAGE INFORMATION

THE JARVIS GROUP
ARCHITECTS, P.A. PLLC

511 SUN VALLEY ROAD
POSTAL BOX 626
KETCHIKAN, IDAHO 83340
PHONE 208.726.4031 FAX 208.726.4037

IDAHO

JORDAN RESIDENCE
1415 WARREN WAGON ROAD

MCCALL

ARCHITECT

ENGINEER

DATE: 2021.10.26
 FILE: DESIGN REVIEW SET
 REVISIONS

NO.	DATE	DESCRIPTION
1	11.11.2021	GARAGE PLANS

PRINT DATE: Friday, November 12, 2021

A2.3



1 ELEVATION: SOUTH
SCALE: 1/4" = 1'-0"



2 ELEVATION: WEST (AT ENTRY)
SCALE: 1/4" = 1'-0"

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DRAWN: _____
DATE: 2021.10.26
FILE: DESIGN REVIEW SET

REVISIONS

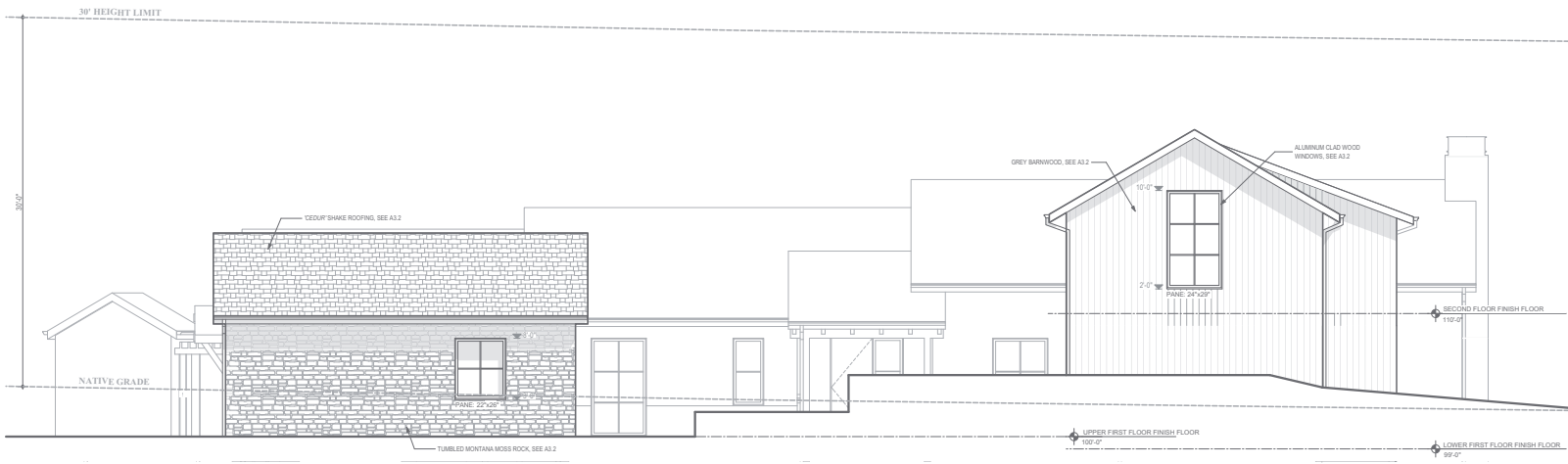
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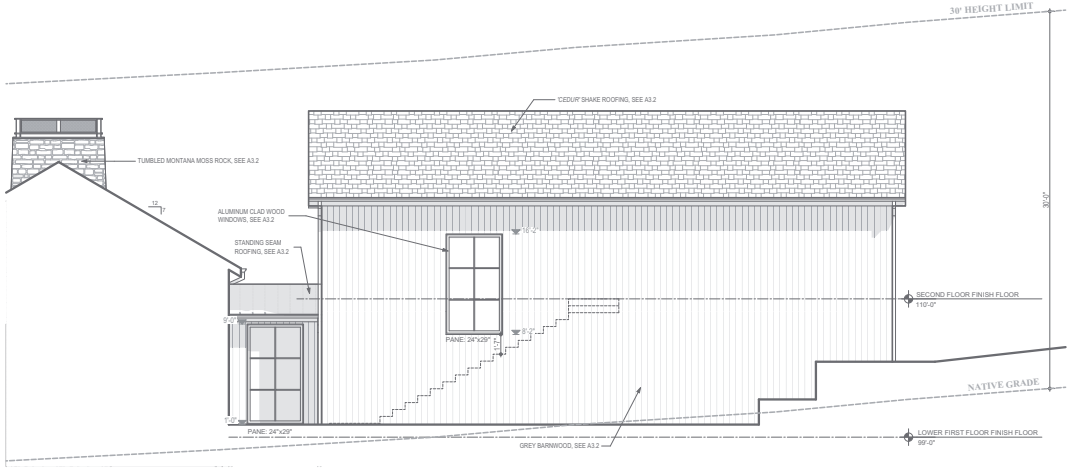
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REVISIONS
NO. DATE DESCRIPTION

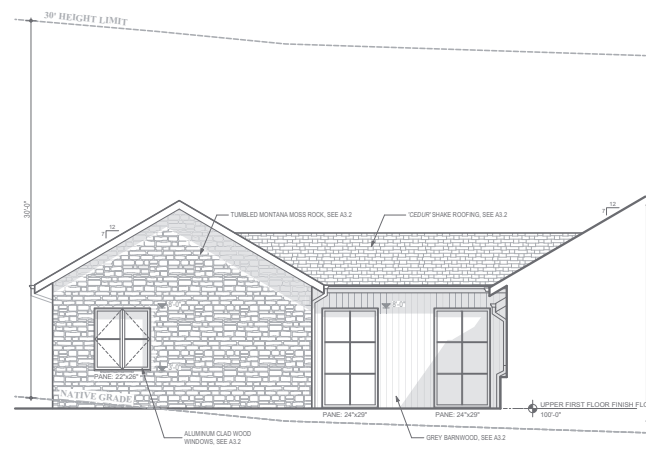
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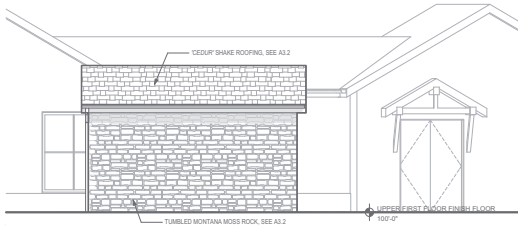
1 ELEVATION: WEST (FROM DRIVEWAY)
SCALE: 1/4" = 1'-0"



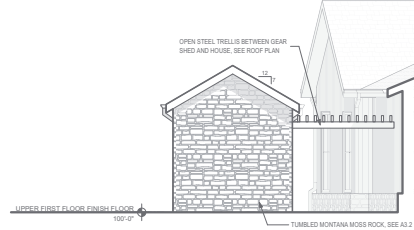
2 ELEVATION: NORTH (COURTYARD)
SCALE: 1/4" = 1'-0"



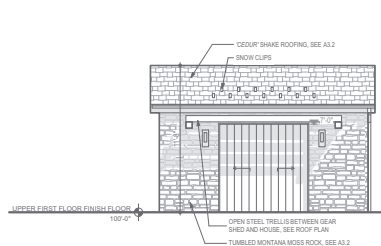
3 ELEVATION: SOUTH (COURTYARD)
SCALE: 1/4" = 1'-0"



4 ELEVATION: NORTH (GEAR SHED)
SCALE: 1/4" = 1'-0"



5 ELEVATION: WEST (GEAR SHED)
SCALE: 1/4" = 1'-0"



6 ELEVATION: SOUTH (GEAR SHED)
SCALE: 1/4" = 1'-0"



RENDERING OF PROJECT

JORDAN RESIDENCE
1415 WARREN WAGON ROAD

MCCALL

ARCHITECT

ENGINEER

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DATE 2021.10.26

FILE DESIGN REVIEW SET

REVISIONS

NO.	DATE	DESCRIPTION

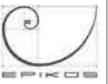
PRINT DATE: Tuesday, October 26, 2021

TREE PROTECTION PROTOCOL

- CONTRACTOR SHALL MEET WITH THE LANDSCAPE ARCHITECT OR CITY OF MCCALL ARBORIST PRIOR TO ANY EARTHWORK.
- CONTRACTOR SHALL FIRST INSTALL CONSTRUCTION FENCING AROUND THE CRITICAL ROOT ZONE (CRZ) OF ALL SIGNIFICANT TREES TO BE SAVED. AT A MINIMUM, THIS WOULD BE 8' FROM THE TRUNK. (SEE PLAN)
- CONSTRUCTION FENCING SHALL BE STRONG AND SECURED TO STURDY STAKES WITH WIRE OR EQUAL, AND HAVE SIGNAGE INDICATING THAT THESE TREES ARE TO BE PRESERVED AND PROTECTED.
- CONSTRUCTION FENCING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION.
- IF FENCING AROUND THE ROOT ZONE IS NOT AN OPTION, A THICK LAYER (12"-15") OF WOOD MULCH (WOOD CHIPS) CAN BE SPREAD ACROSS THE ROOT ZONE TO PREVENT SOIL COMPACTION DURING CONSTRUCTION.
- BRIDGING THE ROOT ZONE WITH SHEETS OF PLYWOOD MAY ALSO BE USED.
- PROTECT THE TRUNK OF THE TREE WITH THE TREE PROTECTION DETAIL INCLUDED ON THE DRAWINGS.
- NO VEHICLE TRAFFIC OR MATERIAL STORAGE SHALL OCCUR WITHIN THE CRZ.
- IF CONSTRUCTION MUST OCCUR WITHIN THE CRZ, THE LANDSCAPE ARCHITECT OR CITY ARBORIST SHALL BE PRESENT DURING CONSTRUCTION.
- IF DURING EXCAVATION ROOTS ARE ENCOUNTERED, THE LANDSCAPE ARCHITECT OR CITY OF MCCALL ARBORIST SHALL PERFORM A VISUAL INSPECTION AND DETERMINE THE BEST COURSE OF ACTION.
- ALL TREE ROOT PRUNING SHALL BE DONE WITH NEAT, CLEAN CUTS OF THE ROOTS WITH A SHARP TOOL AND NO TREATMENT (WOUND DRESSING) OF THE ENDS OF THE ROOTS SHALL BE PERFORMED.
- BRANCH TRIMMING SHALL ONLY BE DONE AT THE DIRECTION OF THE LANDSCAPE ARCHITECT OR CITY OF MCCALL ARBORIST.
- SUPPLEMENTAL WATERING OR IRRIGATION OF TREES SHOULD BE DONE THROUGHOUT THE CONSTRUCTION PROCESS. ITS BEST TO SET A SPRINKLER(S) NEAR THE TREE(S) AT LEAST ONCE A WEEK THROUGH THE CONSTRUCTION SEASON.

GRADING NOTES

- PROPOSED CONTOUR LINES SHOWN REFER TO FINISH GRADES.
- LANDSCAPE CONTRACTOR TO FINISH ALL PLANTING AREAS WITH A MINIMUM OF 4" TOPSOIL IN GRASS AREAS AND 12" OF TOPSOIL IN PLANTED BEDS. PLANTED BEDS ARE ALSO TO RECEIVE 1 1/2" DEPTH BARK MULCH. WASHED DRAIN ROCK TO BE FURNISHED TO A MINIMUM 4" DEPTH. HOLD FINISH GRADES (I.E. TOP OF MULCH) FOR LANDSCAPE AREAS 1" BELOW TOP OF ADJACENT PAVEMENTS, CURBS, ETC.
- PLACE BOULDERS TO RETAIN WHEREVER SLOPES EXCEED 3:1 ON FILL SLOPES AND 2:1 ON CUT SLOPES
- REFER TO STORMWATER MANAGEMENT PLAN FOR EROSION AND SEDIMENT CONTROL GUIDELINES
- DRIVEWAY GRADES SHALL BE SLOPED AWAY FROM THE GARAGE AT A MINIMUM SLOPE OF 2% AND MAXIMUM SLOPE OF 6% FOR A DISTANCE OF NO LESS THAN TEN FEET
- ALL SITE GRADING ADJACENT TO THE NEW RESIDENCE SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING AT A MINIMUM OF 1.5% IN HARDSCAPE AREAS AND 4% IN LANDSCAPE AREAS



EPIKOS
LAND PLANNING
+ ARCHITECTURE

MCCALL OFFICE
303 Colorado Street
P.O. Box 6490
McCall, Idaho 83638
Tel: 208.634.4540

www.EpiKosDesign.com

JORDAN RESIDENCE
1415 WARREN WAGON ROAD
MCCALL, ID Site Postcode/ZIP

Stamp:

CONCEPT

Revisions:

Date: 10/26/2021

Project No:

Drawn by: ES

Checked by: DP

Sheet:

L1.0

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PLANT LIST

SYMBOL

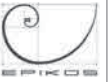
- O Deciduous tree such as Aspen, Maple, Ash, or Oak
- + Coniferous tree such as Engelmann Spruce, Western Larch, or Ponderosa Pine
- TS Tall shrub such as Red Twig Dogwood, or Serviceberry
- MS Medium shrub such as Ash Leaf Spiraea, Ivory Halo Dogwood, Thimbleberry, Ninebark, Rhododendron, Hydrangea, or Rose
- M Mugo Pine
- LS Low shrub such as Spiraea, Snowberry, Alpine Currant, Gooseberry, Native Rose, or Nepeta
- P Perennial such as daylily, Yarrow, Coneflower, Salvia, Lupine, Iris, Columbine, Ornamental Grasses, Hosta, or Daisy
- F Native Fern
- GC Ground cover such as Ajuga, Kinnikinnick, Sweet Woodruff, Wild Strawberry, Sedum, Creeping Thyme, Irish and/or Scotch Moss, Vinca Minor, Bishop's Weed, Bunchberry, or Dianthus
- .
- Native grass blend such as Pinegrass, Fine Fescue, Buffalo Grass, and/or Sheep Fescue

FIRE HAZARD MITIGATION

1. The thirty foot wide zone around the building shall be designed and maintained as a Fire Hazard Mitigation Zone that provides space for fire suppression equipment in case of emergency.
2. Within the thirty foot wide zone:
 - a) grasses shall be kept at or below six inches in height
 - b) shrubs and trees are thinned to a minimum spacing of ten feet or more
 - c) conifers sixteen feet and over in height are limbed up from the ground to a height of eight feet and no more than one-half the total crown height
 - d) dead and down wood is removed
 - e) no tree shall be permitted to overhang a chimney
3. All slash will be removed, converted to mulch, or burned within twelve months of its creation.
4. All dead vegetation should be removed from the homesite.
5. New trees which will mature to a tall height should be planted at least 10' from the roof fascia.
6. The 30' Fire Hazard Mitigation Zone should be irrigated with an automatic irrigation system.
7. Plants contiguous to building structures should be irrigated with an automatic irrigation system.
8. All roofs and gutters should be maintained free of leaves and needles.

GENERAL NOTES

1. Contractor shall verify existing conditions and grades prior to start of construction.
2. The extent of existing underground utilities is not known. Contractor shall field verify existing grades, utilities and conditions, and notify the landscape designer of any discrepancies before starting work, making modifications as directed by the landscape designer. Contractor will be required to complete the work of this project according to the proposed drawings and details. If conflicts arise in the field, contact landscape designer for resolution. Contractor shall take responsibility for any costs incurred due to damage of said utilities.
3. These drawings do not specify safety materials or equipment, methods or sequencing to protect persons and property. It shall be the contractor's sole responsibility to direct and implement safety operations and procedures to protect the owner, landscape designer, subcontractors, the public, and others.
4. If conflicts arise between size of areas and plans, contractor is to contact landscape designer for resolution. Failure to make such conflicts known to the landscape designer will result in contractor's liability to relocate materials.
5. Do not willfully proceed with construction as designed when it is obvious that unknown obstructions and/or grade differences exist that may not have been known during design. Such conditions shall be immediately brought to the attention of the landscape designer. The contractor shall assume full responsibility for all necessary revisions due to failure of such notification.
6. Contractor shall be responsible for any coordination with subcontractors or other general contractors as required to accomplish construction operations.
7. Contractor shall guarantee all plant material for (1) one full year from time of planting. All plants shall be growing vigorously and shall be free from any diseases or deformities.



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JORDAN RESIDENCE
1415 WARREN WAGON ROAD
MCCALL, ID Site Postcode/ZIP

Stamp:

CONCEPT

Revisions:

Date: 10/26/2021

Project No:

Drawn by: ES

Checked by: DP

Sheet:

L2.0

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STORMWATER APPLICATION
City of McCall

Fill in all information. Submit one copy of signed application and three copies of Stormwater Management Plan/Report to the City Engineer.

1. Project Name: Jordan Residence
Location: 1415 Warren Wagon Road
2. Owner's Name: Leonard Jordan
Street: 2500 Magnolia Blvd. W. City: Seattle
State: WA Zip Code: 98199 Phone: (206) 499-4648
3. Project Description: The proposed project consists of the demolition of the existing garage, gravel driveways, concrete pad, and paver pathways. The existing residence will be relocated on the property to serve as a quest house with a new deck. A new residence will be constructed along with an asphalt perimeter of the new residence and leading to the existing beach area.
 - a. Total property area, in acres. 1.12 acres
 - b. Proposed impervious surface (asphalt, rooftop, concrete, sidewalk, etc.) in square feet. There will be approximately 0.35 acres (15,046.8 S.F.) of impervious surface at build-out. Currently there is 0.06 acres (2,504.6 S.F.) of impervious on-site.
 - c. Describe existing vegetation present on site. The existing vegetation on-site is a mix of small to large diameter coniferous trees with a few deciduous trees/shrubs in landscapes areas and grassy open space leading down to the shoreline.
 - d. Start date of construction. Spring/Summer 2022
 - e. Estimated length of time to complete improvements. 12 - 18 months
4. Stormwater Management Plan/Report attached? Yes No
5. Circle the section of the Stormwater Management Plan/Report Checklist which are applicable to project.
A X B X C X D X E X F X
6. Party responsible for operation and maintenance of project, including maintenance of temporary and permanent Best Management Practices:

<u>Todd Allen Construction</u>	<u>Contractor</u>	<u>Todd Allen</u>	<u>11-10-21</u>
Name	Title	Signature	Date
<u>355 Deinhard Lane, McCall, Idaho 83638</u>		<u>(208) 315-0473</u>	<u>(208) 315-0473</u>
Address		Daytime Phone	After Hours Phone

Do not write below this line.

This Stormwater Management Plan/Report is:

Approved: _____

Not Approved: _____

Approved, with conditions: _____

By The City of McCall

_____	_____	_____	_____
Representative	Title	Signature	Date



CRESTLINE ENGINEERS, INC.
 CIVIL ENGINEERING CONSULTANTS
 323 DEINHARD LANE, SUITE C
 PO BOX 2330
 McCALL, IDAHO 83638
 208.634.4140 · 208.634-4146 FAX

PROJECT: Jordan Residence

CLIENT: The Jarvis Group Architects, AIA, PLLC

JOB NO.: 21062 **DATE:** November 10, 2021

BY: RFP

REVISION DATE: _____

RE: Jordan Residence - Stormwater Calculations

Drainage Area Calculations

Drainage Areas	(ft²)	(Acres)
Parcel A	32,317.8	0.74
Parcel B	16,340.0	0.38
Development Area	48,657.9	1.12

Pre-Development: Native Area Surfaces	(ft²)	(Acres)	(%)
Woods (CN = 73)	48,657.9	1.12	100.00%
	48,657.9	1.12	100.00%

Total Impervious Surface Area = 0.0 0.00 0.00%

Post Development: Development Area Surfaces (At Build-out)	(ft²)	(Acres)	(%)
Building Roofs (CN = 98)	7,527.3	0.17	15.47%
Asphalt Driveway (CN = 98)	4,771.8	0.11	9.81%
Grouted Stone Patio/Walks/Terrace (CN = 98)	2,747.7	0.06	5.65%
Wood Deck/Dock (CN = 89)	849.3	0.02	1.75%
Gravel Pathways (CN = 89)	1,954.5	0.04	4.02%
Open Space (CN = 79)	30,807.3	0.71	63.31%
	48,657.9	1.12	100.00%

Total Impervious Surface Area = 15,046.8 0.35 30.92%

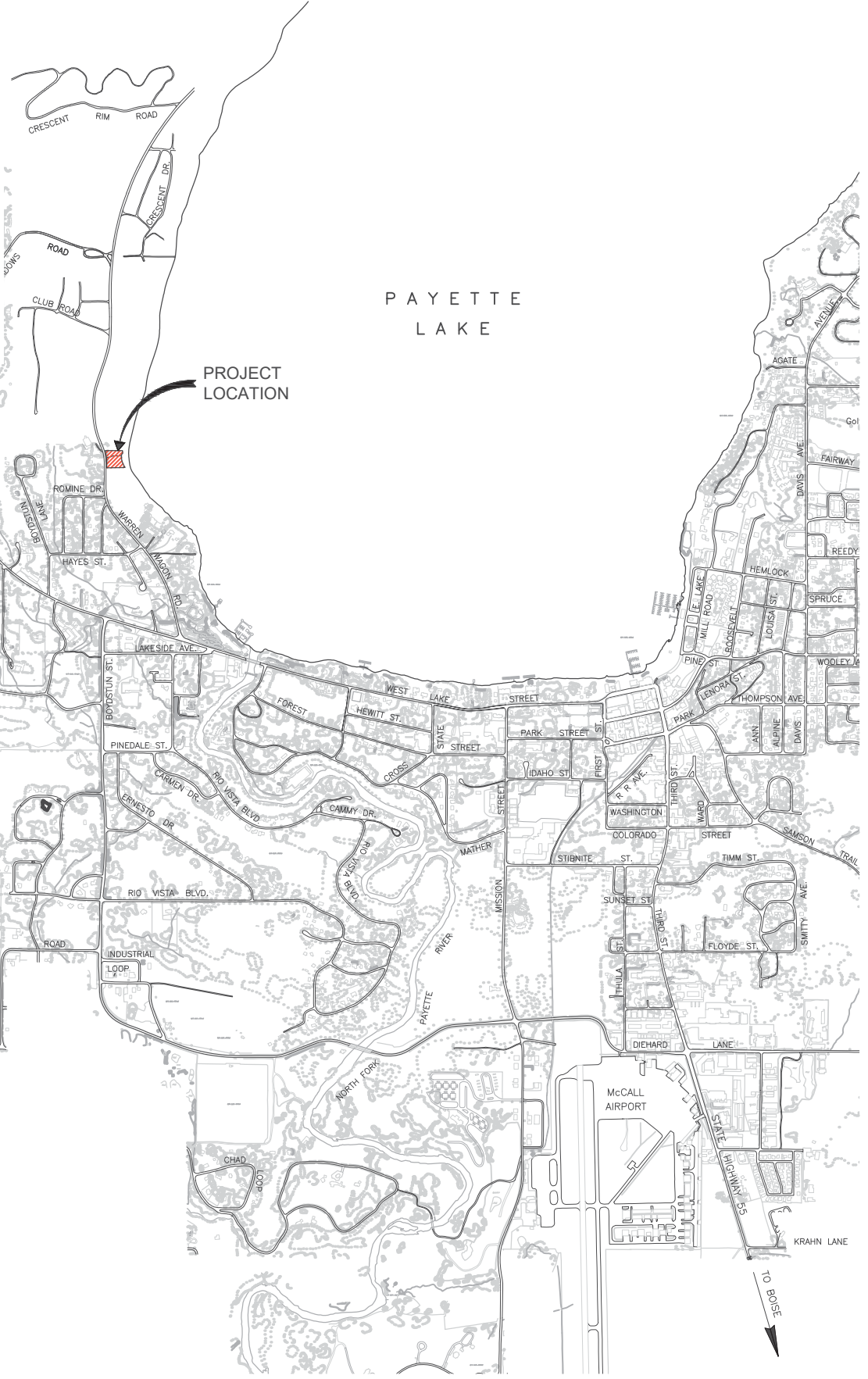
Drainage Area Flow Paths

Pre-Development Flow Path:	Length (ft)	Elevation Change (ft)	Slope (%)
1. Sheet Flow (n = 0.40, Woods - Light Underbrush)	206.6	21.0	10.16%

Total Length/Average Slope = 206.6 21.0 10.16%

Post Development Flow Path:	Length (ft)	Elevation Change (ft)	Slope (%)
1. Sheet Flow (n = 0.24, Grass - Dense Grasses)	20.7	4.0	19.36%
2. Shallow Concentrated Flow (Paved)	100.7	4.5	4.47%
3. Shallow Concentrated Flow (Unpaved)	143.9	13.5	9.38%

Total Length/Average Slope = 265.3 20.0 7.54%



NORTH
SCALE: 1" = 2000'

CRESTLINE ENGINEERS

323 DEINHARD LANE, SUITE C · PO BOX 2330
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JORDAN RESIDENCE VICINITY MAP

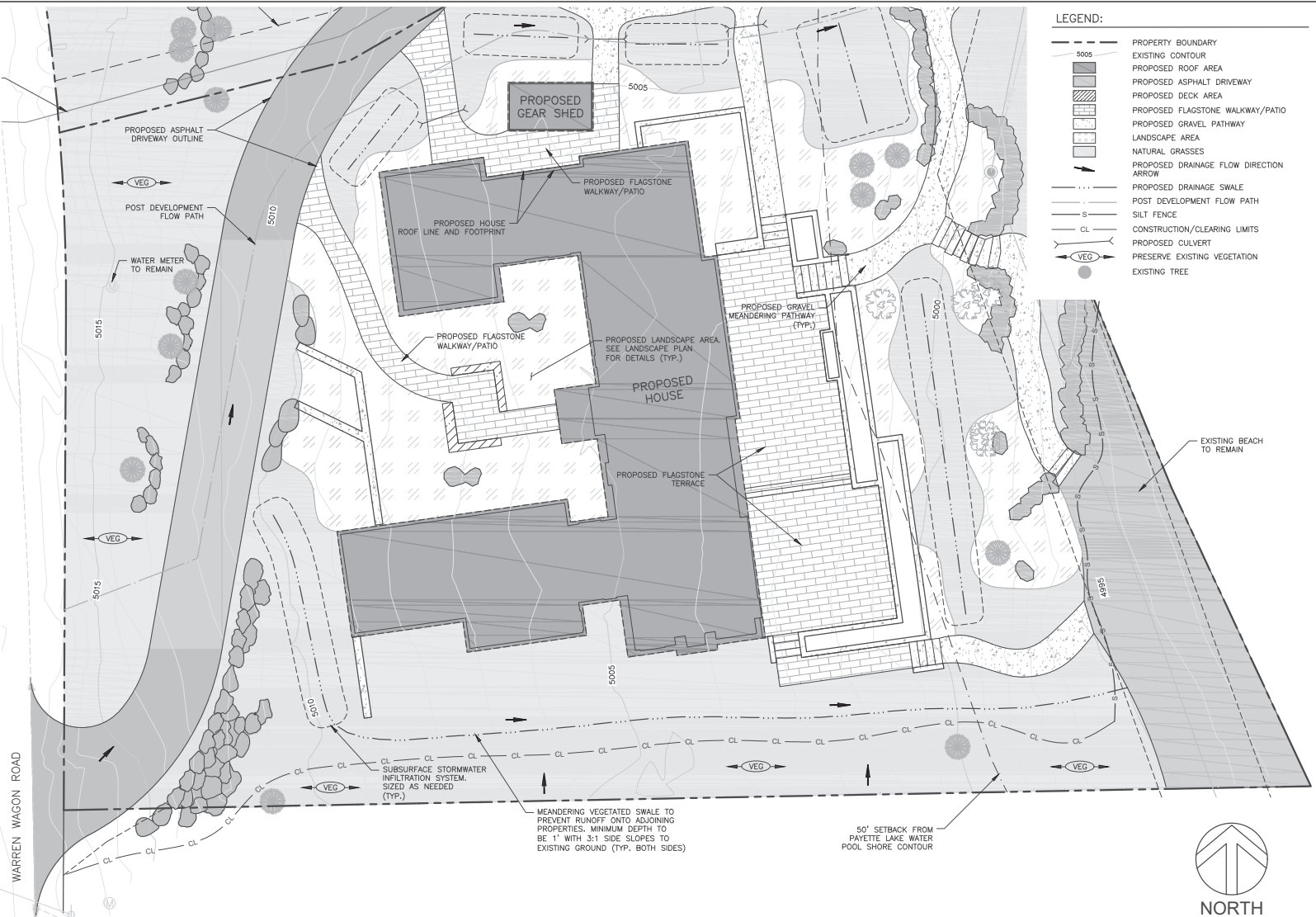
PROJECT	21062	DRAWN	FIGURE NO.
DATE	11/11/2021	RFP	1 OF 1

NOTES:

- EXISTING TOPOGRAPHY AND PROPERTY BOUNDARIES AS SHOWN ON THIS PLAN ARE BASED UPON SURVEY DATA PROVIDED BY DUNN LAND SURVEYS, INC. PROPOSED IMPROVEMENTS AS SHOWN ON THE PLAN ARE BASED UPON DRAWINGS PROVIDED BY THE JARVIS GROUP ARCHITECTS, IA, PLIC.
- THE EXISTING SITE INFORMATION IS PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR'S CONSTRUCTION SURVEY PRIOR TO THE START OF ANY PROJECT CONSTRUCTION. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF THE EXISTENCE OF OBJECTS OR UTILITIES ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THESE DRAWINGS.
- REFER TO THE "STATE OF IDAHO, CATALOG OF STORMWATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES" FOR FURTHER DETAILS ON BMP IMPLEMENTATION AND INSTALLATION.
- ALL EROSION AND SEDIMENT CONTROL BMP'S SHALL BE INSTALLED PRIOR TO THE START OF ANY PROJECT CONSTRUCTION OR EARTH DISTURBING ACTIVITIES AND SHOULD REMAIN IN PLACE UNTIL ALL DISTURBED/EXPOSED AREAS HAVE BEEN STABILIZED AND/OR REVEGETATED.
- THE OWNER AND/OR THEIR SELECTED CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL BMP'S IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REQUIREMENTS.
- THE IMPLEMENTATION OF THESE EROSION AND SEDIMENT CONTROL MEASURES INCLUDING INSTALLATION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THIS PLAN IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL PROJECT CONSTRUCTION IS COMPLETED AND APPROVED BY THE OWNER. THE OWNER SHALL BE RESPONSIBLE FOR ALL MAINTENANCE AFTER THE PROJECT IS APPROVED.
- WATLES MAY BE USED IN PLACE OF SILT FENCE WHERE DETERMINED APPROPRIATE. SILT FENCE HAS BEEN SHOWN ON THE PROPERTY LINES IN SOME AREAS TO PREVENT ENCROACHMENT ONTO NEIGHBORING PROPERTIES.
- WORK ACTIVITIES SHALL TAKE PLACE WITHIN THE CLEARING LIMITS AS SHOWN ON THIS PLAN. CONTRACTOR SHALL PRESERVE NATURAL VEGETATION OUTSIDE OF CLEARING LIMITS.
- STAGING AREA(S) TO BE LOCATED BY CONTRACTOR ALONG WITH PORTABLE TOILETS, GARBAGE RECEPTACLES, CONCRETE WASHOUT, AND ALL OTHER CONTRACTOR FACILITIES.
- ALL SITE GRADING ADJACENT TO THE NEW RESIDENCE SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING AT A MINIMUM OF 1.5% IN HARDSCAPE AREAS AND 4% IN LANDSCAPE AREAS.
- DRIVEWAY GRADES SHALL BE SLOPED AWAY FROM THE GARAGE AT A MINIMUM SLOPE OF 2% AND A MAXIMUM SLOPE OF 6% FOR A DISTANCE OF NO LESS THAN TEN (10) FEET. GRADING OF THE DRIVEWAY SHALL BE IN ACCORDANCE WITH THE DIRECTION OF THE DRAINAGE FLOW DIRECTION ARROWS AS SPECIFIED IN THE STORMWATER MANAGEMENT PLAN.
- AREAS BETWEEN NEW RESIDENCE AND PROPERTY BOUNDARIES SHALL BE SLOPED TO INSURE RUNOFF IS KEPT ON-SITE. SWALES SHALL BE CONSTRUCTED ADJACENT TO NEAR SIDE PROPERTY LINES TO PREVENT RUNOFF FROM FLOWING ONTO ADJOINING PROPERTIES. THESE SWALES ARE INTENDED TO BE FIELD FIT AND MEANDERED AROUND EXISTING VEGETATION AND SITE FEATURES AS NECESSARY.
- REVEGETATION AND STABILIZATION OF ALL DISTURBED PROJECT AREAS SHALL BE IN ACCORDANCE WITH THE PROJECTS LANDSCAPE DESIGN. IF A LANDSCAPE DESIGN/PLAN IS NOT AVAILABLE, DISTURBED AREAS SHALL BE REVEGETATED WITH A GRASS MIXTURE NATIVE TO THAT AREA.
- REFER TO TREE PROTECTION AND GRADING CONCEPT PLAN FOR ADDITIONAL DETAILS ON FINISH GRADING.

LEGEND:

- PROPERTY BOUNDARY
- EXISTING CONTOUR
- PROPOSED ROOF AREA
- PROPOSED ASPHALT DRIVEWAY
- PROPOSED DECK AREA
- PROPOSED FLAGSTONE WALKWAY/PATIO
- PROPOSED GRAVEL PATHWAY
- LANDSCAPE AREA
- NATURAL GRASSES
- PROPOSED DRAINAGE FLOW DIRECTION ARROW
- PROPOSED DRAINAGE SWALE
- POST DEVELOPMENT FLOW PATH
- SILT FENCE
- CONSTRUCTION/CLEARING LIMITS
- PROPOSED CULVERT
- PRESERVE EXISTING VEGETATION
- EXISTING TREE



File: M:\01\T\T\Stormwater\21082\04\DWG\21082-04-Stormwater_C-2_1112.dwg (R) Date: 11/11/2021 12:25 PM
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NO.	REVISION	BY	DATE	DESIGN
				RFPI/GTT
				DRAWN
				RFP
				CHECKED
				BEJ
				APPROVED
				GTT

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JORDAN RESIDENCE
McCALL, IDAHO
 PRELIMINARY STORMWATER MANAGEMENT PLAN - 1

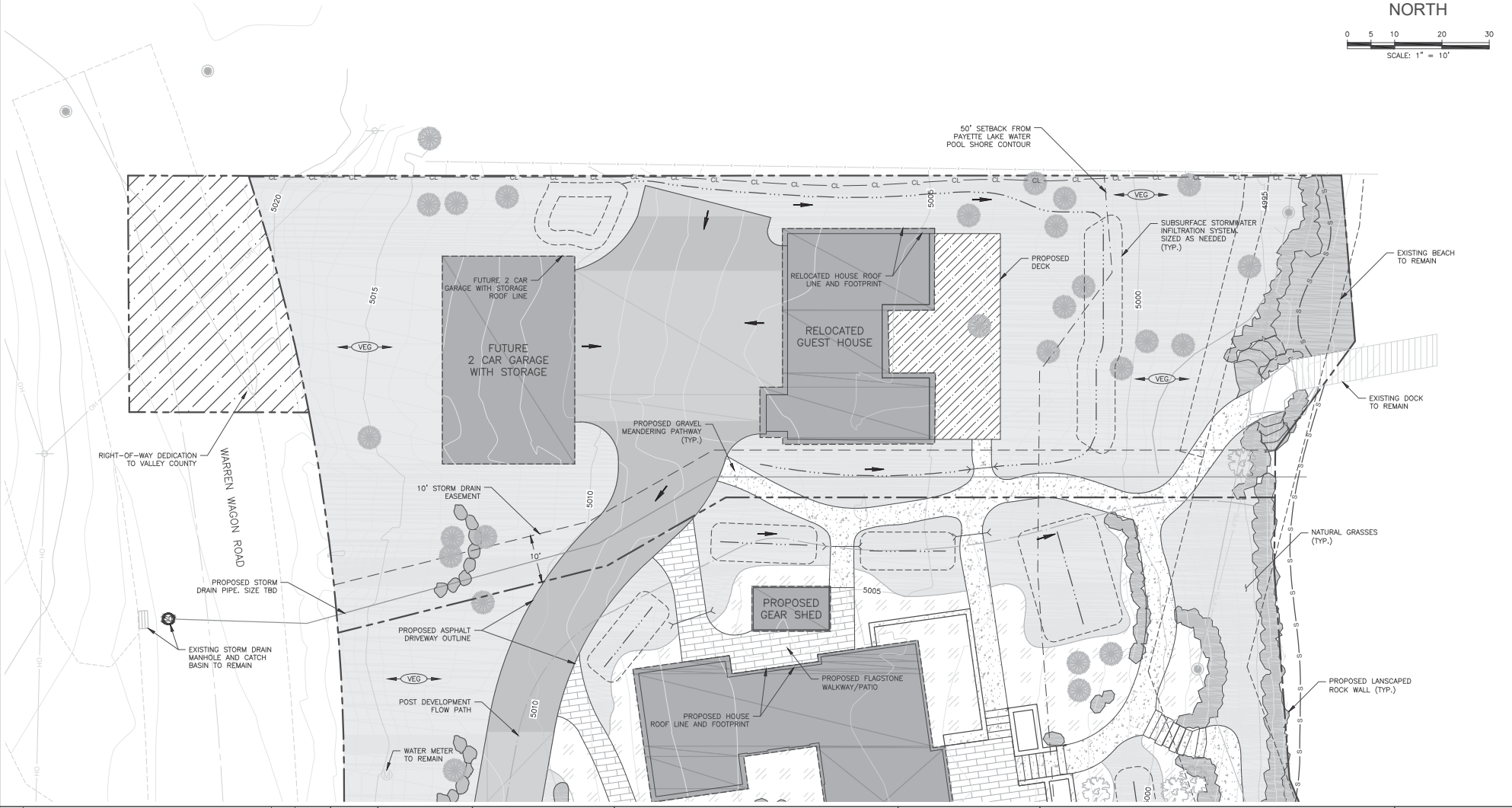
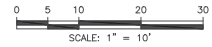
VERIFY SCALE	
BAR IS ONE INCH ON FULL SIZE DRAWING 1"	
PROJECT	21082
DATE	11/11/2021
DRAWING NO.	SHEET NO.
C-2	2 OF 3

NOTES:

1. REFER TO DRAWING NO. C-2, SHEET 2 FOR STORMWATER MANAGEMENT NOTES, LEGEND AND SYMBOLS.



NORTH



NO.	REVISION	BY	DATE	DESIGN
				RFP/GTT
				DRAWN
				RFP
				CHECKED
				BEJ
				APPROVED
				GTT

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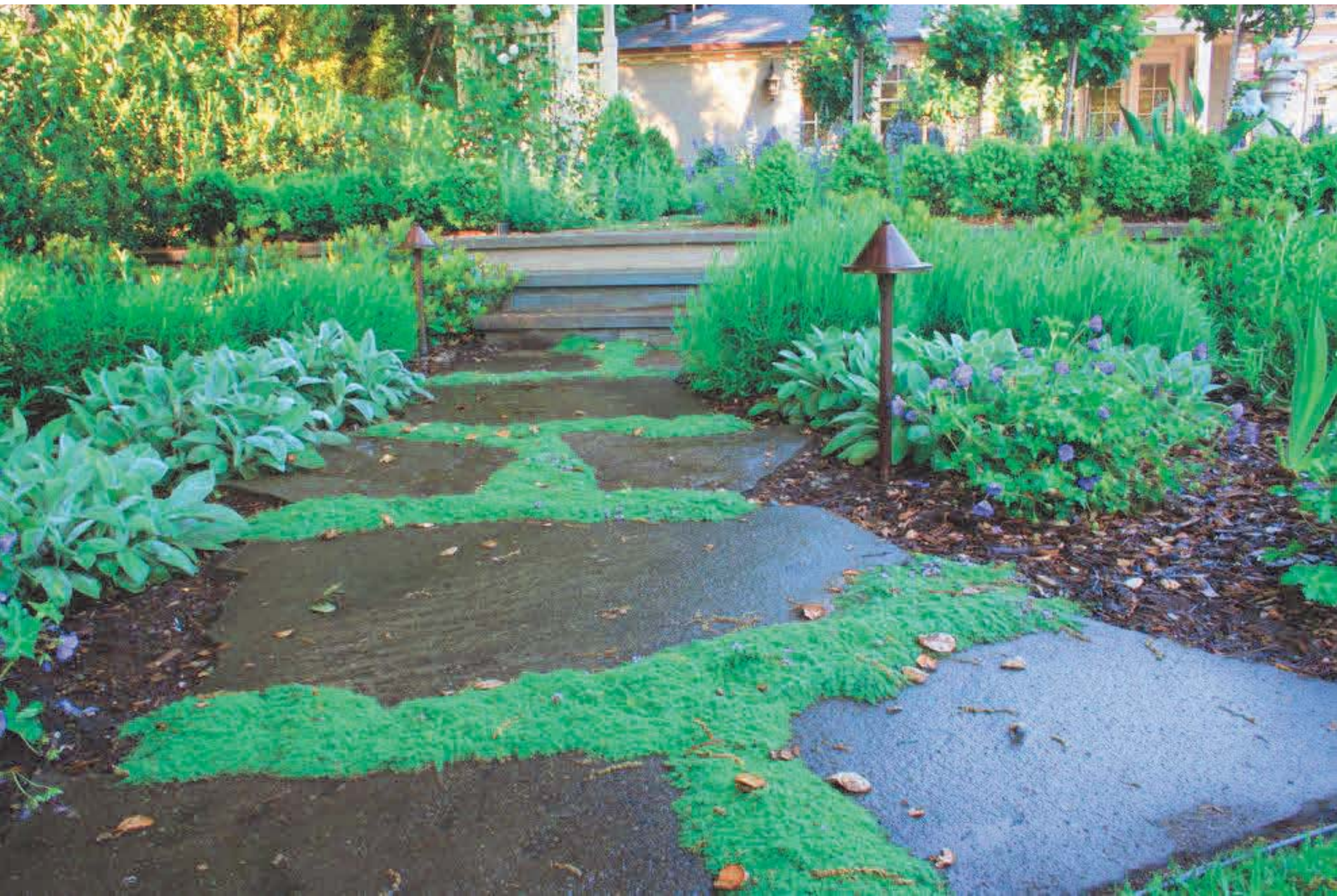
JORDAN RESIDENCE
McCALL, IDAHO
PRELIMINARY STORMWATER MANAGEMENT PLAN - 2

VERIFY SCALE	
BAR IS ONE INCH ON FULL SIZE DRAWING 1"	
PROJECT	21062
DATE	11/11/2021
DRAWING NO.	SHEET NO.
C-3	3 OF 3

Path: M:\0117\JordanResidence\21062\001\04\Stormwater\211112.dwg File Name: 21062_Stormwater_C-3_211112.dwg Plot Date: 11/10/2021 12:24 PM user











From: [Laura Shealy](#)
To: [Brian Parker](#)
Subject: Re: City of McCall Request for Comment - DR-21-39 - 1415 Warren Wagon Road
Date: Monday, November 8, 2021 12:06:08 PM
Attachments: [image001.png](#)
[image002.png](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Brian,

The Jordan residence slopes gradually but directly down to Payette Lake. It is good that they are planning for native plants in the landscaping. Fertilizers should be prohibited in this waterfront environ due to potential contamination of the lake. A large construction site like this home needs all erosion control planning approved ahead of construction. The Lake-A-Syst is an excellent guide for homeowners and builders and covers all aspects of erosion mitigation techniques. The City of McCall has a copy of the Lake-A-Syst. Thanks, Laura Shealy Big Payette Lake Water Quality Council

On Fri, Nov 5, 2021 at 11:24 AM Brian Parker <bparker@mccall.id.us> wrote:

All,

Please provide comment on the attached application by November 19, 2021.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us

Blog: mccallcitysource.com

Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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From: [Kurt Wolf](#)
To: [Brian Parker](#)
Subject: RE: City of McCall Request for Comment - DR-21-39 - 1415 Warren Wagon Road
Date: Friday, November 26, 2021 12:58:59 PM
Attachments: [image001.png](#)
[image002.png](#)

Brian,

I know my comments are late and if there is still time I can get the tree committee to comment following their December 07 meeting.

I visited this site with the design team last summer and given the significant amount of trees between Warren Wagon Road and the new structures and driveway. I would be happy to make further consultations as the project takes shape and once the outlined tree protection measures are in place. At this time, I have no concerns and would be happy to consult as the project takes shape.

Thank You,

Kurt K Wolf
Parks & Recreation Director, City Arborist
City of McCall – Parks & Recreation Dept.
ISA Certified Arborist (PN-7353A) – ISA Tree Risk Assessment Qualification
American Society of Landscape Architects (ASLA)
www.mccallparksandrec.com
216 E. Park Street
McCall, Idaho 83638
Office: 208-634-8967
Cell: 208-315-0063
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)

From: Brian Parker <bparker@mccall.id.us>
Sent: Friday, November 5, 2021 11:24 AM
To: Anette Spickard <aspickard@mccall.id.us>; Bob Fodrea <bob@fodrealandgroup.com>; Chip Bowers <chip@bowerslandsurveys.com>; Chris Curtin <ccurtin@mccall.id.us>; Cynda Herrick <cherrick@co.valley.id.us>; Dale Caza <dcaza@plrwsd.org>; David Simmonds <dsimmonds50@gmail.com>; Delta James <djames@mccall.id.us>; Garrett de Jong (garrett@mccallfire.com) <garrett@mccallfire.com>; Gene Trantham <gtrantham@mccall.id.us>; IDL Jurisdictional Inbox <IDL_jurisdictional@idl.idaho.gov>; ITD Development Services <D3Development.Services@itd.idaho.gov>; ITD District 3 Permits <ITDD3Permits@itd.idaho.gov>; Jasen King, IDL <jking@idl.idaho.gov>; Jeff Bateman <jbateman@plrwsd.org>; jennifer.schildgen@itd.idaho.gov; John Powell <jpowell@mccall.id.us>; Jordan Messner <jordan.messner@idfg.idaho.gov>; jthrower@gmail.com; Kurt Wolf <kwolf@mccall.id.us>; Lance Holloway, DEQ <lance.holloway@deq.idaho.gov>; Laura Shealy BPLWQAC <idchik5@gmail.com>; Laurie Frederick, Valley Co Cartographer <lfr frederick@co.valley.id.us>; Levi Brinkley <lbrinkley@mccall.id.us>; Linda Stokes <lstokes@mccall.id.us>; Lori Hunter (lhunter@co.valley.id.us) <lhunter@co.valley.id.us>; Lorraine Brush <lbrush@plrwsd.org>; Mark Wasdahl, ITD <mark.wasdahl@itd.idaho.gov>; Meredith Todd <mtodd@mccall.id.us>; Michelle Groenevelt <mgroenevelt@mccall.id.us>; Mike Reno <mreno@cdh.idaho.gov>; Morgan Stroud

<mstroud@mccall.id.us>; Nathan Stewart <nstewart@mccall.id.us>; Regan Berkley <regan.berkley@idfg.idaho.gov>; Sabrina Sims <ssims@mccall.id.us>; Sarah Arjona <Sarah.Arjona@itd.idaho.gov>; Scott Corkill, IDL <scorkill@idl.idaho.gov>; Sheri Staley - Idaho Power <sstaley@idahopower.com>; Steve Moser, Idaho Power <smoser@idahopower.com>; Valley County Road Dept <roaddept@co.valley.id.us>

Subject: City of McCall Request for Comment - DR-21-39 - 1415 Warren Wagon Road

All,

Please provide comment on the attached application by November 19, 2021.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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From: [Morgan Stroud](#)
To: toddallenconstruction@gmail.com
Cc: [Brian Parker](#); [Meredith Todd](#)
Subject: DR21-39 - 1415 Warren Wagon Rd - Engineering Review #1
Date: Monday, November 22, 2021 11:08:33 AM
Attachments: [image001.png](#)

The Public Works Department has reviewed the documents submitted for review for **DR21-39** and have the following comments and concerns.

1. There is currently a Record of Survey for these two parcels that was never finalized. The finalization of that process will need to be completed before the site work and building can occur.
2. An access easement should be provided to the garage and relocated cabin through the new parcel that the new home will be located on.
3. 12 foot snow storage and utility easement should be declared along Warren Wagon Road in-lieu of additional right-of-way dedication because Warren Wagon Road is identified to only have around 45.5 feet of existing right-of-way and the standard width for a major collector road is 70 feet.
4. Thank you for submitting a draft of a stormwater drainage report. It appears that the project will likely be able to comply with the City's Drainage Management Guideline's, subject to the following:
 - a. Identifying the upstream flows and volumes which are tributary to the project site and more specifically to the storm drain that is being proposed to be relocated with this project. These values should be used to help determine that the pipe sizing is sufficient and will not cause overflow issues upstream in the storm drain that is located within the Warren Wagon right-of-way.
 - b. It appears that there is an infiltration trench proposed parallel to the lake, however, the preliminary grading does not show that the runoff from the swales will enter it. Adjusting the grading and extending the trench to the south may be needed to capture runoff from the front of the home.
 - c. Finalizing the stormwater drainage report to include the dialog that explains the stormwater management plan, detention basin areas, pipe sizing of the storm drain, and appropriate hydrographs and calculations.
5. A detailed sitework and grading plan with spot elevations shall be provided for engineering review and approval, and incorporated with the architectural plans for construction. Temporary and permanent stormwater BMPs, as contemplated in the Stormwater Report shall be shown on the grading and drainage plan.
6. There will need to be two to three water meters for this project. The third meter is dependent upon if there will be habitable space provided in the new standalone three car garage. There is required to be a minimum of one water meter for each lot. Please show on your site plan where the meters will be located and please use the City of McCall's Standard Details.
7. Depending on the water demands, upsizing of the service line may be required. Providing information of the proposed water usage for each lot and each habitable building will be key in determining if upsizing will be required.
8. One of the site plans should include the location of the watermain Warren Wagon Road and detail how the new water meter(s) are being connected and detail if any service lines will

need to be abandoned due to upsizing requirements.

9. A Public Works Permit will need to be obtained prior to building permit issuance for the driveway onto **Warren Wagon Road**. More information about the Public Works Permit can be found at this link: <https://www.mccall.id.us/public-works-permit#:~:text=City%20of%20McCall%20Public%20Works,the%20city's%20water%20meter%20pits> , or contacting the Streets Superintendent, Cris Malvich at 208.634.8946.

Comments 1 through 8, and any subsequent comments from plan revisions, shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance. Please let me know if you have any questions or concerns.

Thank you,

Morgan Stroud, E.I.T. | Staff Engineer

216 E Park Street | McCall | ID 83638

Direct: 208.634.3458 | Cell: 208.315.2299



City of McCall

McCall Area Planning and Zoning Commission

Staff Report

CUP-21-03, DR-21-33

Middle School Expansion

111 North Samson Trail

December 7, 2021

Applicant: McCall Donnelly School District
Representative: Michael Schlager, Design West Architects
Application: Conditional Use Permit and Design Review
Zoning District: CV - Civic
Jurisdictional Area: City Limits

Description

An application for a Conditional Use Permit and Design Review to expand the existing use permitted to the school district by constructing an addition of approximately 12,000 sq. feet of classroom space on the Payette Lakes Middle School, as well as level out the school campus playing field and update the traffic circulation between the Middle School and Barbara Morgan Elementary School.

Staff Analysis

Project Information

Zoning District: CV - Civic

Comprehensive Plan Designation: Civic

Project Acreage: 39.6-acres

Proposed Use: Expansion of Existing Middle School

From: [Dion Zimmerman](#)
To: [Joe Barton](#)
Cc: [Brian Parker](#); [Michael Schlager](#); [Jason Clay](#); epingrey@mdsd.org; [Bill Hamlin](#)
Subject: RE: McCall Area Planning & Zoning Commission
Date: Wednesday, December 1, 2021 3:36:02 PM
Attachments: [image002.png](#)
[image003.png](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you Joe for speaking with Brian (City of McCall) and yes, we would like to delay the hearing until the January date to allow coordination as you mention below. I will make myself available to attend the in person meeting in McCall. Feel free to schedule it. I assume that you will work with Brian on some of the questions prior to the meeting as well.



Dion Zimmerman, Sr. Architect

Meridian, Idaho Office: 208-888-1768
Ontario, Oregon Office: 541-889-4406
Mobile: 208-866-3457
E: dzimmerman@designwestid.com
DESIGN WEST ARCHITECTS, P.A.

From: Joe Barton [mailto:jbarton@paragonfbk.com]
Sent: Wednesday, December 1, 2021 3:23 PM
To: Dion Zimmerman <dzimmerman@designwestid.com>
Cc: Brian Parker <bparker@mccall.id.us>; Michael Schlager <mschlager@designwestid.com>; Jason Clay <jjclay@mdsd.org>; epingrey@mdsd.org
Subject: RE: McCall Area Planning & Zoning Commission

Dion,

Please confirm via email to Brian Parker (copied on this email) that we would like to pull the Payette Lakes Middle School application from the December 7th P&Z Commission agenda and move it to the January meeting, providing the School District and the design team with an opportunity to meet with City Staff to iron out some of the details associated with the Conditions of Approval listed in the Commission packet.

I talked with Brian and he confirmed that delaying the P&Z Commission meeting was also preferable to the City and it would not disrupt the process and progress to date.

I would also request that a meeting with the School District and the City be setup in the next two weeks (next week if possible). I am thinking it will take a three hour + meeting to get through all the project details. I can be in McCall anytime December 7th through December 17th if you can find an agreeable time for an in-person meeting with the City Staff and the School District.

Let me know if you have any questions or if you need any additional information.

Thanks.

W. Joe Barton, P.E.
(208) 921-8486 Cell



157 West 4th Street
Kuna, Idaho 83634
(208) 922-9138

From: Brian Parker <bparker@mccall.id.us>
Sent: Wednesday, December 1, 2021 9:38 AM
To: Michael Schlager <mschlager@designwestid.com>; Dion Zimmerman <dzimmerman@designwestid.com>; Jason Clay <jclay@mdsd.org>; Joe Barton <jbarton@paragonfbk.com>; epingrey@mdsd.org
Subject: FW: McCall Area Planning & Zoning Commission

Middle School Team,

Your application is on the agenda for the December 7th McCall Area Planning & Zoning Commission. Your item may be found on pages 209-362 of the [Meeting Packet](#).

You may attend in person or via the Microsoft Teams link below. Please let me know if you have any questions.

-----Original Appointment-----

From: Brian Parker
Sent: Thursday, September 23, 2021 7:38 PM
To: Brian Parker; Meredith Todd; Linzie Green; jamesfronkconsulting@gmail.com; Jeffery Hatch; David@luxelocker.com; thoff45@gmail.com; Planning Commission; William L. Punkoney; Nathan Stewart; Michelle Groenevelt; Morgan Stroud; Andrea Nielsen; ross@the-trove.com; Charles Carpy; Dan Quakenbush; sarah@virtualdesignconcepts.com; Luke Vannoy; Todd Ketlinski; Heather Susemihl; Christi Ketlinski; Lucas Winter; 'Todd Allen'; len@madrona.com; dpeugh@epikosdesign.com

Subject: McCall Area Planning & Zoning Commission

When: Tuesday, December 7, 2021 4:30 PM-8:30 PM (UTC-07:00) Mountain Time (US & Canada).

Where: Legion Hall

Microsoft Teams meeting

Join on your computer or mobile app

[Click here to join the meeting](#)

Or call in (audio only)

[+1 208-634-8900,,443175772#](tel:+12086348900,443175772#) United States, Boise

Phone Conference ID: 443 175 772#

[Find a local number](#) | [Reset PIN](#)



To join with your computer click the link that says "Join Microsoft Teams Meeting" above.

[Learn More](#) | [Meeting options](#)

Dimensional Standards:

	Proposed	Required
Front Setback	400-feet	0-feet
Rear Setback	Greater than 450-feet	0-feet
North Side Yard Setback	462-feet	0-feet
South Side Yard Setback	122-feet	0-feet
Lot Coverage	135,372 square-feet (9.8% of allowed)*	1,379,980 square-feet (80% of total)

*Includes Elementary School Building and other outbuildings

Code Sections of Interest

- McCall City Code Section 3.2.02: Meanings of Terms or Words:

SCHOOL: An institution providing full time day instruction which is accredited by and meets the requirements of the Idaho State Board of Education, including nursery schools or kindergartens whose annual session does not exceed the school sessions for full time day schools.

STREET: A right of way which provides vehicular and pedestrian access to adjacent properties. The term "street" also includes the terms highway, thoroughfare, parkway, road, avenue, boulevard, lane, place, and other such terms. Streets are classified as follows (where an existing street has less than the below indicated width of right of way, such fact shall not deprive it of the status of "street", but shall instead evidence city policy for lawful acquisition of right of way in the fullness of time). Streets are designated in title VIII, "Public Ways And Property", of this code.

Collector: A street designated in title VIII of this code for the purpose of carrying traffic from minor streets to other collector streets and/or arterial streets, of which the right of way shall be not less than seventy feet (70') in width.

- McCall City Code Section 3.6.02: Permitted and Conditionally Permitted Uses within Public Zones:
Table 3.6.02 identifies "School, public or private, including vocational" as a conditional use within the CV zoning district.
- McCall City Code Section 3.8.061(C): Parking and Internal Circulation Development Standards: Landscaping And Screening:
 1. Parking lot landscaping shall meet the objectives of reducing the visual and noise impacts from vehicles, softening the expanse of hard surface areas, reinforcing circulation, and providing stormwater benefits.

2. For parking lots over fifty thousand square feet (50,000 sf) in size, a minimum of ten percent (10%) of the site shall be in interior parking lot landscaping. Public art, decorative paving, kiosks, green infrastructure or parking lot configurations that break-up the expanse of pavement may be substituted for interior landscaping.

3. Landscaped areas shall be consolidated to enhance tree and plant material growing conditions that reflect the natural growing patterns of the native landscape, and to provide locations for snow storage, natural drainage, light fixtures and other utilities.

4. Landscape materials shall consist of native trees, vegetation, including grasses, hardy shrubs, or evergreen ground cover, and maintained in good condition. (See City of McCall publication, "Native and Suitable Plants".)

5. Where parking areas adjoin or face any residential property, the parking lot shall be effectively screened by an acceptably designed wall, fence, hedge berm or planting screen.

a. Fence or wall screens shall be not less than four feet (4') or more than six feet (6') in height except in street setback areas where it shall not exceed four feet (4').

b. Planting screens shall not be less than four feet (4') in height.

c. In the event that terrain or other natural features are such that the erection of such fence, wall or planting screen will not serve the intended purpose, then with the approval of the Administrator, in writing, no such fence, wall or planting screen and landscaping shall be required.

d. The use of chain link fencing in any residential zone, CBD and CC zones is prohibited. In all other zones, approval by the Administrator is required.

- McCall City Code Section 3.8.061(F): Parking and Internal Circulation Development Standards:
Internal Circulation: The design of internal circulation shall be integrated with the overall site design and adjacent properties, including the location of structures, pedestrian walkways and landscaping.

1. Pedestrian circulation shall be clearly identifiable using continuous sidewalks, separated walkways within parking areas and well-designed pedestrian crossings.

2. Driveways, aisles and turnaround areas shall meet the standards for fire and refuse access.

3. Bus pullouts may be required when a development is adjacent to an existing or planned bus stop.

- McCall City Code Section 3.8.063: Bicycle Parking:
Table 3.8.063 requires two (2) long term bicycle parking spaces per classroom and four (4) short term bicycle parking spaces per school, both subject to modification through the conditional use permitting process.
- McCall City Code Section 3.8.16(B): Other Requirements:
Sidewalk, Curb And Gutter And/Or Pathways: Sidewalks or pathways shall be required improvements for projects requiring design review approval in the commercial zones, or other districts where existing sidewalks/pathways adjoin the subject property, or where the commission determines sidewalks/pathways are necessary for public safety or located on the McCall area pathways master plan. The requirement for sidewalks/pathways may be waived if the cost of the proposed construction is disproportional to the total construction costs and impact of the project. Sidewalks (and curb and gutter where required) shall meet the standards set forth in the city's improvement standard drawings, as adopted.
- McCall City Code Section 3.13.03(B): Findings for Granting Permit:
A conditional use permit shall be granted only if the commission finds that the use, as applied for, in fact will:
 1. Constitute a conditional use authorized in the zone involved.
 2. Be harmonious with and in accord with the general objectives and with any specific objectives of the comprehensive plan and/or this title.
 3. Be designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or likely character of the neighborhood, and that such use will not change the essential character of the surrounding area.
 4. Not be detrimental to the health, safety and general welfare of persons residing or working in the neighborhood of such proposed use.
 5. Not cause any substantially harmful environmental consequences to any land or waters within the planning jurisdiction.
 6. Not create excessive additional public cost for public facilities and services, and will not be detrimental to the economic welfare of the community.
 7. Be served adequately by essential public facilities and services including highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools. The applicant may be required, as a condition of approval, to mitigate any deficient public service.
 8. Not involve uses, activities, processes, materials, equipment or conditions of operation that will cause unreasonable production of traffic, noise, smoke, fumes, glare, odors or other forms of pollution.
 9. Have vehicular approaches to the property so designed as not to create a detrimental interference with traffic on surrounding public or private thoroughfares, or adversely affect the pedestrian environment.
 10. Not result in the destruction, loss or damage of an important natural, scenic or historic feature.

11. Be on a site of sufficient size to accommodate the proposed use, including the yards, open spaces, snow storage, walls, fences, parking areas, loading zones and design standards applicable.

12. Have a minimal negative economic impact on the neighborhood or surrounding community.

- McCall City Code Section 3.13.03(C): Required Conditions:

The commission may, after review of the application for a conditional use permit and public hearing, require the applicant to meet any specific conditions of approval deemed necessary by the commission to protect the health, safety, general welfare and environment of the community. Such conditions are not limited to, but may include:

1. Limitations on the hours of operation of the use;
2. Limitations on the length of time that the conditional use permit may be exercised before it will expire by its own terms;
3. Additional landscaping and building beautification;
4. Additional or reduced off street parking or transportation improvements; and/or
5. Execution of a written agreement respecting construction of necessary improvements similar in form and content to a subdivision agreement, with its performance secured in the same fashion as performance of a subdivision agreement.

- McCall City Code Section 3.16.07: Design Review Criteria:

The commission or administrator shall determine the following before approval is given:

- (A) The project is in general conformance with the comprehensive plan.
- (B) The project does not jeopardize the health, safety or welfare of the public.
- (C) The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance as enumerated in section 3.16.01 of this chapter.
- (D) The project will have no substantial impact on adjacent properties or on the community at large.
- (E) If applicable, a subdivision design review document has been reviewed and approved by the commission in lieu of the design guidelines.
- (F) For projects in the shoreline and river environs zone, the project will not have an unreasonable and adverse impact on the visual quality of its setting or the water quality.
- (G) For projects in the scenic route zone, the project will preserve and enhance the scenic quality of the street or highway.

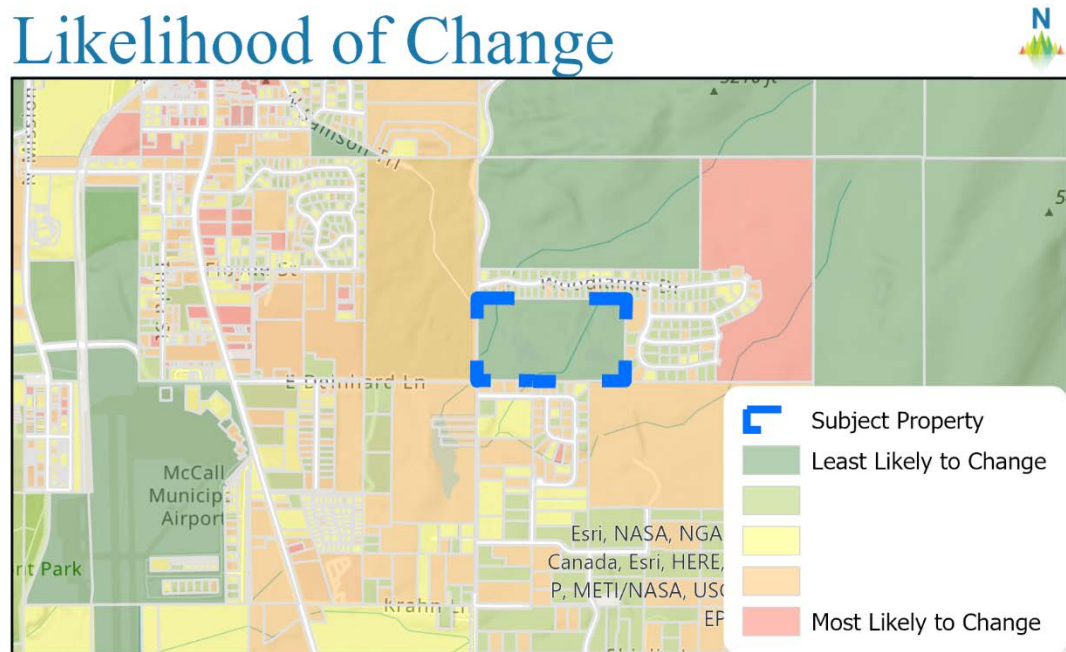
- McCall City Code Section 9.3.04(G): Streets:

Street layout shall conform to the most advantageous development of adjoining areas and the entire neighborhood, and shall provide for the following:

1. Adequate access to adjoining lands to facilitate future subdivisions of such land.
2. Appropriate contribution to the incremental creation of a connected network of streets and circulation within the city and impact area.
3. Implementation of the comprehensive plan.
4. Streets intersecting at right angles, or as nearly as possible. Where possible, four-way intersections shall be used.
5. Half streets shall be prohibited, except where essential to the reasonable development of the subdivision in conformity with the other requirements of these regulations and where the council finds it will be practicable to require the dedication of the other half when the adjoining property is subdivided. In all other instances where a half street is adjacent to an undeveloped tract, the other half of the street shall be platted within such tract.
6. No street names shall be used which will duplicate or be confused with the names of existing streets in any town or county area. Street names shall be subject to the approval of the administrator. Street names, where the street continues across an intersection, shall normally be continuous.
7. If street trees are required by the commission to be installed, the minimum standard shall be of two inch (2") caliper, forty feet (40') to sixty feet (60') apart. Trees or shrubs placed within twenty five feet (25') of a street corner shall not obstruct clear vision of and across the corner between three feet (3') and eight feet (8') above the ground level of the traveled way. Tree species are subject to the approval of the city arborist.
8. Cul-de-sacs and other discontinuous streets shall generally be avoided in favor of a connected network.
9. Cul-de-sac streets, designed to be permanent, shall not be longer than nine hundred feet (900') when measured from the intersection line of the cross street to the center of the cul-de-sac, and shall be provided with a turnaround with a right of way diameter of at least one hundred twenty feet (120') and a paved roadway of at least ninety feet (90'). Cul-de-sac streets shall be private streets, to be maintained by the owners of the subdivision. The city may require pedestrian accessways connecting cul-de-sac streets to adjacent streets, rights of way, or open space. (See section 9.6.05 of this title.)
10. Emergency access streets, which are designed to alleviate excessively long cul-de-sac streets (or other conditions), require the approval of the commission and shall be private streets. The minimum right of way shall be forty feet (40').

Comprehensive Plan Sections of Interest

- Vision in Motion – Our Vision:
McCall is a diverse, small town united to maintain a safe, clean, healthy, and attractive environment. It is a friendly, progressive community that is affordable and sustainable.
- Deep Dive – Future Land Use Designations – Civic:
This land use designation is intended to provide for governmental offices and other civic facilities including governmental, cultural, and recreational facilities. Mixed use is allowed in combination with public uses. On the Future Land Use Map, it is applied to schools, parks, the hospital, and other selected public properties.
- Deep Dive – Transportation – Policy 2.4:
Encourage a connected street network and do not approve new development of permanent dead-end streets.
- Deep Dive – Environment and Natural Resources – Policy 3.4:
Promote reductions in air pollution to minimize impact to human health, sustain or improve the local economy, improve air quality, and reduce the impact of greenhouse gases.
- Deep Dive – Map 5.6 – Likelihood of Change:



Other Long-Range Plan Sections of Interest

- McCall Area Pathways Master Plan: Pathways Facility Classification 4.5.5 – Separated Paved Pathways:

Separated paved pathways are multi-use pathways that are physically separated from motor vehicle traffic and typically accommodate two-way travel. They can be located within the right-of-way of the adjacent roadway or within an independent right-of-way or easement, and they are intended for a variety of users including bicyclists, skaters, pedestrians, joggers, and other nonmotorized users. Separated paved pathways can serve to provide shortcuts through neighborhoods and parks, safe alternatives to busy roadways, recreational opportunities, and access to areas not reachable by motor vehicle. The minimum width of a paved multi-use pathway should be 10-feet. Greater widths of 12-feet or more are desirable where expected user volumes are high, in areas of sharp curves, or where high speed cycling is anticipated such as on steeper hills. Wider pathways or separated pedestrian paths are sometimes needed when use is very heavy although this condition is not anticipated in the McCall Area in the foreseeable future. Paved pathways should be paved with a hard, all weather surface, typically asphalt or concrete. The thickness of the pavement section should be determined based on the loading capacity and characteristics of the existing soils and should be capable of sustaining loads from emergency and maintenance vehicles. The guidelines given in the AASHTO Guide for the Planning, Design, and Operation of Bicycle 40 McCall Area Pathways Master Plan Facilities should be followed in the design of any new or reconstructed separated paved pathways. This document gives further guidance on appropriate pavement widths, shoulders, pavement markings, signage, alignments, street crossings, grades, etc. The McCall Area Pathways Master Plan Map shows existing and desired locations for separated paved pathways.

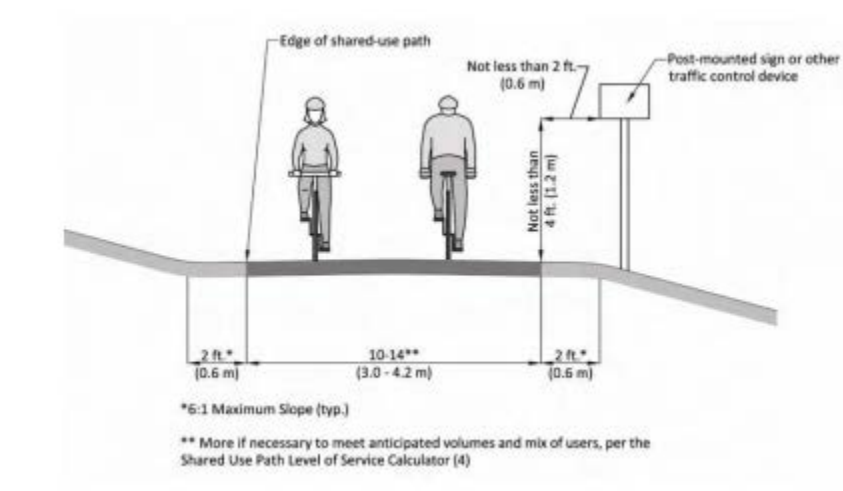


Figure 16 – Example Separate Pathway Cross Section from AASHTO Guidelines

Staff Discussion

- The applicant is proposing the following:
 - An addition of approximately 12,000 square feet of classroom space.

- Updated grading and landscaping of the playground area between the south and east wings of Payette Lakes Middle School
- Modifications to the vehicular circulation plan for the campus including:
 - Re-aligning the access driveway to Barbara Morgan Elementary School and the northern parking lot access driveway to improve traffic flow into the elementary school.
 - Expanding the pick-up/drop-off area at Payette Lakes Middle School and relocating the existing accessible parking stalls to the interior of the parking lot.
 - Redesigning the bicycle and pedestrian pathway network to create more direct connections from the intersection of Deinhard Lane and Samson Trail to Payette Lakes Middle School and Barbara Morgan Elementary School.
 - Generally reconfiguring traffic circulation within the school site to remove left turn movements from the south access point into the Payette Lakes Middle School parking lot, moving all bus service to the north entry point, and creating a two-way entrance/exit at the north entry point.
- The subject property has been subject to several conditional use permits and encumbrances in its history. Below is a brief history of relevant actions on the site:
 - On September 27, 1973, an easement created along the southernmost seventy-foot (70') of the SW ¼ of the NW ¼ of Section 15, T18NR3E. The easement was granted for "road right of way purposes" to provide access to the SE ¼ of the NW ¼ of Section 15, which is generally the current Barbara Morgan Elementary site and a portion of the Woodlands Subdivision No. 2. The southern property boundary of the current school property is roughly centered within this easement area.
 - On December 21, 1978, the current school property was sold to the McCall Donnelly School District. The 70' easement was not reserved in the transaction.
 - On July 14, 1994, the McCall City Council approved CUP-07-1, which facilitated the construction of the Payette Lakes Middle School. The following condition of approval was included within the Finding and Conclusions document:
 - "The applicant shall contract to dedicate the southernmost 35 feet of the tract to the public for Deinhard Lane at such time as Deinhard Lane is proposed to be extended to the east of the subject property, but is not precluded thereby from using the same for access road construction; no City snow plowing of this or any other on-the-site road should be anticipated."
 - On January 7, 2004, the McCall City Engineer recommended to the applicant of SUB-04-16/ZON-04-7, Fox Ridge Subdivision (located directly to the south of the school site), to extend Deinhard Lane, utilizing the 35-foot easement on the school site and 35-feet of the site of Fox Ridge Subdivision.
 - On December 7, 2004, the City of McCall Planning & Zoning Commission hears and continues SUB-04-16/ZON-04-7. The extension of Deinhard Lane was discussed at that time.
 - On January 5, 2005, the McCall Transportation Advisory Committee reviews SUB-04-16/ZON-04-7 and recommends extension of Deinhard.

- On January 10, 2005, the McCall Transportation Advisory Committee reviews SUB-04-16/ZON-04-7 again and determines that an extension of Deinhard Lane is not feasible.
- On January 11, 2005, McCall Area Planning & Zoning Commission approved SUB-04-16. A condition of approval is included explicitly requiring the developer to comply with all engineering recommendations except for the extension of Deinhard Lane.
- On March 22, 2007, the McCall City Council approved CUP-07-01, which facilitated the construction of Barbara Morgan Elementary School. The following condition of approval was included within the Finding and Conclusions document:
 - “If the entranceway into the school, located at the intersection of Deinhard Lane and South Samson Trail, poses a traffic conflict, then staff has the right to review the intersection every 5 years and take remedial action as necessary.”
- The school site has developed its circulation patterns in a relatively ad-hoc manner that hinders the ability to walk or bike to the school and has led to circulation and queuing problems at the middle and elementary schools, which has in turn led to occasional queuing problems at the intersection of Deinhard Lane and Samson Trail (see Public Works review letter, attached).

In addition to the existing pedestrian, bicycle, and automotive transportation issues related to the school site, large parcels to the east and southeast of the school site have been designated an Area of Critical Concern (AOCC) by McCall City Council Resolution 21-25 in anticipation of a large-scale development. The existing roadway network would push traffic from the AOCC site to utilize Samson Trail to get to the school site or to travel west on Deinhard Lane. The construction of a collector street along the southern property boundary of the school site would relieve pressure on the Samson Trail legs of the Samson Trail/Deinhard Lane intersection, allow for alternative routes into the school site to alleviate queuing issues, and allow for the construction of improved bicycle and pedestrian pathways to the elementary and middle schools.

As currently proposed, the expansion of the middle school would exacerbate the existing problems with the traffic circulation and hinder the future ability of the neighboring properties to develop in a manner consistent with McCall City Code Section 9.3.04(G) and the Comprehensive Plan Policies 2.4 and 3.4, in conflict with Conditional Use Permit Standards 2, 3, 7, and 9. As such, staff is recommending that the applicant dedicate an additional 35 feet to the existing 35 foot wide easement along the southern property boundary. Prior to the issuance of a building permit, the applicant should be required to record a 70 foot wide easement along the entirety of the southern property boundary. The easement should be dedicated to the public for the construction of a future public roadway and pedestrian pathways and associated snow storage and drainage.

- The traffic impact study, date stamped by the City on November 18, 2021, asserts that the intersection of Deinhard Lane and Samson Trail will fall below at a level of service (LOS) D or better after the first phase of the Payette Lakes Middle School Expansion. The applicant is proposing the following traffic mitigation measures:
 - Convert the southbound Samson Trail to westbound Deinhard Lane right-turn lane from a stop-controlled lane to a free-running turn lane
 - Extend the three-lane portion of Samson Trail to the northern entrance to the school site to facilitate longer queuing for southbound to southbound and southbound to eastbound traffic on Samson Trail.

- Internal circulation changes to the site as described above.
- The street design of the potential subdivision to the east and south of the school site should include a cul-de-sac or parking area with a connection to the future bicycle and pedestrian pathway connection within the school site.

Staff has the following concerns regarding the following components of the traffic impact study:

- Section 5.16 of the traffic impact study does not identify the parcels on the northwest and southwest corners of the intersection of Samson Trail and Deinhard Lane as potential future development sites. Both are identified as having a high likelihood of change in Map 5.6 of the Comprehensive Plan. Development of either or both of these properties would have substantial impacts on the intersection of Deinhard Lane and Samson Trail as well as the school site. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant should provide a revised traffic impact study with a more thorough accounting of future developments in the area.
- The traffic counts for existing conditions occurred on September 22, 2021. Roadway construction between on Highway 55 between Donnelly and the intersection of Highway 55 and Deinhard Lane was likely skewing the ratio of northbound users from Highway 55 to South Samson Trail. This does not appear to be factored into the traffic impact study. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant should provide a revised traffic impact study with a new traffic count. The new count should occur on a weekday while both schools are in session.
- The proposed mitigation measures are dependent upon implementing the modified internal circulation plan, but no timeline on completing the required improvements is identified. Prior to the issuance of a certificate of occupancy for the expansion of Payette Lakes Middle School, the applicant should construct all necessary internal improvements and implement the modifications to internal circulation proposed within the 2019 traffic impact study.
- The proposed free-running turn lane would create higher speed conflict areas with the existing bicycle and pedestrian pathway adjacent to Deinhard Lane. While moving conflict points between motorized and non-motorized roadway users are not as safe as stop controlled conflict points in any context, this intersection's proximity to the school and the high snow berms that develop around the pathway would likely lead to a substantial visibility issue at the intersection. Additionally, the channelization curbing would likely increase the existing intersection offset between Deinhard Lane and the southern drive aisle to the school site. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant should provide a revised traffic impact study analyzing the safety impacts of the proposed remedies on the intersection of Deinhard Lane and South Samson Trail.
- The site design prescribed for the potential subdivision to the south and east of the school site is very similar to the existing condition at the Woodlands community pool. The neighborhood meeting notes, dated September 1, 2021, identified the informal use of the Woodlands community pool parking area as a school drop off zone as an existing issue. As stated above, it is staff's recommendation that a seventy-foot (70') easement be dedicated to facilitate a public roadway connecting Deinhard Lane to the potential

subdivision to the south and east of the school site, which would avoid creating the same neighborhood problem as currently exists within the Woodlands.

- The traffic impact study, date stamped by the City on November 18, 2021, includes an updated bicycle and pedestrian showing the construction of additional pathways along the existing drive aisle along the southern property boundary, which connect to Payette Lakes Middle School and the existing pedestrian pathway connecting to Barbara Morgan Elementary School. The new pathway is proposed to be eight-feet (8') in width. The existing pathways along the northern drive aisle and the drive aisle to the elementary school are five-feet (5') in width. McCall City Code Section 3.8.16(B) requires separated pathways to be in conformance with the McCall Area Pathways Master Plan, which requires all separated pathways to be a minimum of ten-feet (10') in width. Prior to the issuance of a building permit, the applicant should provide a revised bicycle and pedestrian plan showing all bicycle and pedestrian pathways located on-site to be a minimum of ten-feet (10') in width and constructed to the standards identified in the McCall Area Pathways Master Plan.
- The landscape plan, dated September 21, 2021, does not identify any bicycle parking located on site. McCall City Code 3.8.063 requires two (2) long term bicycle parking spaces per classroom and four (4) short term bicycle parking spaces per school. The proposed expansion of the school would bring the total number of classrooms up to 21, meaning that 42 long term bicycle parking spaces would be required. Prior to the issuance of a building permit, the applicant should be required to provide a revised landscape plan showing 42 long term and four (4) short term bicycle parking spaces. The required bicycle parking should be in conformance with McCall City Code Section 3.8.063(A)-(F) and the Association of Pedestrian and Bicycle Professionals' *Essentials of Bike Parking*.
- The site plan, date stamped by the City on September 25, 2021, identifies a future expansion area at the southern end of the south wing of Payette Lakes Middle School. As the impacts of that expansion are partially being addressed by this application, and the remaining impacts could be addressed through a future design review application, it is staff's opinion that an expansion of an equivalent square footage to what is shown on the site plan may be approved through a design review application and that an additional conditional use permit should not be required.
- The landscape plan, dated September 21, 2021, shows the use of moonbeam tickseed and sunny days ruby coneflower, which are not identified on the Native and Suitable Plants List, and are more suited to less cold and snowy climates. Prior to the issuance of a building permit, the applicant should provide a revised landscape plan utilizing plants from the Native and Suitable Plants List.
- McCall City Code Section 3.8.061(C) requires parking lots greater than 50,000 square feet to have interior landscaping on at least 10% of the parking lot area. The existing parking lot for Payette Lakes Middle School is approximately 60,000 square-feet. Prior to the issuance of a building permit, the applicant should provide a revised landscape plan showing at least 10% of the parking lot area replaced with landscaping.

Comments

Agency –

McCall Public Works

See attached letter dated November 10, 2021.

McCall Fire and & EMS

In an email dated October 8, 2021, the Fire Chief had the following comments:

Fire sprinkler and alarm system plans will need to be submitted to the Idaho State Fire Marshal's Office for plan review.

The traffic plan, including any gates, will need to be approved by the fire district.

Public –

Email received from Tom Kilgore on November 30, 2021, expressing concerns.

Staff Recommendation

Staff recommends approval of this application with the Conditions of Approval listed below.

Conditions of Approval

1. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant shall provide a revised traffic impact study with a more thorough accounting of future developments in the area.
2. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant shall provide a revised traffic impact study analyzing the safety impacts of the proposed remedies on the intersection of Deinhard Lane and South Samson Trail.
3. Prior to scheduling the application for a public hearing with the McCall City Council, the applicant shall provide a revised traffic impact study with a new traffic count. The new count shall occur on a weekday while both schools are in session.
4. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
5. Prior to the issuance of a building permit, the applicant shall be required to record a 70-foot-wide easement along the entirety of the southern property boundary. The easement shall be dedicated to the public for the construction of a future public roadway and pedestrian pathways and associated snow storage and drainage.
6. Prior to the issuance of a building permit, the applicant shall provide a revised bicycle and pedestrian plan showing all bicycle and pedestrian pathways located on-site to be a minimum of ten-feet (10') in width and constructed to the standards identified in the McCall Area Pathways Master Plan.

7. Prior to the issuance of a building permit, the applicant shall provide a revised site plan identifying a minimum of forty-two (42) long-term and four (4) short-term bicycle parking spaces. The required bicycle parking shall be in conformance with McCall City Code Section 3.8.063(A)-(F) and the Association of Pedestrian and Bicycle Professionals' *Essentials of Bike Parking*.
8. Prior to the issuance of a building permit, the applicant shall provide a revised landscape plan utilizing plants from the Native and Suitable Plants List.
9. Prior to the issuance of a building permit, the applicant shall provide a revised landscape plan showing at least 10% of the parking lot area replaced with landscaping.
10. Prior to the issuance of a certificate of occupancy for the expansion of Payette Lakes Middle School, the applicant should construct all necessary internal improvements and implement the modifications to internal circulation proposed within the 2019 traffic impact study.
11. Prior to the issuance of a certificate of occupancy, the applicant shall reseed all disturbed areas with native grasses or wildflowers.
12. Pursuant to McCall City Code Section 3.13.034(B), conditional use permit approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.
13. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.
14. An expansion of an equivalent square footage to what is shown on the site plan associated with this application may be approved through a design review application and that an additional conditional use permit shall not be required. Additional transportation or other improvements may be conditioned within future applications.

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received: _____

Fees Paid: _____

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # _____ Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # _____ Scenic Route (SR) - \$300
- # _____ Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: _____ Email: _____

Mailing Address: _____ Phone: _____

Property Owner 2 (If Applicable): _____ Email: _____

Mailing Address: _____ Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: _____ Email: _____

Mailing Address: _____ Phone: _____

PROPERTY INFORMATION

Address(es) of Property: _____

Legal Description of Property: _____

Zoning District of Property: _____ Project Sq. Footage (If Applicable): _____

Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: *(please attach supplemental information if needed)*

The School District would like to build approximately a 12,000 sf classrooms and multipurpose space addition to the existing Payette Lakes Middle School.

SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

The submittal items identified in the application info sheets are the minimum required materials for the City to accept applications. The City Planner may require additional information based on the specific circumstances of each proposal.

Jason Clay - McCall-Donnally School Dist
Property Owner 1



Property Owner 2 (If Applicable)

Signature

Michael Schlager - Design West Architects
Agent/Authorized Representative



Signature

FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received: _____

Fees Paid: _____

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- # _____ Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
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- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
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- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
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- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
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Applicant/Representative: _____ Email: _____

Mailing Address: _____ Phone: _____

PROPERTY INFORMATION

Address(es) of Property: _____

Legal Description of Property: _____

Zoning District of Property: _____ Project Sq. Footage (If Applicable): _____

Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: *(please attach supplemental information if needed)*

The School District would like to build approximately a 12,000 sf classrooms and multipurpose space addition to the existing Payette Lakes Middle School.

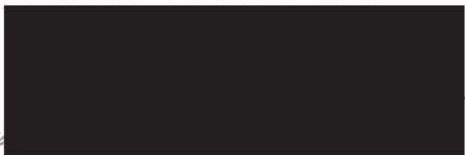
SIGNATURES

The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

The submittal items identified in the application info sheets are the minimum required materials for the City to accept applications. The City Planner may require additional information based on the specific circumstances of each proposal.

Jason Clay - McCall-Donnally School Dist
Property Owner 1



Property Owner 2 (If Applicable)

Signature

Michael Schlager - Design West Architects
Agent/Authorized Representative



FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.



DESIGN
WEST

9/21/2021

City Of McCall
Department of Community and Economic Development

RE: Payette Lakes Middle School Design Review

Project Description:

In response to the recent population growth in Valley County, the McCall- Donnelly School District ran, and passed with a super majority of voter approval, a bond issue to fund a project that included an addition to their existing Payette Lakes Middle School. The existing building located at 111 S. Sampson Trail has been used as the Payette Lakes Middle School since it was built in 1994. With the recent student population growth in the area there is a need for additional classrooms at the existing Middle School. The building will continue to be an educational use, so there will be no change in land use, transfer of ownership, or other significant change in the current operations on the site. The educational use of the building will continue to serve the relatively large number of students living in the surrounding neighborhoods as well as the rest of the community.

The design and use of this project will be in accordance with all City of McCall planning and zoning requirements, and in an effort to serve the community as a whole in the most effective manner possible. The material and form of the building will be designed as a continuation of the existing building, expanding 2 classroom wings, and infilling a recessed portion of the plan. The same masonry block, roofing, fascia, and other architectural details will replicate what currently exists on the building as shown in the attached elevations and renderings. A special effort has been put into the design of the proposed landscaping to comply with Title 3 of the McCall City Code by incorporating native or suitable plants for the McCall area. The proposed site lighting will comply with Title 3 Chapter 14 in that new fixtures will be placed in the building soffit at doorways to provide 'sufficient lighting to promote safety and security' to exit and enter the building without casting excessive light away from the building. The new fixtures will protect the view of the night sky and will comply with the guidelines as laid out in 3.14.02.

We look forward to partnering with the City of McCall as we move forward with this project. Thank you for taking the time to review this application. Feel free to reach out with any questions you may have.

Sincerely,

Michael Schlager
Design West Architects
mschlager@designwestid.com
208-888-1768

MERIDIAN, IDAHO
PULLMAN, WASHINGTON
KENNEWICK, WASHINGTON
ONTARIO, OREGON

DESIGN WEST
ARCHITECTS, P.A.

216 SW FIFTH AVENUE
MERIDIAN, IDAHO 83642
TEL. 208-888-1768
www.designwestpa.com



9/21/2021

City Of McCall
Department of Community and Economic Development

RE: Payette Lakes Middle School Conditional Use Permit

Project Description:

In response to the recent population growth in Valley County, the McCall- Donnelly School District ran, and passed with a super majority of voter approval, a bond issue to fund a project that included an addition to their existing Payette Lakes Middle School. The existing building located at 111 S. Sampson Trail has been used as the Payette Lakes Middle School since it was built in 1994. With the recent student population growth in the area there is a need for additional classrooms at the existing Middle School. The building will continue to be an educational use, so there will be no change in land use, transfer of ownership, or other significant change in the current operations on the site. The educational use of the building will continue to serve the relatively large number of students living in the surrounding neighborhoods as well as the rest of the community.

The design and use of this project will be in accordance with all City of McCall planning and zoning requirements, and in an effort to serve the community as a whole in the most effective manner possible. A special effort has been put into the design of the proposed landscaping to comply with Title 3 of the McCall City Code by incorporating native or suitable plants for the McCall area. The proposed site lighting will comply with Title 3 Chapter 14 in that new fixtures will be placed on the building at doorways to provide 'sufficient lighting to promote safety and security' to exit and enter the building. The new fixtures will it will protect the view of the night sky and will comply with the guidelines as laid out in 3.14.02.

We look forward to partnering with the City of McCall as we move forward with this project. Thank you for taking the time to review this application. Feel free to reach out with any questions you may have.

Sincerely,

Michael Schlager
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Payette Lakes Middle School Conditional Use Permit-

Criteria of Approval Responses:

1. The school use may be permitted as a conditional use in the Civic zone (per table 3.6.02 in McCall City Code). Payette Lakes Middle School was granted a Conditional Use Permit in 1994 for the original school building.

2. The request is for an addition to the building that is currently being used as Payette Middle School and is addressed on the Comprehensive Plan as a Civic zone:

Civic Use (CV) - The CV land use designation is intended to provide for governmental offices and other civic facilities including governmental, cultural and recreational facilities. Mixed use is allowed in combination with public uses. CV is an existing zone district. On the 2007 FLUP, it is applied to schools, parks, hospital, and other selected public properties.

3. The proposed use meets the requirements outlined in Title 3- the school addition will 'complement the landscape, ecology and existing urban character of McCall.'

4. The proposed use is harmonious with the character of the existing neighborhood as the school addition is in direct response to the growing community.

5. The proposed addition will be harmonious with the appearance of the existing school building. The building addition will be built with similar shape and materials of the existing school building.

6. The proposed use will benefit the welfare, health, and safety of the neighborhood by providing a safe building for the students of the growing community to learn in.

7. The proposed addition to the existing school building will not harm the existing land or water on the subject property and adjacent properties. The required storm water mitigation for the addition will be constructed per City of McCall codes.

8. This proposed use is currently a public service, school. All other public services that currently serve the site will continue to serve the site. The proposed project is a public facility requiring more space to adequately serve the growing community.

9. The proposed use of the addition matches the existing use and will comply with City of McCall pollution reduction measures.

10. The proposed use of the school addition matches the existing use and will not affect the pedestrian environment. Additional pedestrian and cyclist pathways are expected to encourage foot and cycle traffic at certain times of the year, decreasing the need for automobile trips.

11. The proposed use of the school addition matches the existing use of the site and the School District is looking at ways to handle the increase of traffic with efficient flow on site to not be a detriment to traffic on surrounding streets.

12. The proposed use will not change the scenic look of the existing building. The proposed addition will be built with similar shape and materials of the existing school building.

13. The proposed use will not affect historic features as there are not any on the existing school site.

14. The total existing school site is over 39 acres (area includes the Barbara Morgan Elementary School). The proposed addition to the middle school building will be built attached to the existing building not affecting existing snow storage, parking, open space requirements, and landscaping. The area of hardscape is expected to be equaled or potentially slightly reduced with the addition of landscaping and green play areas.

15. The proposed school addition will be a positive to the economy of the City of McCall. Communities with great schools encourage residential and business growth and as a result directly add to the economy and tax base of the city.



216 SW 5TH AVE
MERIDIAN, ID 83642
TEL: (208) 888-1768
FAX: (208) 955-6885

Meeting Report

Payette Lakes Classroom Expansion
Neighborhood meeting
9-1-2021

Prepared by:

Michael Schlager

Distribution:

- City of McCall Community Development

Meeting Attendees

- Dion Zimmerman
- Michael Schlager
- Jason Clay
- Eric Pingrey
- See Sign-In Sheet

Date Issued: 9-21-2021

Meeting Report: The following represents the understanding of discussions held and decisions reached in this meeting. The minutes are organized in subject order and do not necessarily follow the chronological order of the meeting. Follow up comments and clarifications are in response to items during the meeting that the applicant did not readily have an answer to, but indicated they would research and clarify for the report.

DZ-Dion Zimmerman

CC-Community Comment

FC- Follow Up Comment, or Clarification from applicant after the meeting.

PLMS – Payette Lakes Middle School

BMES- Barbara Morgan Elementary School

- DZ presented a summary of the proposed classroom renovation as indicated in the attached exhibits
- DZ acknowledged that the 1994 Conditional use permit required the district to dedicate 35' at the southern edge of the property for future road extension.
- DZ presented the traffic control plan as indicated in attached exhibit.
- CC numerous concerns were brought up regarding the proposed Pine Creek Ranch proposed development, and the traffic that potential future development might generate. The general consensus was that none of the home owners wanted the traffic from the proposed development to impact their neighborhood.
 - o FC The applicant has acknowledged the proposed future development, and is considering options to mitigate traffic concerns within the budget available for site improvements.
- CC Woodlands home owners expressed concerned about claim that non-Woodlands residents are dropping their kids off at the Woodlands club house to use the pedestrian path that the school has provided for the neighborhood to access the school site.
- CC Woodlands home owners expressed concerned about claim that traffic sometimes travels 40-50 mph down Woodlands Drive.
- CC indicated that traffic on the school site backs up and becomes very congested at pickup and drop off times from school generated traffic. DZ referred to the traffic study plan (attached) and explained the revised traffic routing to alleviate congestion and stacking problems.
- CC proposed a student drop off on Spring Mountain Road (Sampson Trail) away from the school, and having the students walk from that proposed drop off area to the school rather than drop off directly in front of the school.

- FC the applicant is considering the possibility of creating alternative drop off locations and patterns.
- CC proposed providing temporary modular classrooms rather than building classroom additions as presented prior to the school bond election.
 - FC this proposal does not match what was proposed to voters prior to running the school bond issue and will not be an option as part of this project.
- CC presented a concern that the school bus stops at the Woodlands at 7:00 AM and kids are on the bus for an hour prior to school unless Woodlands parents drop their kids off at the school.
- CC presented a concern that there is no enforcement of parking and traffic at events on the school site.
 - FC The applicant is exploring options for mitigating on site traffic and congestion during special events.
- CC asked about potential redesign of the school building to allow additional space for a road through the existing playground area to serve the proposed future development to the east.
 - FC The design of the building does not encroach on any required right of ways and will move forward as planned.
- CC indicated that every parent dropping their child off at the school should be charged \$1.00 each time they pick up or drop off a student at the school.
 - FC This proposal is impractical and will not be implemented.
- CC asked whether the “Future Classrooms” identified on the attached plan would be shelled out, or completely constructed at a later date. DZ clarified that all construction on those potential classrooms would be at a later date.
- CC requested that a 35’ easement be granted where the previous CUP required the 35’ dedicated right of way at the south edge of the site. DZ commented that the required 35’ right of way has been maintained on the school district’s property as required by the previous CUP.
- CC asked how many acres school site is.
 - FC Assessor’s map lists the property at 39.6 acres.
- CC asked the enrollment numbers at Payette Lakes Middle School and Barbara Morgan Elementary
 - FC 2019-2020 enrollment per the State Department of Education indicated 422 students at BMES and 328 students at PLMS.
- CC asked how students would evacuate the building in the event of a catastrophe. DZ indicated that the school district has emergency action plans.
- CC indicated that the road to the proposed future Pine Creek Ranch subdivision must go through the school site because it would not work to bring the additional traffic down the road where they live.
 - FC The applicant will maintain the 35’ right of way at the south edge of their property as required by the 1994 conditional use permit.
- CC asked does the traffic study address a catastrophe at the school.
 - Catastrophes are addressed by the school districts emergency action plans, this is beyond the scope of the vehicular traffic study.
- CC recommended staggering bell schedule for BMES and PLMS in order to cut down peak vehicular traffic.
 - The applicant will look into the option of staggering bell schedules to reduce peak traffic.
- CC asked how many students were in each bus load. DZ was unable to provide the number of students on each bus load, but indicated that about 65% of students are dropped off.
- CC proposed more bus routes with smaller school buses in order to cut down on time students would need to spend on the bus.
 - FC this proposal would not be financially feasible and the applicant does not plan to implement this proposal.
- CC asked how many students would the school district grow by once COVID was over. I.e. how many students would remain doing online school, how many currently online students would be returning to in person, and how many new students would move into the district given the communities recent

growth. DZ indicated that given the unpredictability to the covid situation so far that it would be difficult to predict this number.

- CC asked how the concerns being voiced would get addressed. DZ explained the Conditional Use Permit process, and that the meeting report would be submitted to the City to be incorporated into the public record for consideration prior to the CUP hearing.
- CC asked how traffic safety affected the design of the school. DZ referenced the traffic study plan (attached) and the modified traffic patterns and stacking area.
 - o FC The attached traffic study plan is based on an in depth traffic study that the district had completed in 2019.
- CC asked if a bike / pedestrian path up the north end of the site from Samson Trail to BMES can be added.
 - o FC the applicant will consider this proposal if funding is available.
- CC inquired about construction timeline. DZ indicated that the preliminary timeline for construction was to break ground as soon as weather / site conditions allowed in the spring of 2022, likely April or May, and likely run through October of 2022 barring any unforeseen slowdowns.
- CC asked how construction traffic would interface with school traffic. DZ indicated that the construction manager would coordinate all construction access, and logistics, as is typical on most occupied construction sites.
- CC asked will the information we requested in this meeting be answered. DZ indicated that factual information requested, student counts, etc. would be responded to and incorporated into the meeting report which will be publicly available with the rest of the CUP documents.
 - o FC comments are responses to questions the questions that were not answered during the meeting.
- CC asked how will contractor labor be housed during construction? DZ indicated that this would be an item to be addressed by the contractors at the time of bidding, and qualified local crews would be able to bid the project to possibly alleviate some of the potential temporary housing issues.
- CC asked what the Division of Building Safety reviewed, specifically if they reviewed life safety issues due to traffic. DZ explained that DBS reviews fire and life safety, accessibility, building code compliance, etc. within the building.
- CC asked who reviews the site traffic, and a district representative indicated that the site traffic review was conducted by the McCall fire department.
- CC noted that those portions of the building roof were reinsulated and may not match original drawings. DZ indicated that Design West did the architectural work for the added insulation construction work, and has access to the retrofit documents.
- CC noted that the school had commissioned a spring water study of the site.
 - o FC Design West will request this document from the district and share it with the site design team.

Attachments:

Sign-In Sheet

Sketch Renderings

Site Plan Exhibit

Site Traffic Schematic Exhibit



216 SW 5TH AVE
MERIDIAN, ID 83642
TEL: (208) 888-1768
FAX: (208) 955-6885

Meeting Report

Payette Lakes Classroom Expansion
Neighborhood meeting (digital)
9-8-2021 7:00-8:00 PM

Meeting Attendees

- Dion Zimmerman
- Bill Hamlin
- Jason Clay
- Thaddeus Hoffman Neighbor
- Wade Schwark Neighbor
- Shauna Enders Neighbor
- Sheree Sonfield Neighbor
- Bob Bechaud Neighbor
- Kristen McCoy Neighbor

Prepared by:

Dion Zimmerman

Date Issued: 09.09.21

Distribution:

- City of McCall Community Development

Meeting Report: The following represents the understanding of discussions held and decisions reached in this meeting. The minutes are organized in subject order and do not necessarily follow the chronological order of the meeting. Follow up comments and clarifications are in response to items during the meeting that the applicant did not readily have an answer to, but indicated they would research and clarify for the report.

DZ-Dion Zimmerman

CC-Community Comment

FC- Follow Up Comment, or Clarification from applicant after the meeting.

PLMS – Payette Lakes Middle School

BMES- Barbara Morgan Elementary School

- DZ presented a summary of the proposed classroom renovation as indicated in the attached exhibits
- DZ acknowledged that the 1994 Conditional use permit required the district to dedicate 35' at the southern edge of the property for future road extension.
- DZ presented the traffic control plan as indicated in attached exhibit and explained this was completed in 2019 but revisions continue as we develop a traffic revision to the site. We will continue to work with the engineering teams and the City of McCall to come up with a solution that is safe and works for traffic of the school. We want to be good neighbors but a roadway that will cost millions of dollars is not in the school bond or budget.
- CC asked if the School owns the road on the school property or is it owned by the City? Is the existing school road on the edge of the southern property line of the 35' ROW. BH indicated that to his knowledge the city has not taken possession of the road but has the right to do so when and if they choose.
- CC inquired about construction timeline. DZ indicated that the preliminary timeline for construction was to break ground as soon as weather / site conditions allowed in the spring of 2022, likely April or May, and likely run through October of 2022 barring any unforeseen slowdowns.
- CC 40+ students live in TWL subdivision and many others are dropped off within the subdivision near the rec. center.

- CC Will the street development and the traffic study be continued to be developed?
- CC The traffic is getting worse since Ponderosa Park had directed traffic down the Spring Mountain Road. Something may need to be done with turning lanes.
- CC The parking at graduation and football games is hectic and drivers park along Sampson Trail and cause congestion. DZ-you are not the first to state this and Jason Clay is looking at potentially adding parking on site or perhaps using the open state-owned lot adjacent to Sampson Trail.

Attachments:

Site Plan Exhibit

3D Renderings

Site Traffic Schematic Exhibit

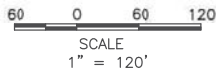
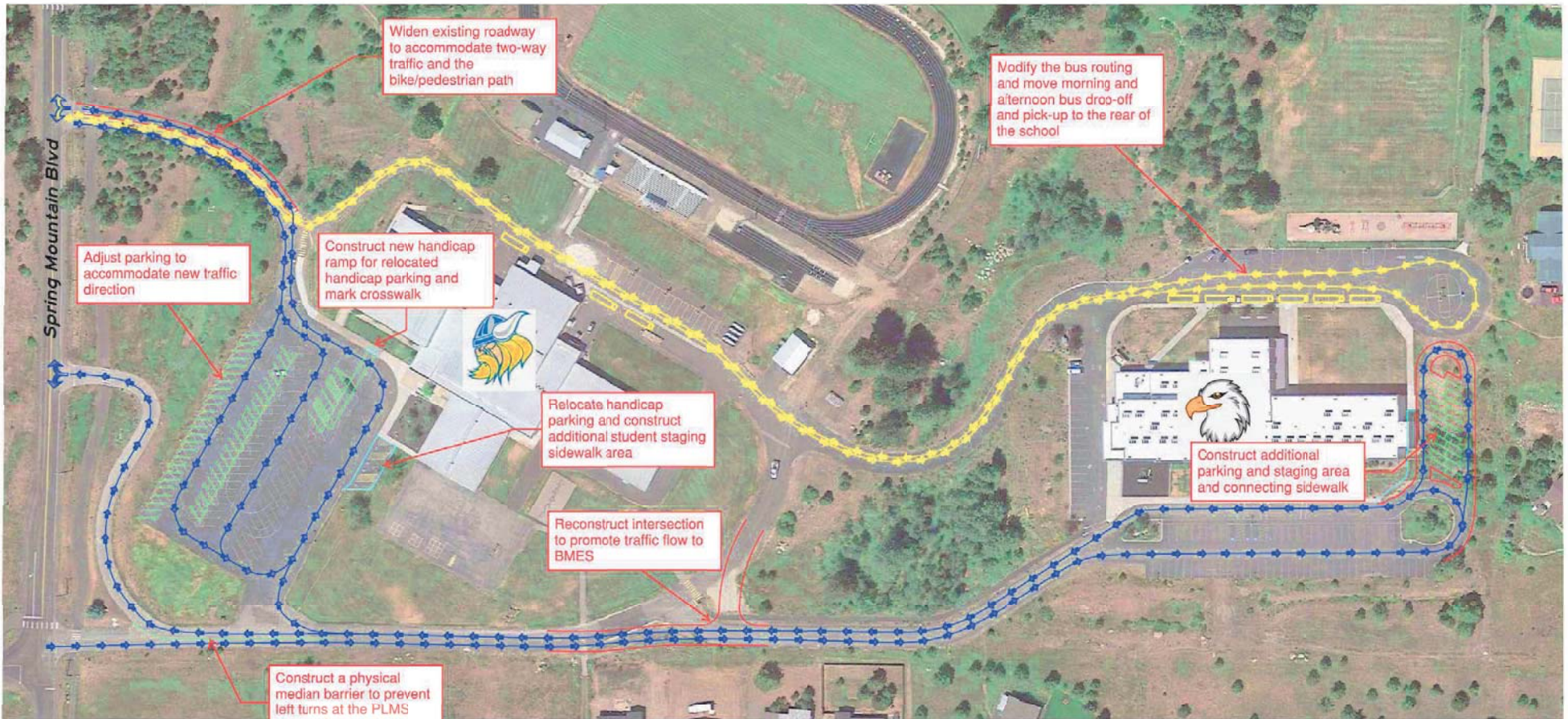


PROJECT: **PAYETTE LAKES SIGN IN**

DATE: **9-1-2021**

ARCHITECTS RE:

NAME	EMAIL	ADDRESS
John & Ginger Riggins	westlbr@yahoo.com	662 Koski Dr McCall
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Sheree Sonfield	sonfield@telus.net	664 Woodlands Dr, McCall
Bob Bechaud	bechaud@telus.net	664 Woodlands Dr.
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Marjorie Elliott	mkelliott44@gmail.com	661 Woodlands Dr.
Jacey Wilkinson	sswilk@msn.com	678 Koski Dr



LEGEND	
	Bus Route
	Bus Staging/Loading
	Drop-Off/Pick-Up Route
	New Access Road & Parking Areas
	Pavement markings

EXHIBIT 7 2020-2021 SCHOOL YEAR CHANGES MAP

BARBARA R MORGAN ELEMENTARY &
PAYETTE LAKES MIDDLE SCHOOL
TRAFFIC FLOW ANALYSIS

PAYETTE LAKES MIDDLE SCHOOL - FLOOR PLAN



Existing landscape area

possible new landscape area



JULY 23, 2021
SCALE: NOT TO SCALE

11,983 S.F. TOTAL ADDITION
4,409 S.F. TOTAL REMODEL

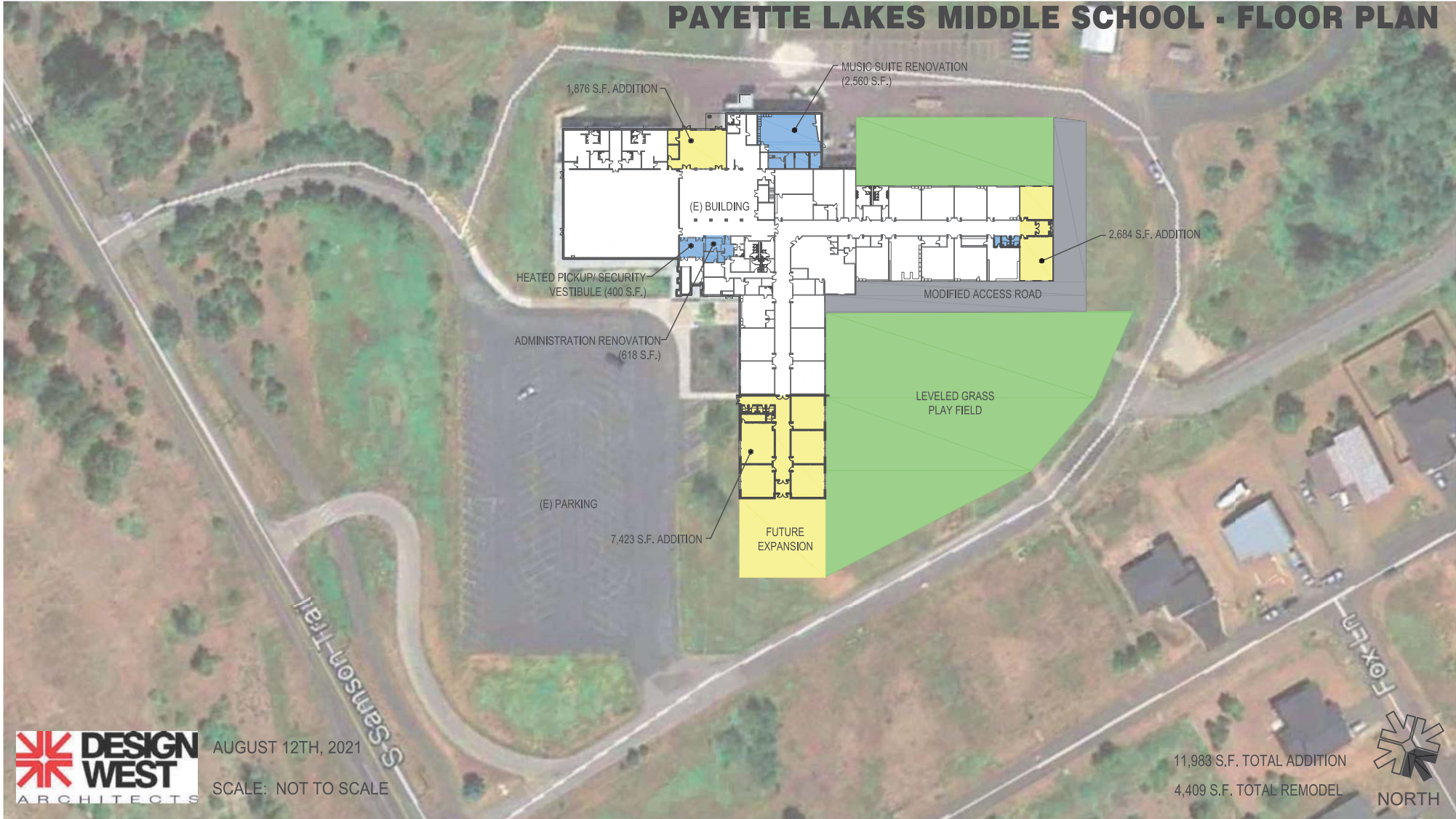


NORTH

PAYETTE LAKES MIDDLE SCHOOL - ADDITIONS



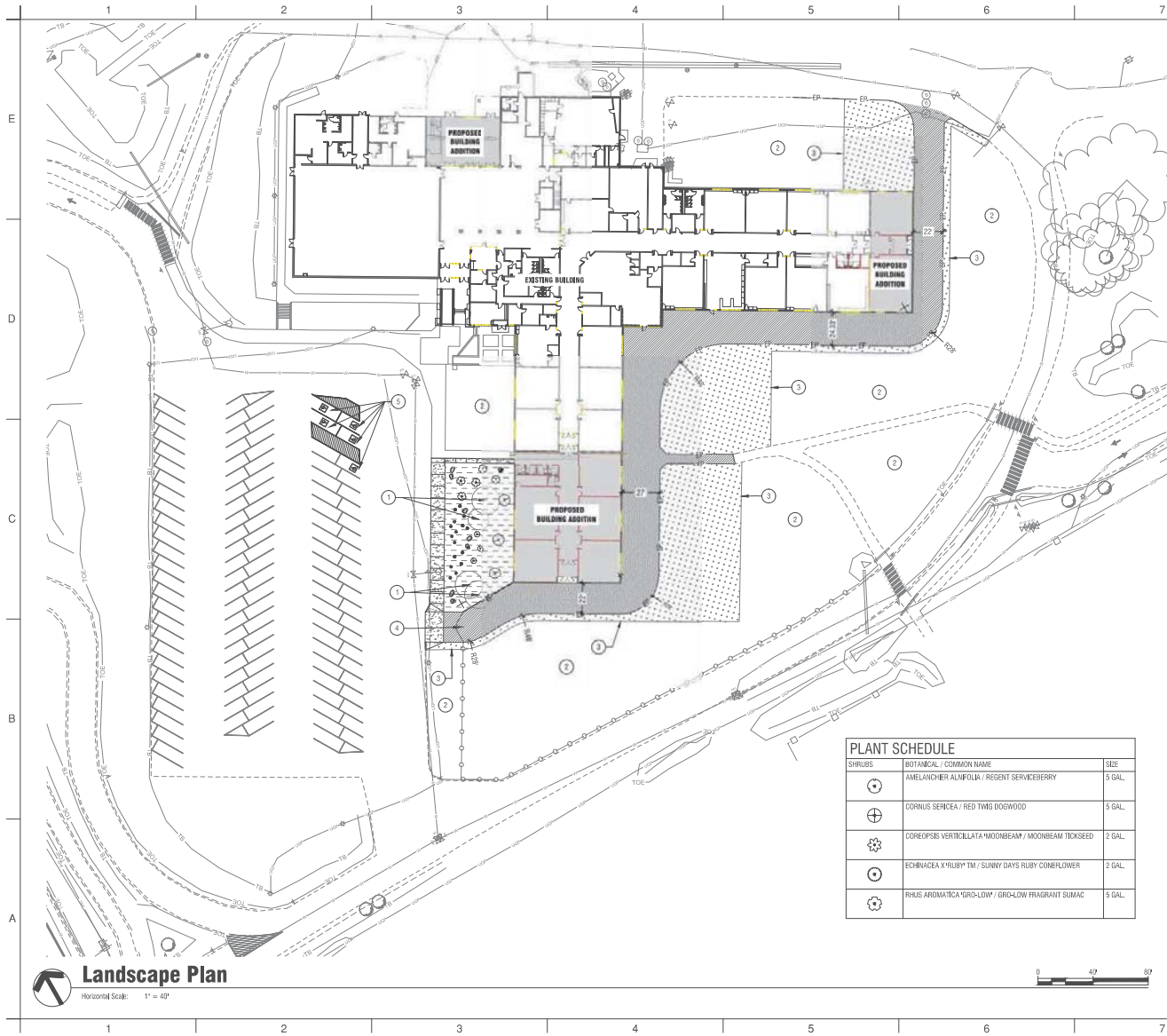
PAYETTE LAKES MIDDLE SCHOOL - FLOOR PLAN



AUGUST 12TH, 2021
SCALE: NOT TO SCALE

11,983 S.F. TOTAL ADDITION
4,409 S.F. TOTAL REMODEL





Landscape Plan Notes:

- A. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO INITIATION OF ANY DEMOLITION OR CONSTRUCTION OPERATIONS. ANY DAMAGE TO EXISTING UTILITIES SHALL BE CONTRACTOR'S RESPONSIBILITY.
- B. ALL PLANT MATERIAL SHALL CONFORM TO THE CURRENT AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z66.1) SPECIFICATIONS.
- C. ALL PLANT MATERIAL SHALL BE INSTALLED AS PER DETAILS AND CONTRACT SPECIFICATIONS.
- D. CONTRACTOR SHALL COORDINATE PLANTING WITH IRRIGATION CONTRACTOR.
- E. NO SUBSTITUTIONS WILL BE ALLOWED WITHOUT THE WRITTEN CONSENT OF THE LANDSCAPE ARCHITECT.
- F. ALL NON-TURF PLANTERS SHALL RECEIVE 3" LAYER OF DECORATIVE ROCK MULCH.
- G. IN THE EVENT OF A DISCREPANCY, NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY.

Tree Protection Notes:

- A. PROTECT THE CRITICAL ROOT ZONE OF THE TREES TO BE RETAINED ON SITE. (NOTE: CRITICAL ROOT ZONE IS THE AREA DIRECTLY BELOW THE DRIP LINE OF THE TREE.)
- A.A. CONSTRUCT PROTECTIVE FENCING OF CHAIN-LINK AROUND THE CRITICAL ROOT ZONE PRIOR TO DEMOLITION OR CONSTRUCTION.
- A.B. DO NOT ALLOW COMPACTION BY EQUIPMENT TRAFFIC DURING CONSTRUCTION OR DURING DEMOLITION.
- A.C. DO NOT ALLOW CEMENT TRUCKS TO RISE WITHIN THE PROTECTION AREA, ANYWHERE THAT TREE ROOTS EXIST OR IN PLANNED PLANTING BEDS.
- A.D. DO NOT STOCKPILE MATERIALS, DEBRIS OR DIRT WITHIN THE TREE PROTECTION AREA.
- A.E. MAINTAIN WATERING WITHIN THE CRITICAL ROOT ZONE FROM MID-MARCH TO MID-OCTOBER AT THE RATE OF NOT LESS THAN THE EQUIVALENT OF 1-1.2" OF WATER OVER THE ENTIRE AREA PER WEEK.
- A.F. DO NOT TRUCKS, EXCAVATE, FILL OR OTHERWISE OBTURB THE SOIL WITHIN THE CRITICAL ROOT ZONE.
- A.G. ADJUST PROPOSED IMPROVEMENT LOCATIONS AS REQUIRED TO AVOID DAMAGING TREE ROOTS.
- B. PROTECT THE CROWN AND TRUNK OF TREES TO BE RETAINED ON SITE.
- B.A. OPERATE EQUIPMENT IN SUCH A WAY AS TO AVOID CONTACT WITH TREE TRUNKS OR BRANCHES.
- B.B. PRUNING OF PUBLIC PROPERTY TREES SHALL BE PERFORMED BY A LICENSED ARBORIST.
- C. ALL TREES DAMAGED OR DESTROYED DURING CONSTRUCTION SHALL BE REPLACED USING A 4" CALIPER TREE MATCHING SPECIES OF THE DAMAGED/DESTROYED TREE.

Material Legend:

	ASPHALT PAVING		STANDARD CONCRETE FLATWORK
	TURF SOD		3" DEPTH DECORATIVE ROCK MULCH TO MATCH EXISTING
	LARGE BASALT BOULDERS, 3-4" DIA. TYP.		

Fence Legend:

6" CHAIN LINK FENCE (MATCH EXISTING FENCE HEIGHT)

EXISTING CHAIN LINK FENCE

Keynotes:

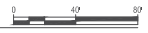
- 1. RETAIN AND PROTECT EXISTING TREES. REFER TO TREE PROTECTION NOTES THIS SHEET FOR MORE INFORMATION.
- 2. RETAIN AND PROTECT EXISTING LANDSCAPE AND IRRIGATION.
- 3. CREATE SMOOTH TRANSITION FROM NEW SOD TO EXISTING LAWN.
- 4. 22-FT WIDE CHAIN LINK GATE.
- 5. RELOCATED ADA PARKING.

Automatic Underground Irrigation Notes:

- A. ALL LANDSCAPED AREAS SHALL HAVE AN AUTOMATIC UNDERGROUND SPRINKLER SYSTEM WHICH ENSURES COMPLETE COVERAGE AND PROPERLY ZONED FOR REQUIRED WATER USES.
- B. EACH HYDROZONE IS TO BE IRRIGATED WITH SEPARATE INDIVIDUAL STATIONS.
- C. POP-UP SPRINKLER HEADS SHALL HAVE A MINIMUM RISE HEIGHT OF 18" AT PLANTER BEDS, 6" AT LAWN AREAS.
- D. PLANTER BEDS ARE TO HAVE DRIP IRRIGATION SYSTEMS - WITH DRIP CONTROL ZONE KIT AND 150 MESH FILTER (MIL).
- E. ELECTRONIC WATER DISTRIBUTION/TIMING CONTROLLERS ARE TO BE PROVIDED. MINIMUM CONTROLLER REQUIREMENTS ARE AS FOLLOWS:
 - E.A. PRECISE INDIVIDUAL STATION TIMING
 - E.B. RUN TIME CAPABILITIES FOR EXTREMES IN PRECIPITATION RATES
 - E.C. AT LEAST ONE PROGRAM FOR EACH HYDROZONE
 - E.D. SUFFICIENT MULTIPLE CYCLES TO AVOID WATER RUN-OFF
 - E.E. POWER FAILURE BACKUP FOR ALL PROGRAMMED INDIVIDUAL VALVED WATERING STATIONS WILL BE DESIGNED AND INSTALLED TO PROVIDE WATER TO RESPECTIVE HYDRO-ZONES.
- F. INDIVIDUAL VALVED WATERING STATIONS WILL BE DESIGNED AND INSTALLED TO PROVIDE WATER TO RESPECTIVE HYDRO-ZONES.
- G. THE IRRIGATION SYSTEM SHALL BE DESIGNED TO PROVIDE 100% COVERAGE WITH HEAD TO HEAD SPACING OR TRIANGULAR SPACING AS APPROPRIATE.
- H. SPRINKLER HEADS SHALL BE ADJUSTED TO REDUCE OVERSPRAY ONTO IMPERVIOUS SURFACES SUCH AS SIDEWALKS, DRIVEWAYS, AND PARKING AREA.
- I. EACH VALVE SHALL BE INSTALLED IN A VALVE BOX LARGE ENOUGH TO ALLOW FOR MAINTENANCE AND REMOVAL, ONLY ONE VALVE PER BOX.

PLANT SCHEDULE		
SHRUBS	BOTANICAL / COMMON NAME	SIZE
	AMELANCHIER ALNIFOLIA / REGENT SERVICEBERRY	5 GAL.
	CORNUS SERICEA / RED TIGER DOGWOOD	5 GAL.
	COREOPSIS VERTICILLATA / MOONBEAM / MOONBEAM TICKSEED	2 GAL.
	ECHINACEA X 'SUNNY DAYS' RUBY COMEFLOWER	2 GAL.
	RHUS AROMATICA 'GRD-LOW' / GRD-LOW FRAGRANT SUMAC	5 GAL.

Landscape Plan
Horizontal Scale: 1" = 40'



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SPOKANE, WASHINGTON
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DRAWN BY: JMG
CHECKED BY: JMG
JOB NUMBER: 21053

REVISIONS



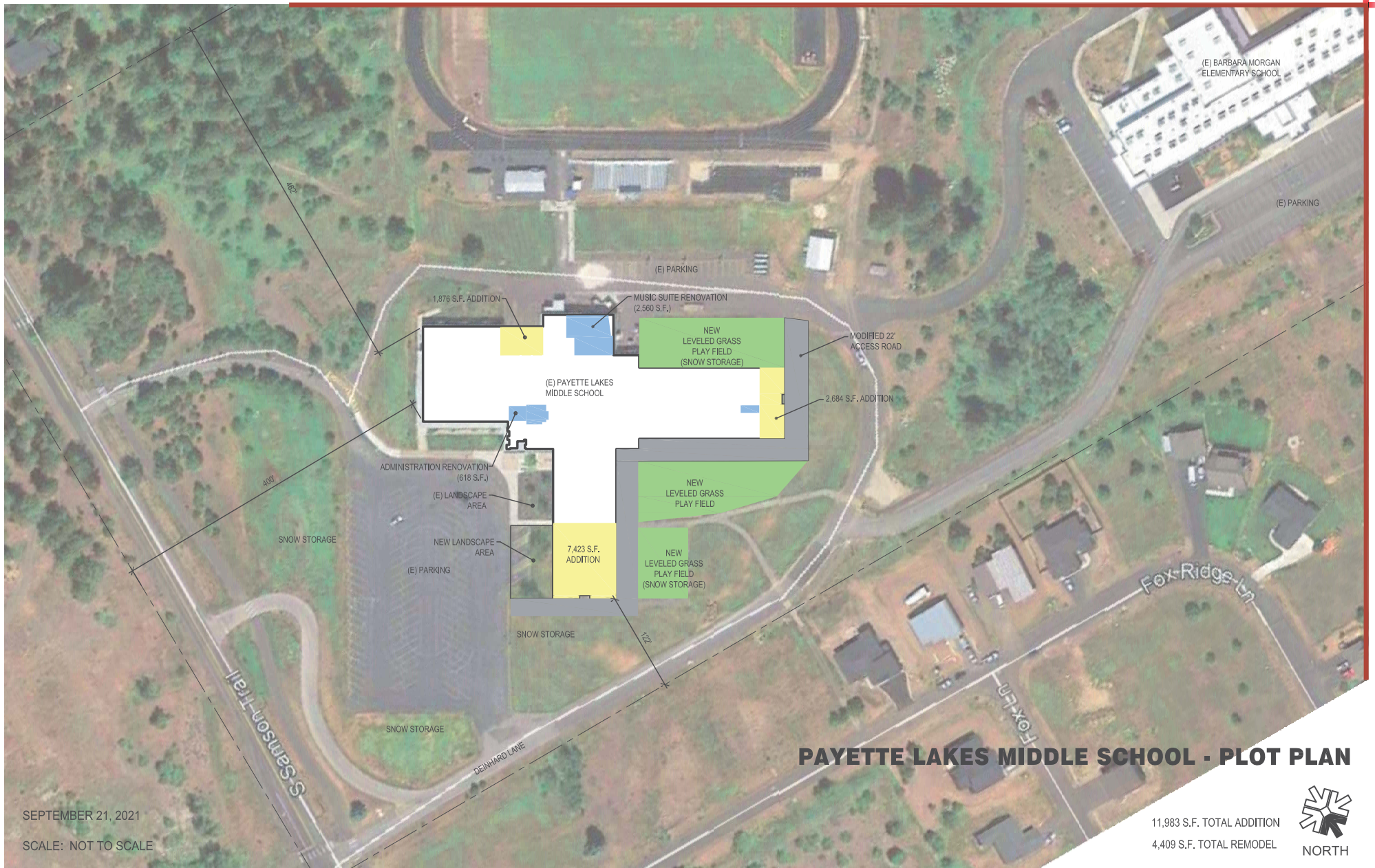
PAYETTE LAKES MIDDLE SCHOOL ADDITION
111 S SAMISON TRAIL
MCCALL, ID 83638

DATE: 09/21/21

SHEET NAME: Landscape Plan

SHEET: L1.00





PAYETTE LAKES MIDDLE SCHOOL - PLOT PLAN

SEPTEMBER 21, 2021
 SCALE: NOT TO SCALE

11,983 S.F. TOTAL ADDITION
 4,409 S.F. TOTAL REMODEL

 NORTH

Payette Lakes Middle School



East Elevation (small addition proposed on end of wing)



South Elevation



Southwest Elevation (addition proposed on end of wing)



North Elevation

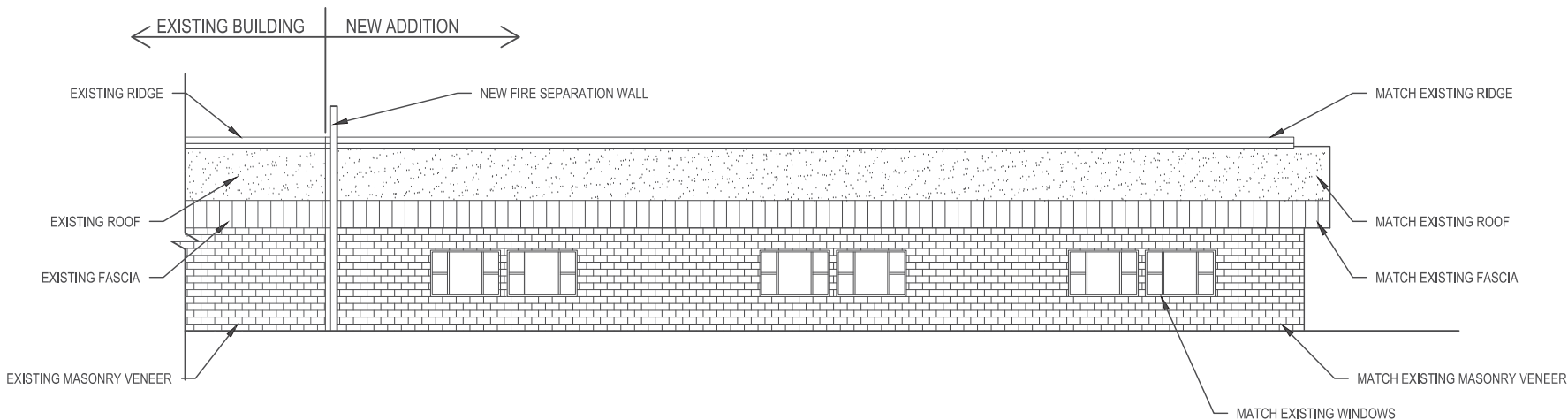


Northeast Elevation (proposed infill addition)

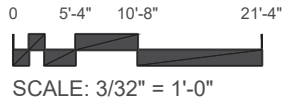
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By Brian Parker at 12:39 pm, Oct 11, 2021

A



WEST ELEVATION, SOUTH WING



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10/11/21

ELEVATION A

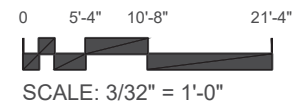
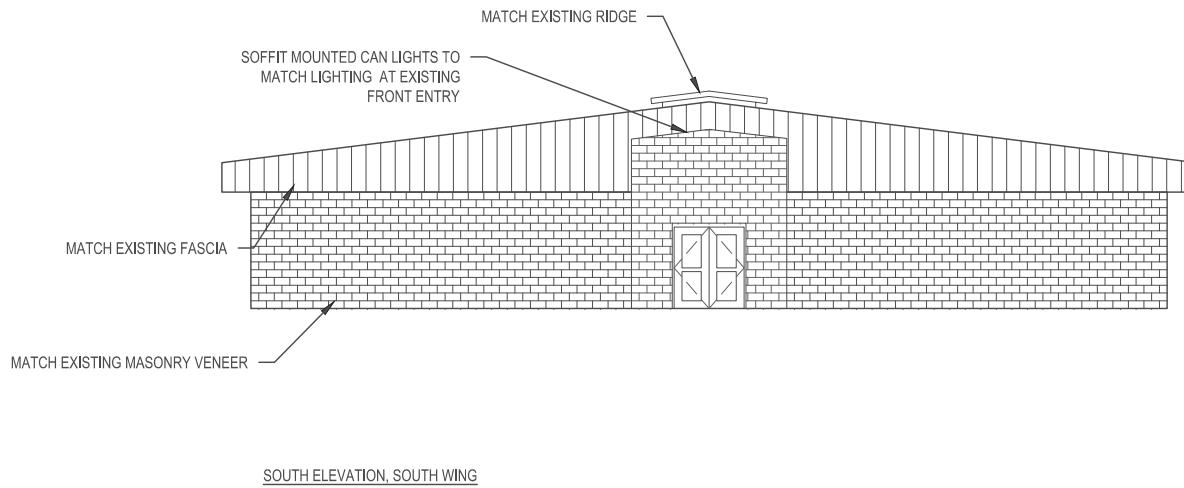


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B



PAYETTE LAKES MIDDLE SCHOOL ADDITION
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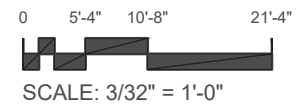
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JOB NUMBER 21053	DATE 10/11/21
ELEVATION B	

C



EAST ELEVATION, SOUTH WING

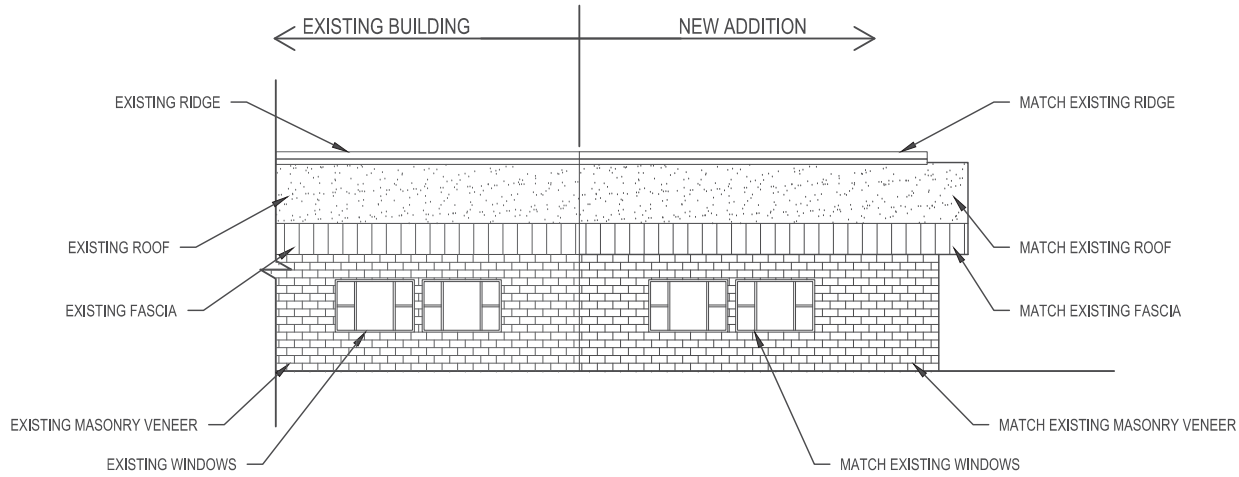


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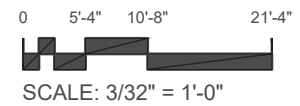
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PAYETTE LAKES MIDDLE SCHOOL ADDITION
111 S. SAMSON TRAIL
MCCALL, IDAHO 83638

D



SOUTH ELEVATION, EAST WING



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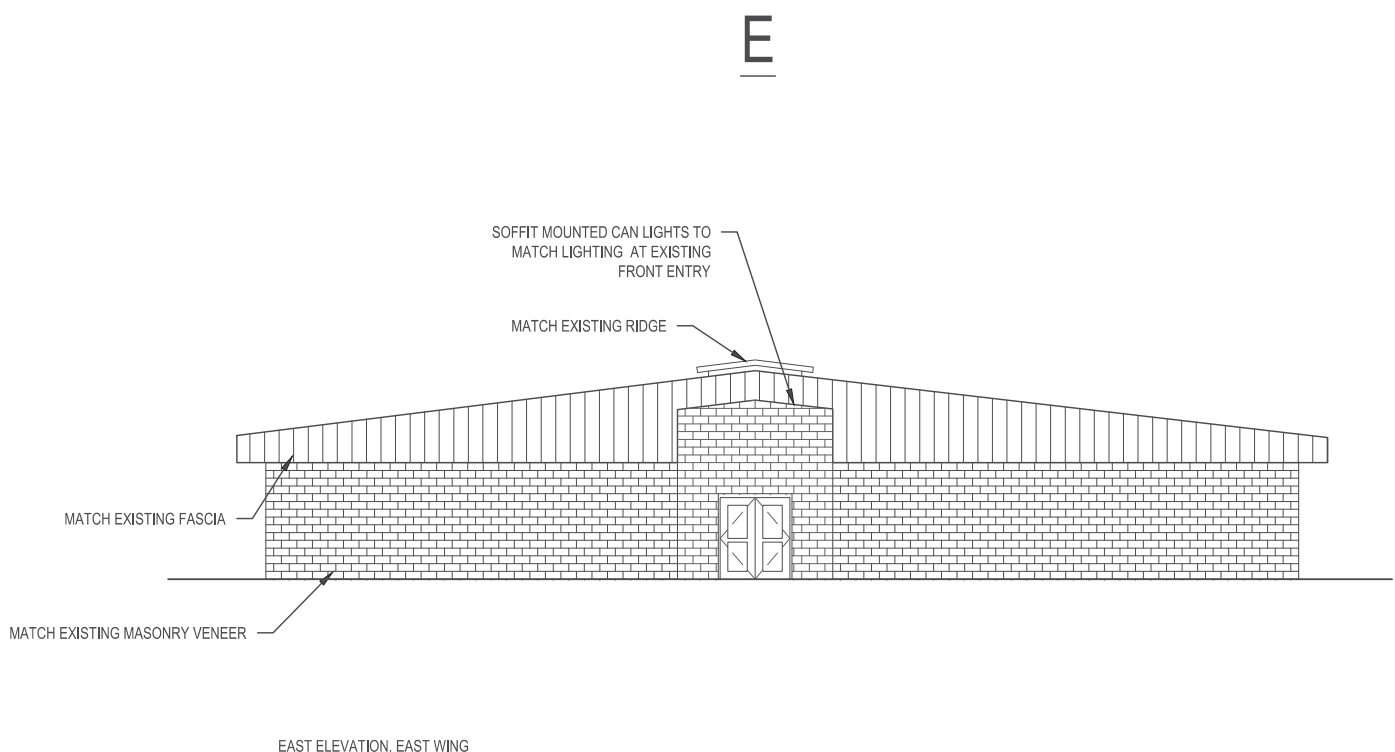
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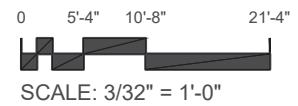
JOB NUMBER
21053

DATE
10/11/21

ELEVATION D



EAST ELEVATION, EAST WING



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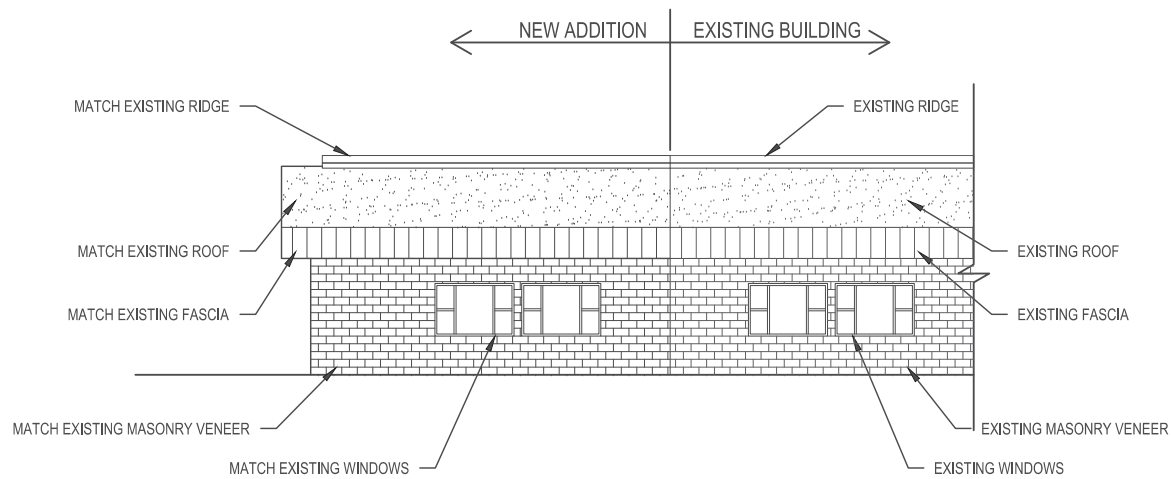
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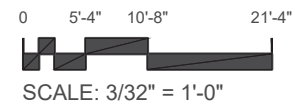
DATE
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ELEVATION E

F



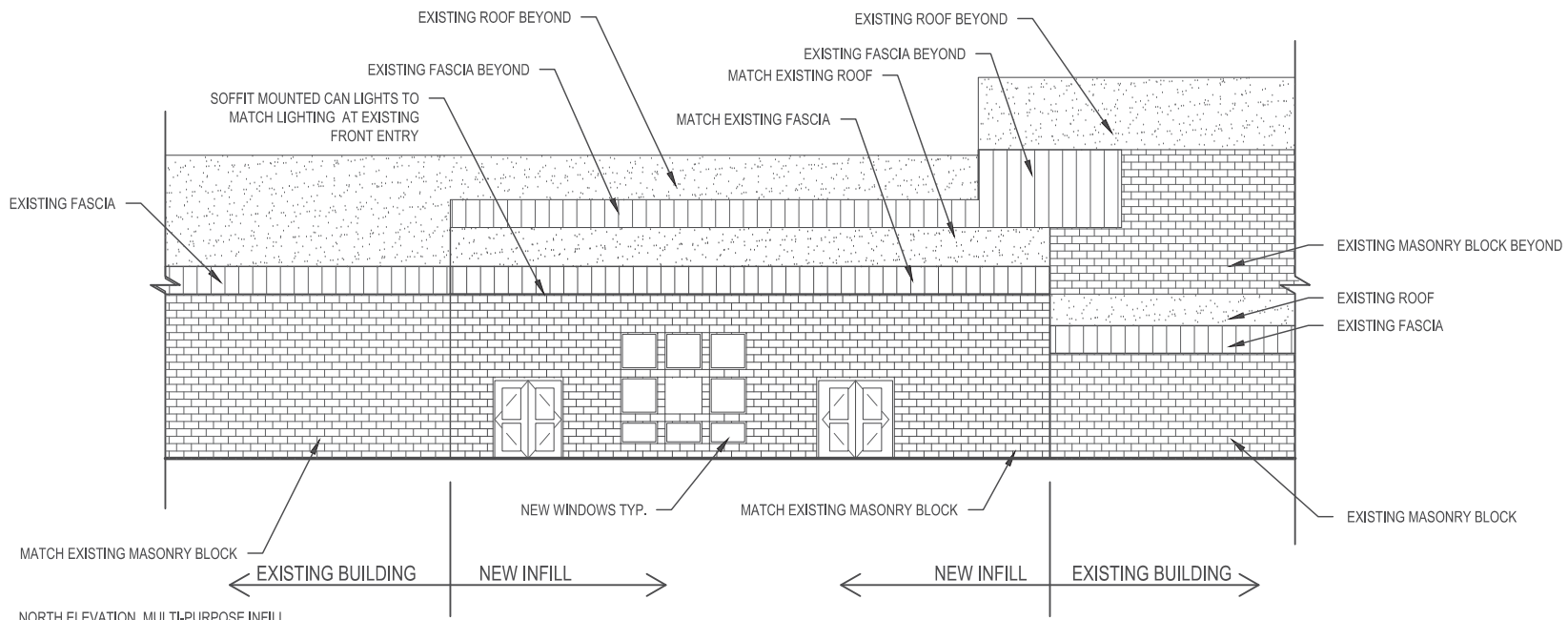
NORTH ELEVATION, EAST WING



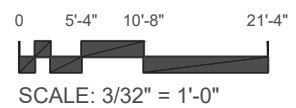
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	ELEVATION F	

PAYETTE LAKES MIDDLE SCHOOL ADDITION
111 S. SAMSON TRAIL
MCCALL, IDAHO 83638

G



NORTH ELEVATION, MULTI-PURPOSE INFILL



JOB NUMBER
21053

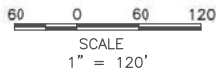
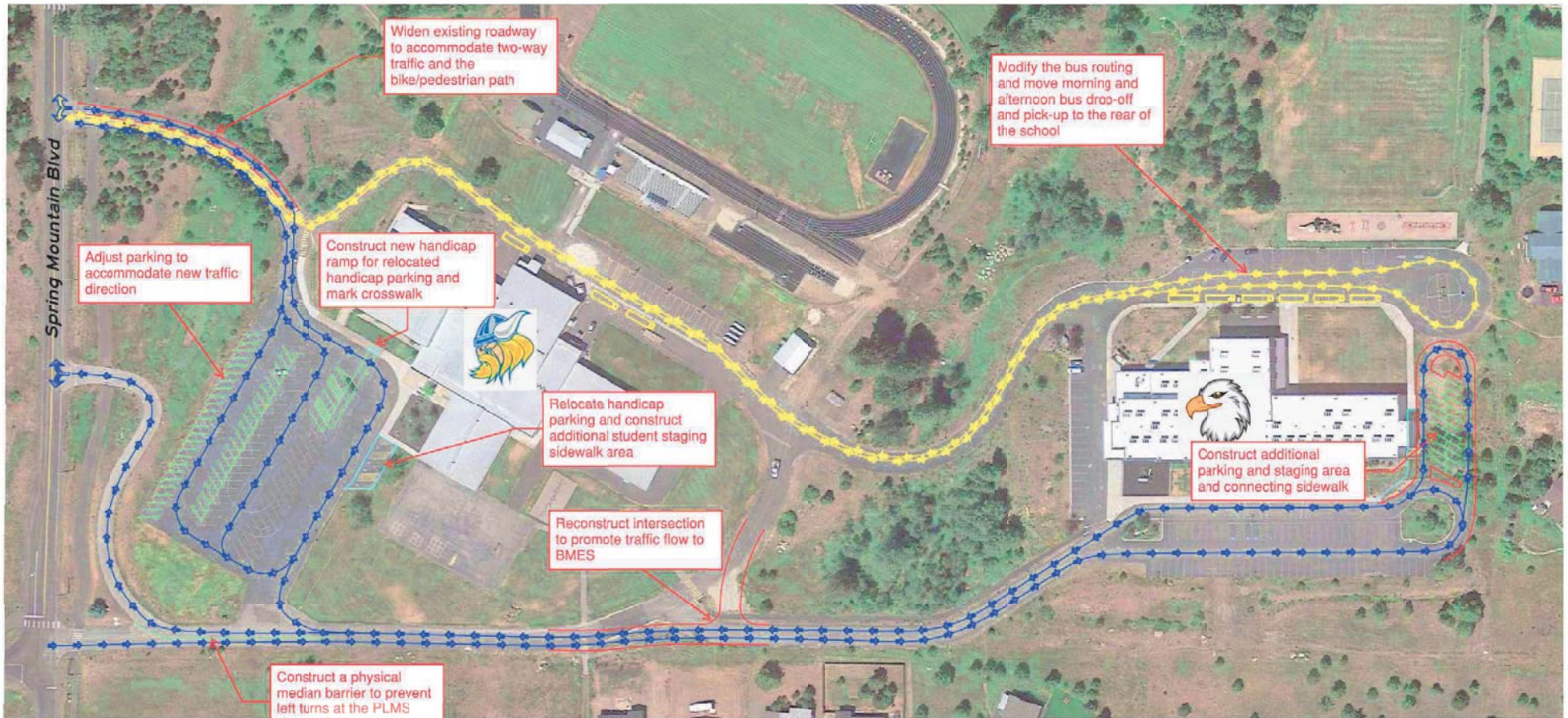
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ELEVATION G

PAYETTE LAKES MIDDLE SCHOOL ADDITION
111 S. SAMSON TRAIL
MCCALL, IDAHO 83638



LEGEND	
	Bus Route
	Bus Staging/Loading
	Drop-Off/Pick-Up Route
	New Access Road & Parking Areas
	Pavement markings

EXHIBIT 7 2020-2021 SCHOOL YEAR CHANGES MAP

BARBARA R MORGAN ELEMENTARY &
PAYETTE LAKES MIDDLE SCHOOL
TRAFFIC FLOW ANALYSIS

RECEIVED

By Brian Parker at 9:27 am, Nov 18, 2021



TRAFFIC IMPACT STUDY

FOR THE

**PAYETTE LAKES MIDDLE SCHOOL
&
BARBARA MORGAN ELEMENTARY SCHOOL
CAMPUS**

NOVEMBER 17, 2021

Prepared By:
Paragon Consulting, Inc.
157 W. 4th Street
Kuna, Idaho 83634



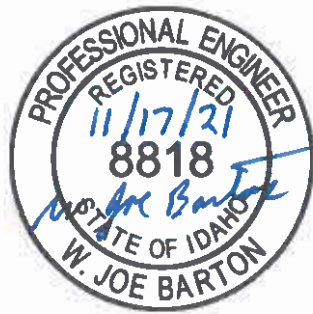
TRAFFIC IMPACT STUDY

FOR THE

PAYETTE LAKES MIDDLE SCHOOL & BARBARA MORGAN ELEMENTARY SCHOOL

COMPLETED FOR THE

McCall-Donnelly School District



November 17, 2021

ENGINEER:

*PARAGON CONSULTING, INC.
157 W. 4TH STREET
KUNA, IDAHO 83634*

OWNER:

*MCCALL-DONNELLY SCHOOL DISTRICT
120 IDAHO STREET
MCCALL, IDAHO 83638*

Telephone: (208) 922-9138

Telephone: (208) 634-2161

**PAYETTE LAKES MIDDLE SCHOOL
&
BARBARA MORGAN ELEMENTARY SCHOOL
TRAFFIC IMPACT STUDY**

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Appendix E: Traffic Analysis Worksheets (Future Campus Capacity Redistributed to Reflect a Campus North Approach Entry Lane to the Middle School and With Mitigation Along Public Roadways)

1.0 Executive Summary

The McCall-Donnelly School District (MDS) Plans to remodel and expand the Payette Lakes Middle School to accommodate five additional classrooms. The Payette Lakes Middle School shares a common campus with the Barbara Morgan Elementary School. The school campus is located northeast of the E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) intersection. The addition of five middle school classrooms is anticipated to **expand the school enrollment capacity by 125 students based on 25 students per classroom.**

The Payette Lakes Middle School expansion also includes site construction of the following significant features:

- Realign the on-campus intersection of the Barbara Morgan Elementary School access road and the Payette Lakes Middle School north parking lot access road to accommodate better traffic flow to the elementary school
- Regrade the playground area located between the south and east wings of the Payette Lakes Middle School to accommodate a more useful activity area for the students
- Relocate the handicap parking at the Payette Lakes Middle School and expand the available student pick-up and drop-off area
- Modify the existing campus entrance at the intersection of E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) to accommodate pedestrian and bicycle access from the public pathway system to the internal middle school and elementary school pathways

1.1 Summary of Report Findings

Existing Conditions

All intersections and school campus approaches to the public roadway system (E Deinhard Ln and S Samson Trail intersection, school campus middle exit and school campus north exit) **operate at acceptable levels under the current traffic and school enrollment conditions.**

On campus traffic circulation and on campus vehicle queueing currently has minimal to no impact on the public roadway system and intersections. However, under some circumstances, the student drop-off and pick-up vehicle queue at the Payette Lakes Middle School has the potential to spill back and impact the E Deinhard Lane and S Samson Trail intersection. Therefore, expanding the available student drop-off and pick-up area at the Payette Lakes Middle School will reduce the potential for this queueing to spill back to the intersection.

Pedestrian and bicycle access to the school campus is currently available at the following locations:

- Near the northwest corner of the school property, to the public pathway along S Samson Trail
- Near the northeast corner of the school property, to the Woodlands Subdivision
- Midway along the south school property boundary, to the Fox Ridge Subdivision

However, direct bicycle and pedestrian access to the campus internal pathways is not available near the E Deinhard Ln and S Samson Trail intersection, resulting in some bicycle and pedestrian traffic within the vehicle travel areas.

Future Conditions

All intersections and school campus approaches to the public roadway system **will continue to operate at acceptable levels up to the current school student capacities.** **Therefore, no campus access improvements are necessary until current enrollment grows by another 120 students, to an enrollment of 850 students** on campus. Furthermore, the campus access and circulation plan modifications identified in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report

will also operate at acceptable levels up to a student enrollment of 850 students on the campus. These modifications include converting the campus north exit to two-way traffic for direct access to the Middle School and prohibiting left turns at the Middle School parking lot connection to the Elementary School access road.

Growing beyond the enrollment of 850 students on the campus will necessitate improvements within the public roadways and intersections. The following improvements will operate at acceptable levels through the Payette Lakes Middle School remodel capacity (adding 125 students to the current campus capacity of 850 students) plus 25% growth of the local traffic: ^{120?}

25% growth over how many years?

- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot
- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road
- E Deinhard Lane and S Samson Trail intersection
 - Convert the existing southbound to westbound right turn lane to a free running right which operates outside the stop sign requirements on the southbound approach. Merge the free running right receiving lane on E Deinhard Lane with the existing travel lane west of the intersection.
- S Samson Trail
 - Widen S Samson Trail to extend three lanes from the E Deinhard Lane and S Samson Trail intersection to north of the school campus property.

A very large off-set is created with this layout. Which I would think would create more issues.

What would the 3 lanes accommodate? 2 thru's and a right turn or is it a center left turn?

As available ground to the south and east of the school property develops, the development should extend a public roadway to the school property boundary and provide a parking area or turnaround. Bicycle and pedestrian access to/from the school property to the parking area or turnaround should be constructed.

could include vehicular connections if it is warranted.

Expanding the available student drop-off and pick-up area at the Payette Lakes Middle School should be included with the Middle School remodel/expansion site work to reduce the potential for vehicle queuing to spill back to the public roadway system.

Site work associated with the Payette Lakes Middle School remodel/expansion should include modifying the existing campus entrance at the intersection of E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) to accommodate pedestrian and bicycle access from the public pathway system to the internal middle school and elementary school pathways, therefore completing bicycle and pedestrian access to all quadrants of the school property.

Are there more pedestrian/bicycle facilities that need to be constructed other than what is being proposed with the expansion?

2.0 Introduction

Paragon Consulting, Inc. has been retained to provide a Traffic Impact Study for the proposed improvements at the existing Payette Lakes Middle School and Barbara Morgan Elementary School campus, near the intersection of E Deinhard Lane and S Samson Trail in McCall, Idaho. This TIS evaluates the potential traffic impacts resulting from the proposed work at the school campus and provides information related to campus access, student drop-off and pick-up areas, bicycle and pedestrian access and other associated items.

2.1 Project Description

The McCall-Donnelly School District (MDSD) operates the Payette Lakes Middle School and the Barbara Morgan Elementary School on a common school campus located northeast of the E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) intersection.

The MDSD contracted with Paragon Consulting, Inc. to complete a Traffic Flow Analysis in 2019. The purpose of the analysis was to identify potential internal, on-site, campus traffic flow issues and

recommend possible mitigation to accommodate vehicle queueing during student drop-off and pick-up times. The Traffic Flow Analysis was completed in July 2019 and provides a good starting base line for the traffic analysis associated with the expansion plans at the Payette lakes Middle School.

The MDSD is currently working towards construction of a building expansion at the Payette Lakes Middle School to add five classrooms, along with associated site construction. The MDSD intends to construct the building expansion and associated site work in the 2022 construction season. Site work construction includes the following:

- Realign the on-campus intersection of the Barbara Morgan Elementary School access road and the Payette Lakes Middle School north parking lot access road to accommodate better traffic flow to the elementary school
- Regrade the playground area located between the south and east wings of the Payette Lakes Middle School to accommodate a more useful activity area for the students
- Relocate the handicap parking at the Payette Lakes Middle School and expand the available student pick-up and drop-off area
- Modify the existing campus entrance at the intersection of E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) to accommodate pedestrian and bicycle access from the public pathway system to the internal middle school and elementary school pathways

The MDSD has future (beyond 2022) plans for additional site improvements at the school campus. The date of future improvements and the order in which future improvements are prioritized will depend on available budget/funding and continued monitoring of campus use, area traffic and student enrollment. Some of the future site improvements may include the following:

- Reconstructing the asphalt surface within the Middle School parking lot
- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot
- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road
- Constructing additional vehicle queueing area at the Elementary School to better accommodate student drop-off and pick-up
- Partnering with the City of McCall to widen S Samson Trail to accommodate three vehicle lanes from E Deinhard Land to north of the school property

2.2 Scope of This Report

This Traffic Impact Study analyzes the transportation related elements associated with the existing Payette Lakes Middle School and Barbara Morgan Elementary School campus and future impacts associated with increases in student enrollment and the proposed increase in student capacity at the Middle School. The following key elements are included within this Traffic Impact Study:

- Analysis of existing intersections and school access locations
 - E Deinhard Lane & S Samson Trail (campus south access)
 - Campus middle access to S Samson Trail
 - Campus north access to S Samson Trail
 - Middle School parking lot approach to the Elementary School access road
- Bicycle and pedestrian access to the school campus
- Site traffic circulation and vehicle queueing associated with student drop-off and pick-up
- Future traffic projections
- Analysis of intersections and school access locations under future traffic conditions
- Additional considerations items identified in Idaho Code 67-6519(3) for school facilities

2.2 Intersection Performance Criteria

Roadway intersection and school approach performance is measured using the *Highway Capacity Manual* procedures. The *Highway Capacity Manual* procedures identify a Level of Service (LOS) based on the average delay of vehicles traveling through an intersection. The LOS ranges from LOS A, representing the best conditions, to LOS F, representing the worst or failing conditions. For this Traffic Impact Study, **a minimum LOS of D is considered acceptable**, as this is typical in urban or semi-urban areas.

It should be noted that when completing intersection analysis using the *Highway Capacity Manual* procedures, the analysis is based on the peak 15-minute traffic within the analysis peak hour. This results in a worst-case traffic scenario, indicating the intersection will likely experience better conditions during all other periods of the day. Traffic around school sites is typically condensed around the student drop-off and pick-up times. **As a result, the peak 15-minute traffic near school sites results in quite conservative results with respect to LOS.** Therefore, outside the peak 15-minute traffic period, intersections are anticipated to perform with considerably less delay to the vehicles traveling through the intersection.

3.0 Existing Conditions

The existing conditions at the Payette Lakes Middle School and the Barbara Morgan Elementary School are analyzed to provide an understanding of what significant elements are impacting traffic flow on the school campus and campus access locations.

3.1 School Enrollment & Capacities

What about traffic generated by faculty? Are they included in these counts?

Payette Lakes Middle School

The Payette Lakes Middle School was initially constructed on the school campus and opened in 1996. The Middle School currently has a student capacity of 400 students based on 16 classrooms and 25 students per classroom. The current 2021/2022 school year student enrollment at the Middle School is 323 students. This is a slight increase over the student enrollment of 312 for the 2019/2020 school year.

Barbara Morgan Elementary School

The Barbara Morgan Elementary School was added to the school campus in 2007. The Elementary School currently has a student capacity of 450 students based on 18 classrooms and 25 students per classroom. The current 2021/2022 school year student enrollment at the Elementary School is 410 students. This is a slight decrease over the student enrollment of 414 for the 2019/2020 school year.

School Campus

The school campus, with the Middle School and the Elementary School, has a current student capacity of 850 students and a current student enrollment of 733 students. **Therefore, approximately 120 additional students may be enrolled on the campus before existing student capacity is exhausted.**

3.2 Existing Roadway, Bicycle & Pedestrian facilities

Roadways and Vehicular Access

The Payette Lakes Middle School and Barbara Morgan Elementary school campus is currently accessed via three public roadway approaches along S Samson Trail (Spring Mountain Boulevard). All entering vehicles, including school staff, parents, buses and delivery vehicles, enter the campus approach at the E Deinhard Lane and S Samson Trail intersection. This single entrance lane provides access to the Middle School and Elementary School parking lots as well as access to the rear of the Middle School and Elementary School.

All vehicles exiting the school campus to S Samson Trail via the campus middle approach and the campus north approach. The campus middle approach primarily serves vehicles exiting from the Middle School and the campus north approach primarily serves vehicles exiting from the Middle School as well as vehicles exiting from the rear to the Middle School and the Elementary School.

Additional descriptions and exhibits relative to the internal campus roadways and traffic flows are provided in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report.

Figure 1 shows the existing vehicle lane configuration and the intersection control (stop sign locations) for the three campus approaches to the public roadway system.

Bicycle and Pedestrian Facilities

The City of McCall maintains a public bicycle and pedestrian pathway system along E Deinhard Lane and along S Samson Trail. The pathway extends to Highway 55, to the west. The pathway along S Samson Trail extends from Fox Ridge Road, approximated 200 feet south of the school campus, across the school campus frontage and continues north of the campus.

The on-campus bicycle and pedestrian facilities currently have direct access to the public pathway system at the north campus approach. However, the location of this campus access is not conducive to use by pedestrians and bicycles approaching the school from the west or south. This results in pedestrians and cyclists using the vehicle access at the E Deinhard Lane & S Samson Trail approach, intermixing pedestrians, bicycles and vehicles.

Additional pedestrian and bicycle access to the school campus is currently available near the northeast corner of the school property, to the Woodlands Subdivision. This on-campus pathway provides direct access to the Woodland Subdivision recreation center parking lot.

Finally, a third bicycle and pedestrian access to the school campus is located midway along the south school property boundary, to the Fox Ridge Subdivision. This access extends to Fox Ridge Road.

Figure 2 depicts the existing bicycle and/or pedestrian access to the school campus as well as proposed bicycle and/or pedestrian facilities proposed for construction with the Middle School remodel/expansion project.

3.3 Existing Site Traffic Circulation & Queueing

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report provides relative information and exhibits for the on-site traffic circulation and vehicle queueing. The discussion in this section highlights and expands on some of the information presented in the 2019 report.

General Site Circulation

All school campus traffic, including parents, school staff, school buses and delivery vehicles currently enter the school campus at the intersection of E Deinhard Lane and S Samson Trail (campus south approach). Once on campus, drivers have the option of proceeding straight to the Elementary School or turning left into the Middle School parking lot.

The 2019 report identified the left turn movement into the Middle School parking lot as a potential issue during inclement weather and slick road conditions. Although this left turn movement functions well during dry roadway conditions, some drivers may have difficulty getting their vehicle moving after stopping to make the left turn during slick roadway conditions. because the entry road is on an incline from the intersection.

Figure 1

Existing Intersection Control & Lane Configuration

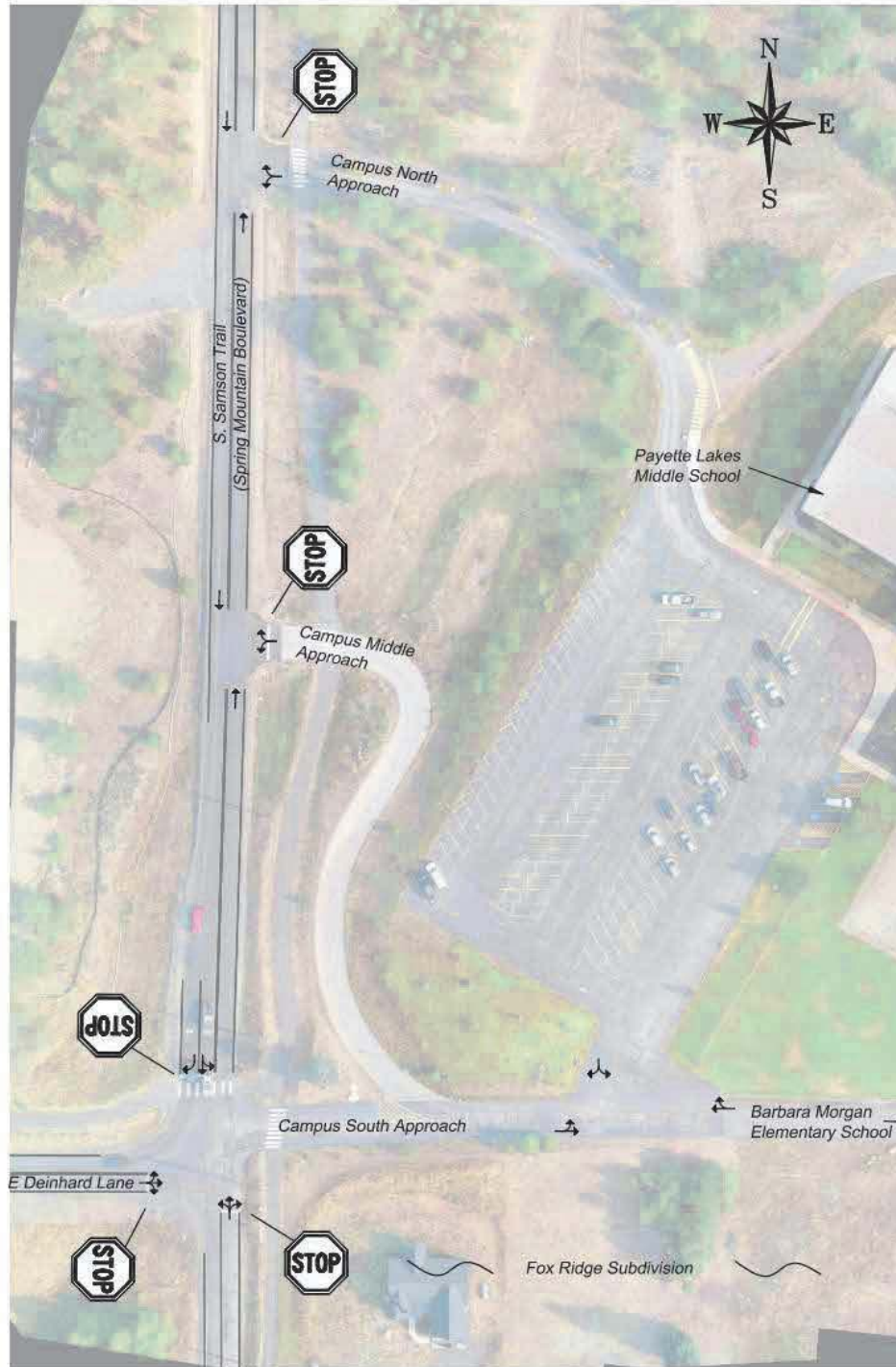
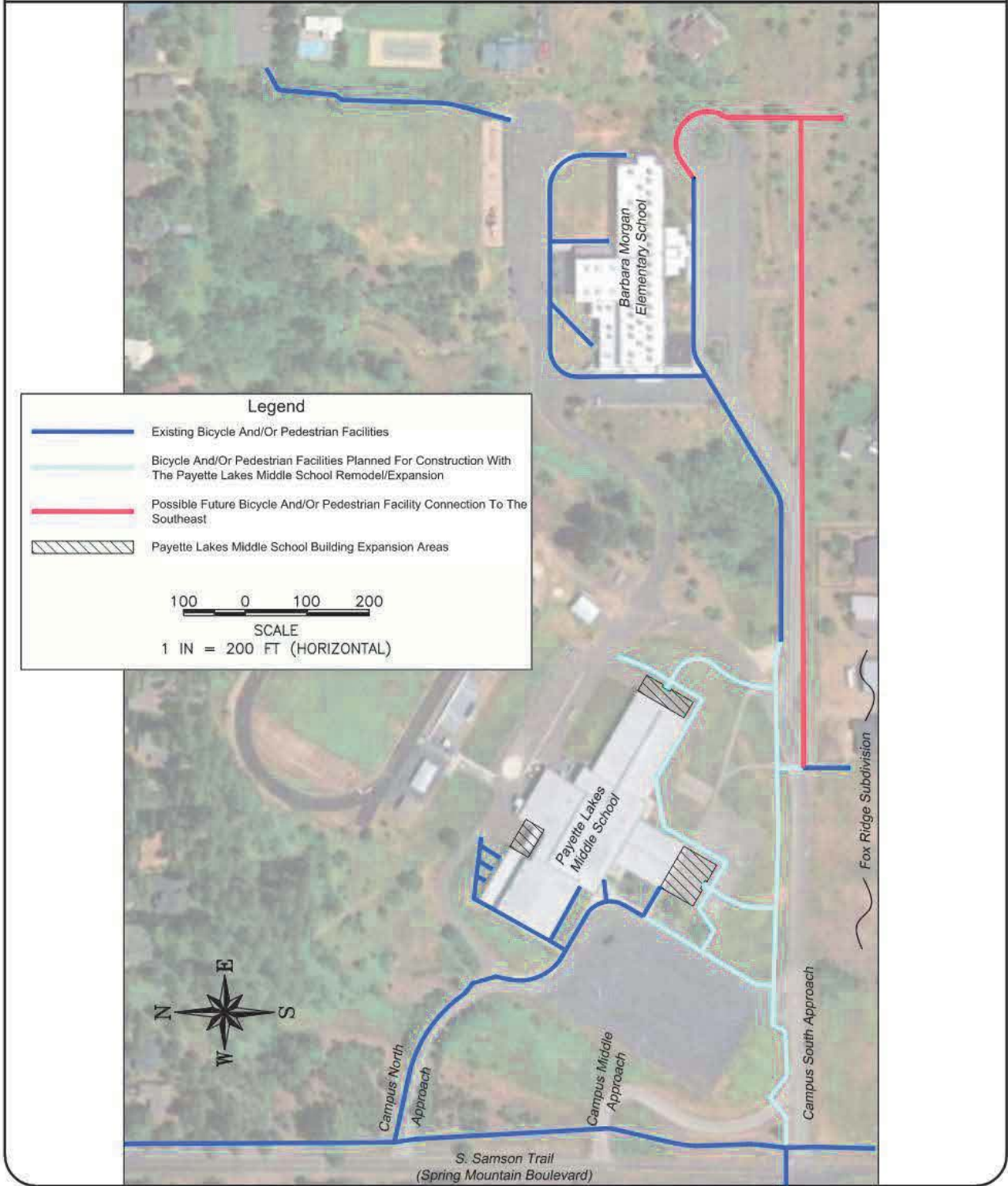


Figure 2

Payette Lakes Middle School & Barbara Morgan Elementary School
Campus Bicycle & Pedestrian Access Plan



Traffic exiting the campus has the option of using the campus middle approach or the campus north approach from either the Elementary School or the Middle School. However, almost all Elementary School traffic currently exits at the campus middle approach and almost all Middle School traffic currently exits at the campus north approach.

Payette Lakes Middle School Vehicle Queueing

During student drop-off and pick-up at the Payette Lakes Middle School, vehicles are staged along the drive isle on the east side of the Middle School parking lot. However, there is currently limited area with adjacent sidewalk available for the drop-off and pick-up zone. Additionally, drivers do not typically block the handicap parking area, therefore reducing the available vehicle queueing area.

School administration and staff indicated that the Middle School vehicle staging occasionally backs up to the Elementary School access road. This queueing backup has the potential to spill back onto the Elementary School access road which could then impact traffic at the campus south approach (E Deinhard Lane and S Samson Trail).

The proposed site construction planned with the Middle School remodel/expansion includes relocating the handicap parking at the Payette Lakes Middle School and expanding the available student pick-up and drop-off area by extending the sidewalk out to the Elementary School access road. This will significantly improve the efficiency of the Middle School student drop-off and pick-up lane.

Barbara Morgan Elementary School Queueing

During student drop-off and pick-up at the Barbara Morgan Elementary School, vehicles are staged along the curb in front of the school and in the parking stalls on the south side of the school. The vehicle staging line has been observed to back up around the traffic circle, through the parking lot and down the hill towards the Middle School.

Although the vehicle queueing at the Elementary School presents some difficulties at the Elementary School, this vehicle queueing currently has no impacts to the public roadway system or the traffic circulation near the Middle School.

The MDSD would like to complete some future on-site improvements to adjust where the vehicles are queued with respect to the parking area and access road. However, it is unlikely that current budgets will allow improvements near the Elementary School. at this time?

School Bus Circulation & Loading

Bus traffic on the campus is currently mixed with the vehicular traffic. Currently, buses proceed to the Elementary School where they stage adjacent to the parking stalls on the west side of the school, while additional buses proceed directly to the backside (northeast side) of the Middle School. Following release of the Elementary School students in the afternoon, the buses staged at the Elementary School proceed to the backside of the Middle School where some students must change buses prior to departing the campus. Approximately twelve school buses entered and exited the school campus in the peak hour traffic counts completed on September 22, 2021.

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report includes some suggestions for separating the campus bus traffic from the general traffic. These recommendations were based on bus route and time logistics in that separating the bus traffic reduced the potential for buses to get hung up in the general traffic, therefore throwing off route times. Separating bus traffic from general traffic on the internal campus site is not anticipated to have an impact to traffic conditions on the public roadway intersections. any positive impacts?

School buses exit the campus via the campus north approach, intermixed with the general traffic exiting primarily from the Middle School.

The queuing from the middle school will back up the elementary school, which may block the intersection? I thought the concern was the vehicles queuing in the access road to the middle school because they are waiting to make a left at the middle school parking lot.

So vehicles wanting to go to the middle school for pick-up/drop-off will:
1. enter the campus.
2. Go around the elementary school parking lot.
3. If the parking lot is full, queue along the access road going west until they can make it to the parking lot, or the child makes their way to the vehicle via sidewalk.

3.4 Existing Traffic & Traffic Adjustments

Traffic Counts

Traffic counts were taken on Monday, April 15, 2019 to support the analysis completed in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report. Information pertaining to the 2019 traffic counts can be found in the referenced report.

Updated traffic counts were taken to support the analysis completed within this Traffic Impact Study. These counts were taken on September 22, 2021. The weather during the traffic count was sunny with temperatures in the morning around 40 degrees and temperatures in the afternoon around 70 degrees. The roadways were dry and weather was not a factor impacting traffic flow.

The morning drop-off traffic count revealed the **a.m. peak hour of traffic** was from 7:30 a.m. to 8:30 a.m. with the peak 15 minutes occurring between 7:45 a.m. and 8:00 a.m. No significant traffic congestion was observed on the morning of the traffic count.

The afternoon pick-up traffic count revealed the **p.m. peak hour of traffic** was from 2:30 p.m. to 3:30 p.m. with the peak 15 minutes occurring between 3:00 p.m. and 3:15 p.m. As with the a.m. traffic count, no significant traffic congestion was observed on the afternoon of the traffic count.

Heavy vehicles (school buses, dump truck, large deliver vehicles, semi-trucks, etc.) were counted separately from general traffic. There were 32 heavy vehicles counted during the a.m. peak hour, comprising of approximately 4.3% of the total traffic at the E Deinhard Lane and S Samson Trail intersection. There were 28 heavy vehicles counted during the p.m. peak hour, comprising of approximately 4.6% of the total traffic at this intersection. **Based on this data, 5.0% heavy vehicles were used for all intersection analysis within this report.**

In addition to the vehicle traffic, there were eight pedestrians/bicycles that crossed the S Samson Trail crosswalk at E Deinhard Lane during the a.m. peak hour and eleven pedestrians/bicycles that used the crosswalk during the afternoon peak hour.

A summary of the a.m. and p.m. peak hour traffic counts collected on September 22, 2021 is presented in Figure 3. A breakdown of the traffic counts, with 15-minutes increments, is included in Appendix A.

Traffic Estimates for Current School Capacities

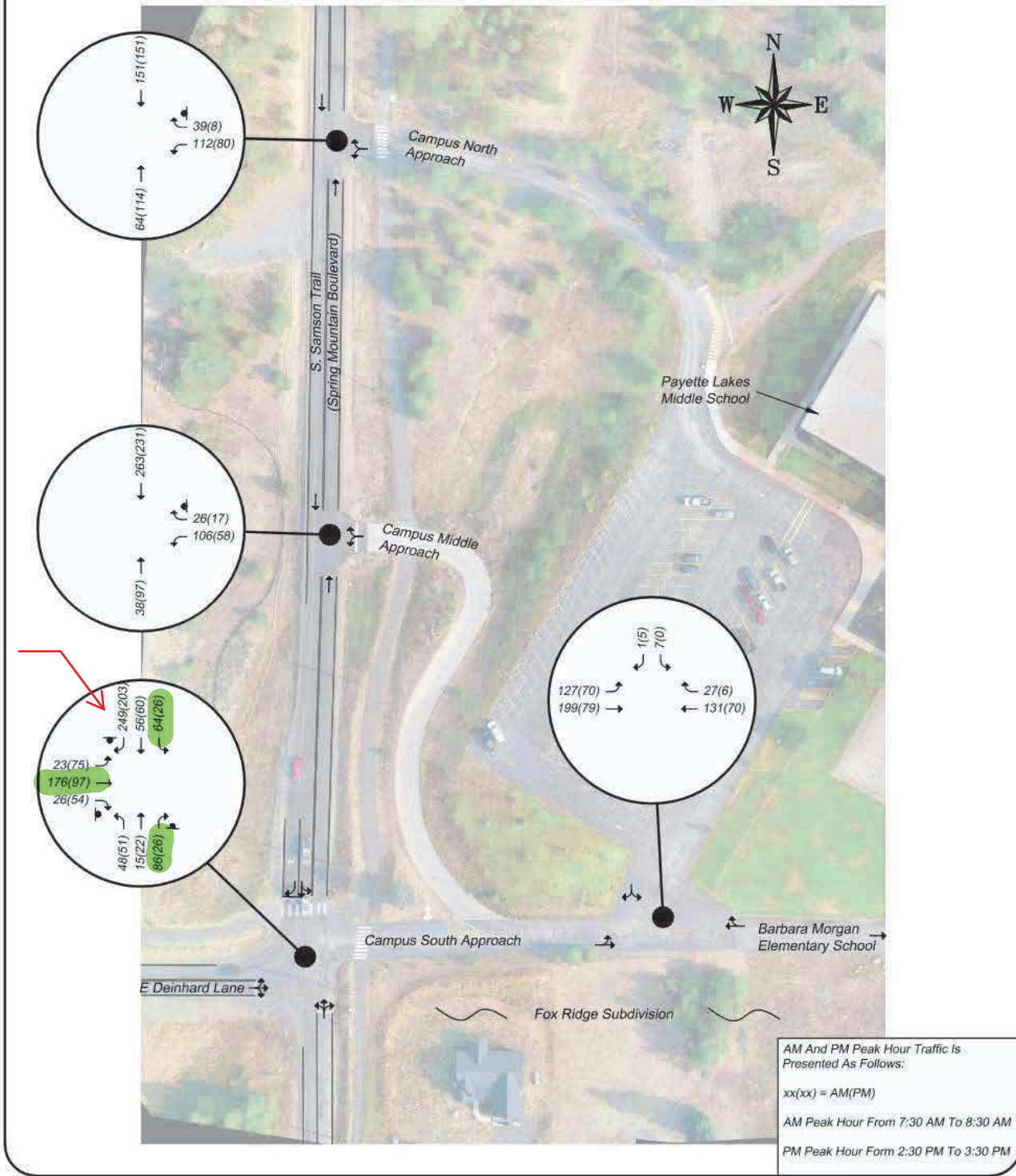
Along with the current student enrollments and school capacities presented in Section 3.1, the MDSD provided additional student attendance information for September 22, 2021 (the day the most recent traffic counts were completed). This attendance information included the following:

- Payette Lakes Middle School
 - 32 students were reported absent
 - Approximately 100 students stayed after school for athletics
- Barbara Morgan Elementary School
 - 39 students were reported absent
 - Approximately 24 students stayed after school for after school programs

Incorporating this attendance data, **the a.m. traffic count would be representative for 291 students in attendance at the Middle School and the p.m. traffic count would be representative for 191 students in attendance at the Middle School.** Similarly, **the a.m. traffic count would be representative for 371 students in attendance at the Elementary School and the p.m. traffic count would be representative for 347 students in attendance at the Elementary School.**

Figure 3

September 22, 2021 Traffic Count Data



To ensure the existing conditions analysis represents the worst-case scenario, the traffic counts that were collected on September 22, 2021 were factored up to be representative of the current student capacity at each school. For example, traffic associated with the Middle School in the a.m. period were factored up by 1.37 (400 student capacity/291 students represented during the a.m. count).

it seems like this would only address the traffic of 64 AM vehicles. Majority (262 AM vehicles) of the vehicles would still need to go through the intersection in some form, even if it is to get to the northern access.

The results of factoring the September 22, 2021 traffic count up to current school capacities is shown in Figure 4.

Traffic Redistribution

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report suggests modifying the campus access to allow entering the campus at the campus north approach. The report also suggests prohibiting left turns at the Middle School parking lot intersection with the Elementary School access road.

The traffic counts shown in Figure 4, representing the current campus student capacity, were redistributed to reflect the suggested modifications to the campus access. These changes would result in all Elementary School traffic entering at the campus south approach (E Deinhard Lane) and exiting at the campus middle approach. All Middle School traffic would enter and exit at the campus north approach.

This traffic redistribution would reduce the total intersection traffic volume at E Deinhard Lane and S Samson Trail by redistributing the traffic entering the Middle School to the campus north approach, effectively removing all traffic coming from the north and going to the Middle School from the intersection.

is this statement assuming that all vehicles south bound on Samson Trail are for the Elementary School?

Redistribution of the traffic representing the current campus capacity provides the opportunity to analyze the suggested modifications to ensure there are no operational deficiencies with the existing lane configuration and controls on the public roadways.

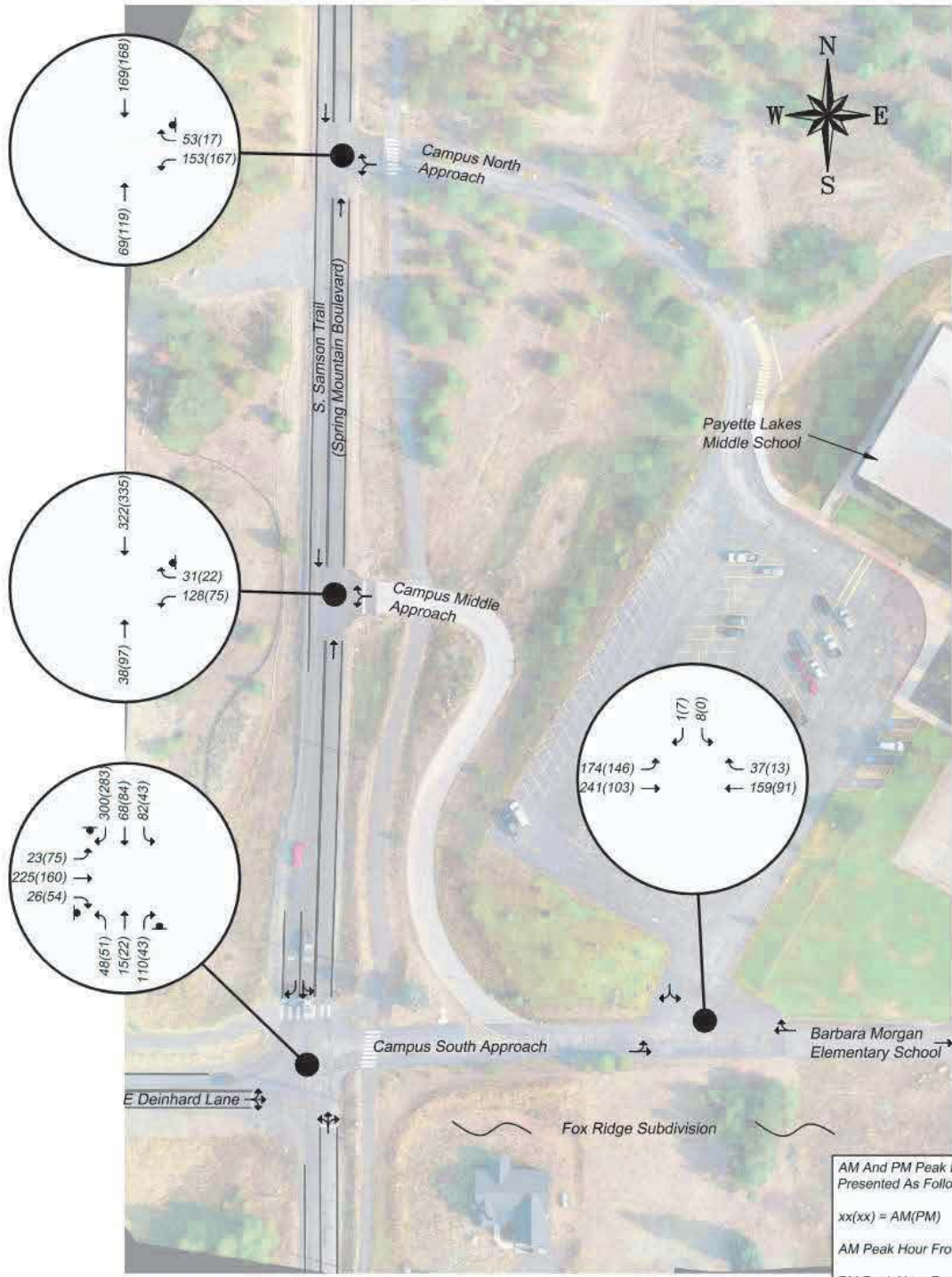
Figure 5 shows the results of redistributing the Figure 4 traffic as described.

How were the different traffic volumes differentiated between PLMS, BMES, and non school traffic?

Was there a separate inflation factor incorporated with BMES?

Figure 4

Traffic Projection
 2021 Traffic Count Projection To Current Campus Capacity
 (450 Elementary Students And 400 Middle School Students)

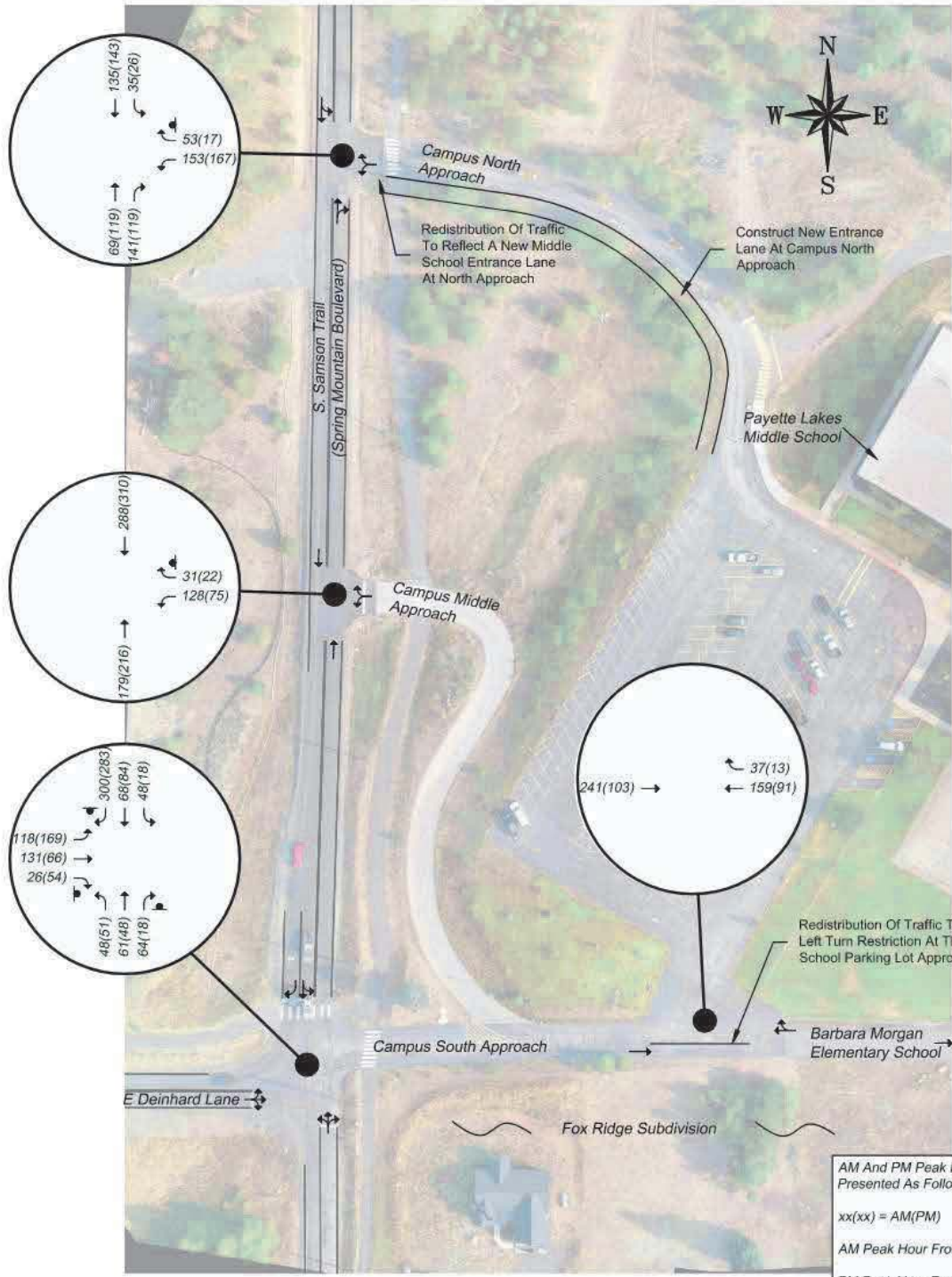


AM And PM Peak Hour Traffic Is Presented As Follows:
 xx(xx) = AM(PM)
 AM Peak Hour From 7:30 AM To 8:30 AM
 PM Peak Hour Form 2:30 PM To 3:30 PM

Figure 5

Traffic Projection & Redistribution

2021 Traffic Count Projected To Current Campus Capacity (450 Elementary Students And 400 Middle School Students)
 Redistribution Associated With A New Middle School Entrance At The Campus North Approach



3.5 Existing Traffic Analysis

Analysis of Traffic Estimates for Current School Capacities

The traffic volumes representing the current campus student capacity, as shown in Figure 4, were analyzed under the existing lane configuration and intersection control, as shown in Figure 1. The results of these analysis are shown in Table 1. Appendix B includes copies of the analysis worksheets.

Table 1 - Intersection Analysis for Current School Capacities and Existing Lane Configurations and Intersection Control					
Intersection	Lane Group	School AM Peak Hour		School PM Peak Hour	
		LOS	Delay (sec.)	LOS	Delay (sec.)
E Deinhard Lane & S Samson Trail (Campus South Approach)	NB-LTR	C	16.0	B	10.7
	SB-LT	C	16.2	B	11.2
	SB-R	D	32.0	B	13.8
	EB-LTR	D	30.4	C	15.8
S Samson Trail & Campus Middle Approach	WB-LR	C	17.8	C	16.2
S Samson Trail & Campus North Approach	WB-LR	B	14.7	B	14.8
PLMS Approach to BMES Access Road	SB-LR	F	56.1	A	9.3
	EB-LT	A	9.4	A	8.4

On-site

As shown in Table 1, all intersections operate at acceptable levels except the southbound Middle School parking lot approach to the Elementary School access road. The Middle School south parking lot exit was only used by nine vehicles during the a.m. peak hour traffic count. Furthermore, the analysis indicates this lane is operating at a volume to capacity level of 0.2 and has only a single vehicle queued at any given time waiting to enter the Elementary School access road. Therefore, no mitigation for this vehicle movement is recommended.

It is also noteworthy that the analysis of the southbound Middle School parking lot approach to the Elementary School access road indicates that only two vehicles are typically queued at the eastbound approach, while a vehicle waits to turn left into the Middle School parking lot. This indicates that the Middle School parking lot entrance is not spilling back into the E Deinhard Lane & S Samson Trail intersection. Additional discussion of the potential for this approach to spill back into the public roadway intersection is included in Section 3.5, Existing Site Traffic Circulation & Queueing.

Analysis of Traffic Redistribution

As described in Section 3.3, Existing Traffic & Traffic Adjustments, the traffic representing the current campus student capacity was redistributed to reflect the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report suggested modifications to the campus access.

The redistributed traffic volumes, as shown in Figure 5, were analyzed with the suggested changes to the campus access locations, including modifying the campus north access to include a campus entry lane for the Middle School. The results of these analysis are shown in Table 2. Appendix C includes copies of the analysis worksheets.

Table 2 - Intersection Analysis for Current School Capacities Redistributed for a Campus North Approach Entry Lane					
					850 students
Intersection	Lane Group	School AM Peak Hour		School PM Peak Hour	
		LOS	Delay (sec.)	LOS	Delay (sec.)
E Deinhard Lane & S Samson Trail (Campus South Approach)	NB-LTR	C	16.7	B	10.9
	SB-LT	B	13.8	B	10.5
	SB-R	D	32.7	B	13.9
	EB-LTR	D	31.7	C	15.9
S Samson Trail & Campus Middle Approach	WB-LR	D	25.7	C	20.3
S Samson Trail & Campus North Approach	WB-LR	C	20.9	C	18.8
	SB-LT	A	8.1	A	8.0

highlighted =worse than before, some slightly, but still worse.

As shown in Table 2, all intersections operate at acceptable levels under the traffic representing the current campus student capacity and traffic redistribution to accommodate an entry lane at the campus north approach.

Note Table 2 does not include analysis results at the Middle School parking lot approach to the Elementary School access road. The proposed redistribution of traffic would include eliminating left turn movements at this location, effectively reducing any delay to minimal amounts.

Existing Traffic Conclusions

Based on site observations and the traffic analysis presented above, it is apparent the intersection of E Deinhard Lane and S Samson Trail will function at acceptable levels until the campus student enrollment reached 850 students, an increase of approximately 120 students over the current 2021/2022 school year enrollment. This intersection will also function at acceptable levels, up to a student enrollment of 850 students on campus, if the traffic was redistributed to reflect the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report suggested modifications to the campus access.

Additionally, both the campus middle approach and the campus north approach will function at acceptable levels up to a student enrollment of 850 students on the campus. The campus approaches will function acceptable under the existing campus access scenario or the suggested modifications to the campus access.

future = 245 additional students @ capacity?

4.0 Future Conditions

The following sections provide an overview of anticipated improvements on the Payette Lakes Middle School and Barbara Morgan Elementary school campus as well as an analysis of the potential impacts of future student enrollment and local traffic growth.

4.1 School Expansion & Site Construction

2022 Construction Plans

The MDSO plans to complete remodel/expansion construction at the Payette Lakes Middle School in 2022. The school expansion will add five middle school classrooms, expanding the school enrollment capacity by 125 students based on 25 students per classroom.

The Payette Lakes Middle School expansion also includes site construction of the following significant features:

- Realign the on-campus intersection of the Barbara Morgan Elementary School access road and the Payette Lakes Middle School north parking lot access road to accommodate better traffic flow to the elementary school
- Regrade the playground area located between the south and east wings of the Payette Lakes Middle School to accommodate a more useful activity area for the students
- Relocate the handicap parking at the Payette Lakes Middle School and expand the available student pick-up and drop-off area
- Modify the existing campus entrance at the intersection of E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) to accommodate pedestrian and bicycle access from the public pathway system to the internal middle school and elementary school pathways
- Miscellaneous site construction necessitated by the Middle School building expansion limits

Potential Future Campus Improvements

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report identified possible future campus improvements beyond those identified for construction with the Middle School remodel/expansion. The date of future improvements and the order in which future improvements are prioritized will depend on available budget/funding and continued monitoring of campus use, area traffic, student enrollment and the ongoing maintenance requirements of the existing infrastructure. Some of the future site improvements may include the following:

- Reconstructing the asphalt surface within the Middle School parking lot
- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot (see Section 4.2, Future Site Access for additional information)
- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road
- Constructing additional vehicle queuing area at the Elementary School to better accommodate student drop-off and pick-up
- **Constructing additional bicycle and pedestrian connections to the currently undeveloped land south and east of the campus (see Section 4.3, Future Bicycle & Pedestrian Access for additional information)**

could include vehicular connections if it is warranted.

4.2 Future Site Access

Modification to Existing Campus Access

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report identified possible future campus access modifications to improve on campus traffic circulation and reduce the potential for on campus traffic to spill back onto the public roadway system. The significant campus access modification identified in the 2019 report include the following:

- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot
- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road

When implemented, these two modifications should be constructed at the same time to effectively separate Middle School and Elementary School traffic entering the school campus. Some of the results of implementing these access changes include the following:

- All drivers visiting the Elementary School would enter the campus at the campus south approach (E Deinhard Lane & S Samson Trail) and the majority of this traffic would exit the campus at the campus middle approach
- All drivers visiting the Middle School would enter the campus at the campus north approach and the majority of this traffic would also exit the campus at the campus north approach
- Providing a vehicle entry lane at the campus north approach would reduce overall intersection traffic at E Deinhard Lane and S Samson Trail
- Vehicles exiting the school campus would be somewhat balanced between the campus middle approach and the campus north approach **slightly, but true**
- Left turn movements at the Middle School parking lot connection to the Elementary School access road would be eliminated, removing the potential for this traffic movement to spill back into the intersection of E Deinhard Lane and S Samson Trail
 - The frequency of stopping on the hill, entering the campus from E Deinhard Lane and S Samson Trail, would be significantly reduced or eliminated resulting in fewer vehicles having difficulty in this area during slick roadway conditions

As discussed in Section 3, either the existing campus approach layout or the approach modifications identified in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report will function at acceptable levels up to a campus student enrollment of 850 students (an increase of 120 students over the current 2021/2022 school year enrollment). **Therefore, the MDSD should plan on implementing the campus approach modification when the campus student enrollment reaches 850 students, or sometime before that threshold.**

Consideration of a New Campus Access to the East

We understand the City of McCall has been approached concerning potential development of available land to the south and east of the Payette Lakes Middle School and Barbara Morgan Elementary School campus. Considering this potential land development, the City and the Developer have requested that the MDSD consider a public roadway through the school property, along the school's southern boundary.

The MDSD has dedicated a 30-foot-wide utility easement along the school's southern property boundary to the City of McCall to allow the City to extend public utilities (specifically domestic water service) through the school property to service land to the south and east of the campus. We are not aware of any dedication of public right-of-way, permanent roadway easement or any prior commitment from the MDSD to extend a public roadway through the school property.

Should be 35' and it was for a future E Deinhard Lane extension from conditions of approval?

From Brian's research, this statement does not seem true anymore.



Furthermore, when the Fox Ridge Subdivision was developed to the south of the school property, the detail of possibly extending E Deinhard Lane to the east was apparently missed by all parties involved.

Within the Fox Ridge Subdivision, there are no dedicated public right-of-way, permanent roadway easements or building setback restrictions that would accommodate extension of E Deinhard Lane along the common property boundary of the Fox Ridge Subdivision and the MDSO property.

The suggestion of extending E Deinhard Lane through the school property, with the entire roadway and public right-of-way to be within the school property and not shared with the adjacent Fox Ridge Subdivision, is detrimental to the MDSO. It may be possible that there is a feasible solution of extending E Deinhard Lane through the school property, but not without extensive analysis (by the land developer and/or the City of McCall) of the impacts and the on-campus mitigation that would be necessary. Some of the challenges that the developer and/or the City of McCall must address during the analysis of the E Deinhard Lane extension proposal include the following:

- The proximity of an E Deinhard Lane extension to the existing school parking areas, the Middle School playground area and the existing internal campus roadways would necessitate a complete redesign and reconstruction of the entire on-campus vehicle and pedestrian/bicycle infrastructure.
- An E Deinhard Lane extension would effectively eliminate the southern on-campus access road connection between the Middle School and the Elementary School. The existing on-campus northern access road is not currently used as access for the general public. This would further necessitate a rework of the entire on-campus vehicle and pedestrian/bicycle infrastructure.
- As described previously in this report, we recommend a future change to the current campus access that would eliminate left turn movements at the Middle School parking lot connection to the Elementary School access road, therefore removing the potential for this traffic movement to spill back into the intersection of E Deinhard Lane and S Samson Trail. As proposed by the developer/City of McCall, the extension of E Deinhard Lane would include a Middle School parking lot entrance at this location. The distance from the E Deinhard Lane and S Samson Trail intersection to the proposed Middle School parking lot approach is likely insufficient and may result in impeding the E Deinhard Lane and S Samson Trail intersection.
- As described previously in this report, the student drop-off and pick-up staging at the Middle School can back up around the traffic circle, through the parking lot and down the hill towards the Middle School. As proposed by the developer/City of McCall, the extension of E Deinhard Lane would eliminate much of this student drop-off and pick-up queue area which may result in queueing into the new public roadway.

We recognize that as land to the east of the school campus develops, more direct access to those properties is needed, rather than using the public roadways to navigate to the campus access locations along S Samson Trail. Therefore, we recommend that a public roadway be extend from the development area south and east of the school property up to the school property boundary and either a turn around (cul-de-sac) or parking area be provided directly adjacent to the school property. Then a bicycle and pedestrian connection from the end of the public roadway can be provided onto the school campus as described in Section 4.3, Future Bicycle & Pedestrian Access, and as shown on Figure 2. This solution also stages the area to extend the roadway to the E Deinhard Lane and S Samson Trail intersection if the developer and/or City of McCall are able to adequately address and fund all the potential impacts identified above and any other concerns the MDSO may have related to the new roadway and impacts to on-campus facilities.

It should also be noted that any connection of future roadways east of the campus property to the existing internal campus roadway system may induce cut-through traffic on the school campus and is therefore not recommended.

4.3 Future Bicycle & Pedestrian Access

Figure 2, as referenced in Section 3.2, Existing Roadway, Bicycle & Pedestrian facilities, shows the existing bicycle and/or pedestrian facilities on the school campus and adjacent roadways. Additionally, Figure 2 shows two phases of future bicycle and/or pedestrian facilities.

2022 Bicycle & Pedestrian Construction Plans

The bicycle and/or pedestrian facilities planned for construction with the Payette Lakes Middle School remodel/expansion are shown in cyan on Figure 2 and include the following:

- An 8-foot pathway connection from the public pathway system at E Deinhard Lane and S Samson Trail to the new pedestrian and bicycle facilities being constructed at the Middle School and extending along the southern access road to connect to the existing Fox Ridge Subdivision pathway and the existing pathway to the Elementary School
- New 10-foot sidewalk along the Middle School parking lot to accommodate additional student drop-off and pick-up area
- New 8-foot sidewalk around the Middle School south and east wings
- Three sidewalk/pathway connections from the Middle School to the new pathway along the campus southern access road

Potential Future Bicycle & Pedestrian Facilities

The bicycle and/or pedestrian facilities identified as possible future facilities are shown in red on Figure 2 and include the following:

- A new pathway connection to the currently undeveloped land south and east of the campus property, connecting to a future cul-de-sac or parking lot as described in Section 4.2, Future Site Access
- A new pathway connection between the currently undeveloped land south and east of the campus property to the existing pathway that goes between the school campus and the Fox Ridge Subdivision

These potential future bicycle and pedestrian facilities should be constructed when the land to the south and east of the school campus is developed.

4.4 Traffic Projections & Analysis

In order to analyze the traffic associated with the Payette Lakes Middle School expansion, the existing traffic counts are projected to reflect the campus student capacity following the completion of the Middle School remodel/expansion. Additionally, traffic not directly associated with the school campus is also projected to account for future traffic growth in the area.

Traffic Projections

As described in Section 3, the “existing” traffic analysis for the Payette Lakes Middle School and Barbara Morgan Elementary School campus included existing local traffic and school campus traffic projected to account for the current campus student enrollment capacity of 850 students. The Middle School remodel/expansion will add five class rooms to the school, therefore increasing the Middle School student enrolment capacity by 125 students, based on 25 students per classroom.

The traffic information shown in Figure 4 (based on a campus student capacity of 850 students) is projected to included traffic associated with a campus student capacity of 975 student (450 Elementary School students and 525 Middle School Students).

In addition to increasing campus related traffic, the existing local traffic is also projected to account for area traffic growth. For this analysis, the local traffic was increase by 25% over the existing traffic. This

traffic increase would represent an approximate 2.25 % annual traffic growth over the next ten years. For reference, the total AM peak hour intersection count for the E Deinhard Lane and S Samson Trail intersection went from 724 vehicles counted in April of 2019 to 743 vehicles counted in September of 2021. This represents a 1.25% annual traffic growth over the past two years.

Figure 6 shows the future traffic projections for a campus student capacity of 975 students and the local traffic increased by 25%. The distribution of traffic shown on Figure 6 assumes the current campus access and internal circulation remains unchanged.

Future Traffic Redistribution

As discussed in Section 4.2, Future Site Access, the MDSD should plan on implementing campus approach modifications when the campus student enrollment reaches 850 students, or sometime before that threshold. These approach modifications include the following from the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis:

- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot
- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road

To properly analyze these approach modifications, the traffic shown in Figure 6 is redistributed to reflect the Elementary School traffic entering at the campus south approach (E Deinhard Lane & S Samson Trail) and exiting the campus at the campus middle approach, and the Middle School traffic entering and exiting at the campus north approach.

Figure 7 shows the redistribution of the future traffic.

Figure 6

Future Traffic Projection
 2021 Traffic Count Projected To Future Campus Capacity
 (450 Elementary Students And 525 Middle School Students) Plus 25% Growth Of Local Traffic

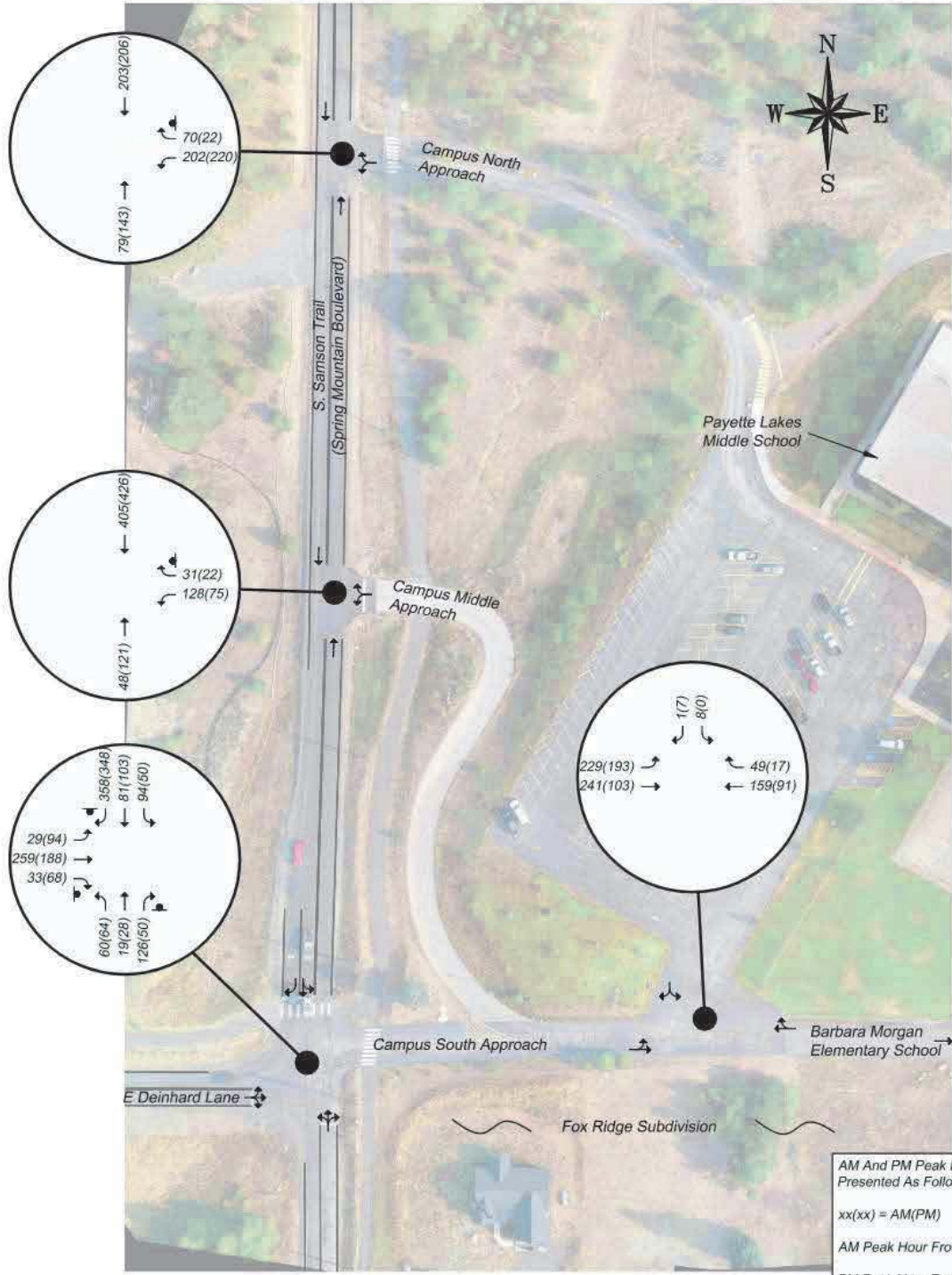
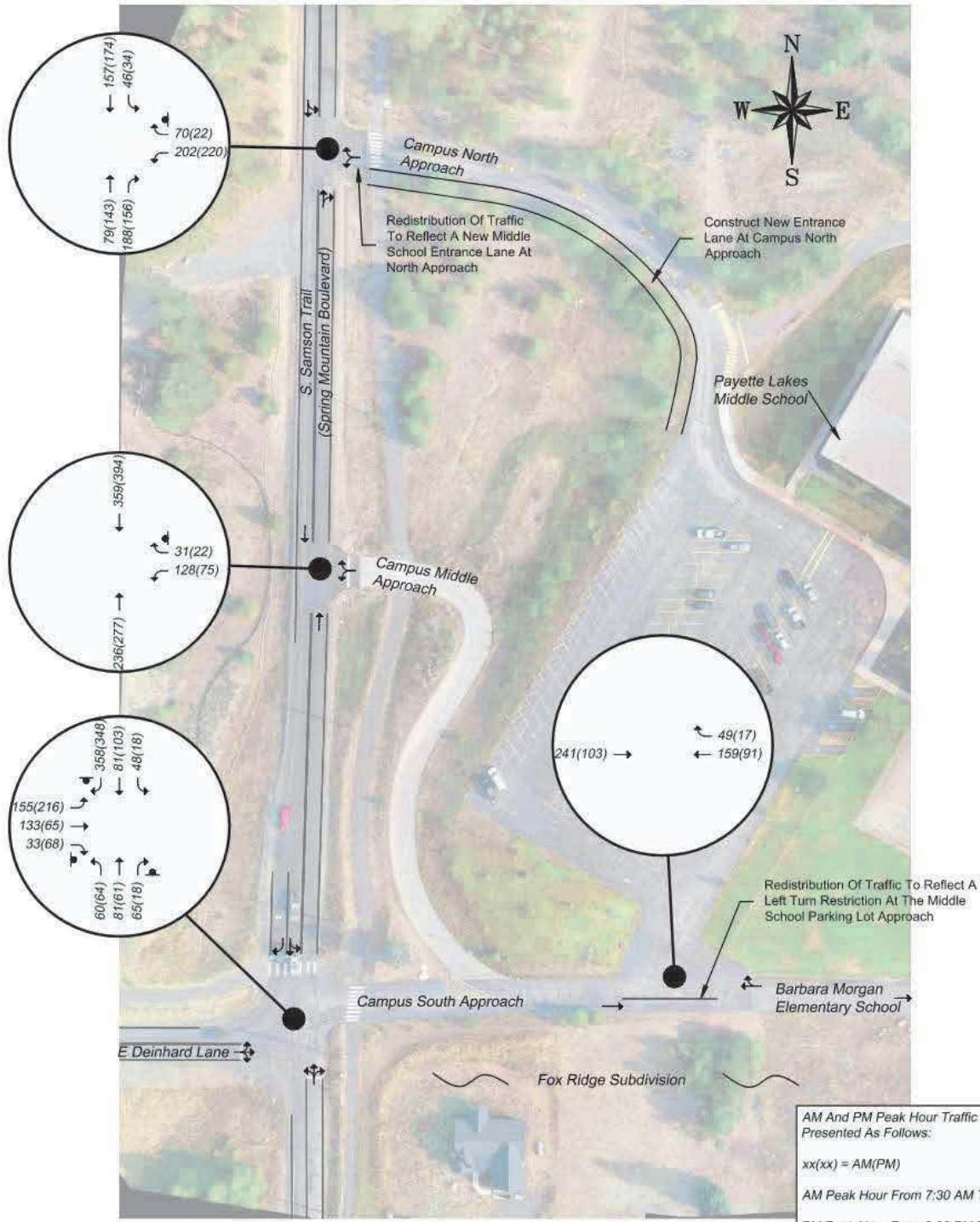


Figure 7

Future Traffic Projection

2021 Traffic Count Projected To Current Campus Capacity
 (450 Elementary Students And 525 Middle School Students) Plus 25% Growth Of Local Traffic
 Redistribution Associated With A New Middle School Entrance At The Campus North Approach



Analysis of Future Traffic Redistribution

Considering this report recommends modifying the campus circulation and approach configuration once the campus student capacity reaches 850 students, all future traffic analysis assume these modifications have been implemented. Therefore, the redistributed traffic volumes, as shown in Figure 7, were analyzed with the suggested changes to the campus access locations, including modifying the campus north access to include a campus entry lane for the Middle School. The results of these analysis are shown in Table 3. Appendix D includes copies of the analysis worksheets.

a table to show the future undistributed would be helpful to show if the proposed internal improvements help with the future more than they do with the current counts.

Table 3 - Intersection Analysis for Future School Capacities Redistributed for a Campus North Approach Entry Lane					
Intersection	Lane Group	School AM Peak Hour		School PM Peak Hour	
		LOS	Delay (sec.)	LOS	Delay (sec.)
E Deinhard Lane & S Samson Trail (Campus South Approach)	NB-LTR	C	23.4	B	12.7
	SB-LT	C	16.4	B	11.7
	SB-R	F	95.6	C	21.4
	EB-LTR	F	63.2	C	24.5
S Samson Trail & Campus Middle Approach	WB-LR	E	46.6	D	30.6
S Samson Trail & Campus North Approach	WB-LR	F	54.9	E	37.6
	SB-LT	A	8.4	A	8.3

As shown in Table 3, all intersections have lane groups or approaches that operate at unacceptable levels. Therefore, once the school campus enrollment exceeds 850 students, the MDSD and the City of McCall should consider partnering to complete mitigation improvements as discussed below.

Future Traffic Mitigation

Although multiple scenarios of future improvements may mitigate the future traffic delays along the school campus frontage, analysis of the following improvements show satisfactory results (see Figure 8 for a conceptual layout of the described improvements):

- E Deinhard Lane and S Samson Trail intersection
 - Convert the existing southbound to westbound right turn lane to a free running right which operates outside the stop sign requirements on the southbound approach. Merge the free running right receiving lane on E Deinhard Lane with the existing travel lane west of the intersection.
- S Samson Trail
 - Widen S Samson Trail to extend three lanes from the E Deinhard Lane and S Samson Trail intersection to north of the school campus property.

This creates a large off-set and a questionable ped and bike crossing.

Figure 8

Future Mitigation
Intersection Control & Lane Configuration Along S Samson Trail

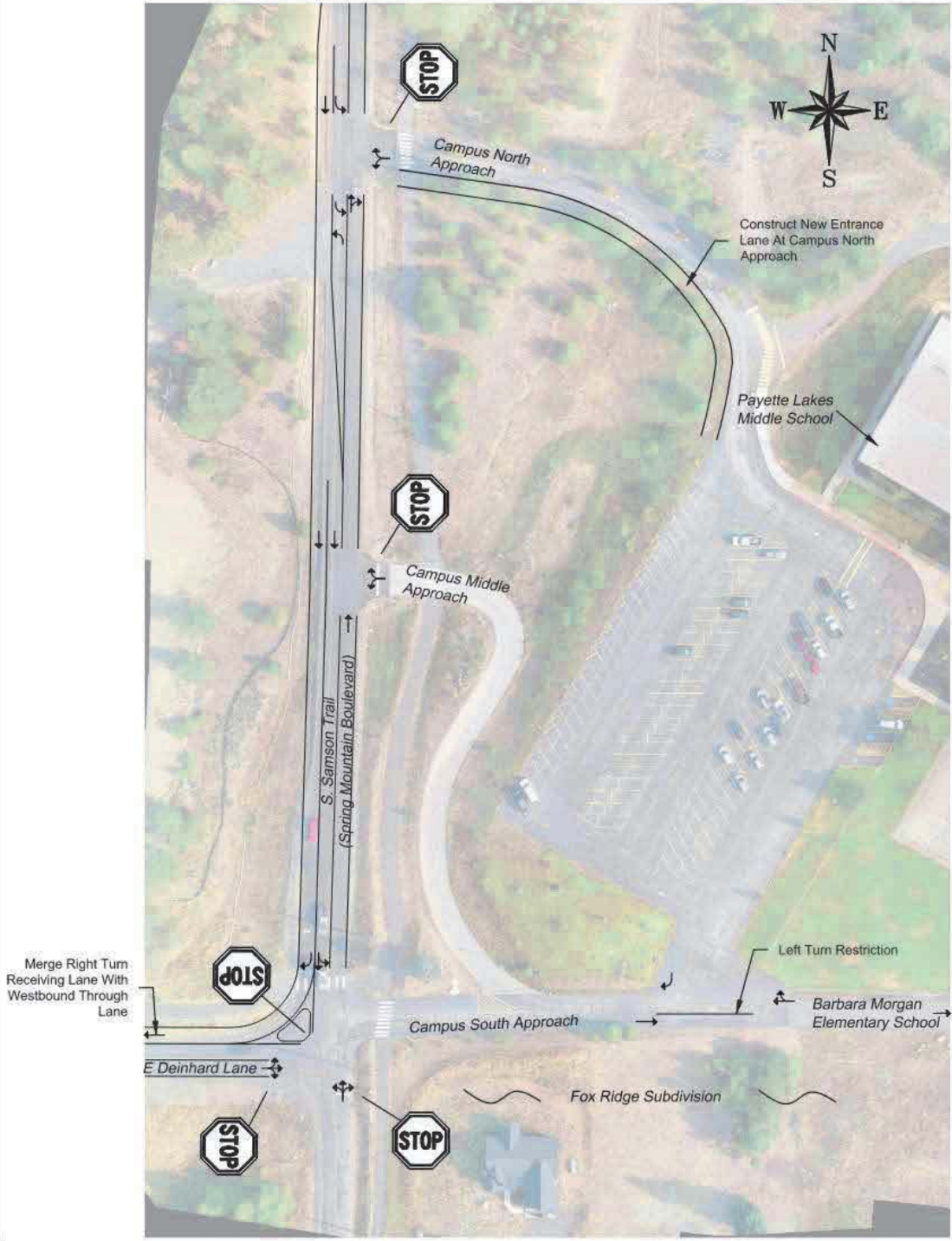


Table 4 shows the analysis results for the future traffic shown in Figure 7 applied to the intersections with mitigation improvements. Appendix E includes copies of the analysis worksheets.

Table 4 - Intersection Analysis for Future School Capacities Redistributed for a Campus North Approach Entry Lane with Mitigation Improvements					
Intersection	Lane Group	School AM Peak Hour		School PM Peak Hour	
		LOS	Delay (sec.)	LOS	Delay (sec.)
E Deinhard Lane & S Samson Trail (Campus South Approach)	NB-LTR	C	16.5	B	13.2
	SB-LT	B	13.3	B	12.4
	EB-LTR	D	32.0	D	32.3
S Samson Trail & Campus Middle Approach	WB-LR	D	25.3	C	21.0
S Samson Trail & Campus North Approach	WB-LR	C	23.7	C	23.4
	SB-LT	A	8.4	A	8.4

As shown in Table 4, all intersections operate at acceptable levels under the future traffic redistribution to accommodate an entry lane at the campus north approach, with mitigation improvements along the public roadways. does this analysis include a delay associated with offset intersections?

5.0 *Special Conditions for School Facilities*

Idaho Code 67-6519(3) identifies specific items to be addressed for school facilities. This Section 5 responds to each of the items identified in Idaho code, as appropriate to the project at the Payette Lakes Middle School and Barbara Morgan Elementary School campus.

5.1 Land Use Master Plan

The MDSD parcel is located in the City of McCall, Valley County, Idaho. The parcel is currently zoned Civic and the City of McCall Future Land Use Plan map indicates no change in zoning is planned. The Civic land use incorporates schools (public or private), including vocational, as allowed land uses under a conditional use permit.

5.2 School Bus Plan

The subject parcel is currently developed with an elementary and middle school which have active busing for students. The current project imposes no changes to the on-campus bus circulation or the off-campus busing plan. Future modifications to the school access and circulation may also modify the on-campus bus circulation.

5.3 Access Safety

through 1 intersection

Current access to the school campus is via three public roadway approaches along S Samson Trail (Spring Mountain Boulevard). The Local Highway Technical Assistance Council on-line accident mapping was reviewed for potential high accident locations and safety concerns. Within the five years reported on the map, only one, single car, accident was reported in 2016, at the intersection of E Deinhard Lane and S Samson Trail. Therefore, no high accident locations are identified near the school campus and the existing campus access points appear to be functioning safely.

5.4 Pedestrian Plan

See Figure 2 and Sections 3.2 and 4.3 of this report for information relative to the campus pedestrian and bicycle access.

5.5 Crossing Guard Plan

The majority of the existing campus students are either bused to the campus or are dropped off by parents. There are marked crosswalks at the intersection of E Deinhard Lane and S Samson Boulevard. Crossing guards are not currently used at these crosswalks and implementation of crossing guard use is not planned based on the expansion plans at the Middle School.

5.6 Barriers Between the Roadways and the School

The existing Middle School and Elementary School are set back significantly from the public roadway and the campus is elevated with respect to the public roadway. Therefore, no physical barrier is necessary between the public roadway and the schools.

5.7 Location of the School Zone

20 MPH school zone signing is already established along E Deinhard Lane and S Samson Trail for the school campus. No changes to the established school zones are planned with this project.

5.8 Need for a Flashing Beacon

No flashing beacons are currently installed at the roadway crosswalks. Considering the majority of the existing campus students are either bused to the campus or are dropped off by parents, and the proposed Middle School expansion is not expected to change the percentage of students walking or biking to school, no flashing beacons are planned with the project.

5.9 Need for Traffic Control

As discussed within this report, all campus access points are anticipated to function at acceptable levels up to a campus student enrollment of 850 students and will continue to operate at acceptable levels, beyond 850 students, with some mitigation improvements along the campus frontage. No changes to the existing traffic control (i.e. signal installation) is necessary under the mitigation improvements identified within this report.

5.10 Anticipated Future Improvements

See previous sections of this report for information relative to future on-campus and off-campus improvements.

5.11 Speed on Adjacent Roadways

The current posted speed limit on the nearby roadways (E Deinhard Lane and S Samson Trail) is 25 MPH. No change to the current posted speed limit is necessitated by this project.

5.12 Traffic Volumes on Adjacent Roadways

See previous sections of this report for information relative to current traffic volumes and future projected traffic volumes.

5.13 Effect on a Roadway's Level of Service

See previous sections of this report for information relative to current Levels of Service and future projected Levels of Service for the campus access points and adjacent public roadway intersection.

5.14 Need for Acceleration or Deceleration Lanes

Traffic at the school campus approaches and on the public roadway do not warrant acceleration or deceleration lanes.

5.15 Internal Traffic Circulation

See previous sections of this report for information relative to on-campus traffic circulation.

5.16 Anticipated Development on Surrounding Undeveloped Parcels

The existing campus has undeveloped land to the **south and east** of the MDS property. See previous sections of this report for information relative to future bicycle and pedestrian facility connections to the undeveloped land. Additionally, previous sections of this report recommend a public roadway be extended to the school property boundary with a cul-de-sac or parking lot to provide access to the school campus via the on-campus bicycle and pedestrian facilities.

5.17 Zoning in the Vicinity

The MDS school property is located with the City of McCall. Current zoning in the vicinity of the school includes Low Density Residential, Residential (1 Acre), Rural Residential (10 Acres), and Community Commercial.

5.18 Access Control on Adjacent Roadways

The school campus has three existing access locations along S Samson Trail. No change to these existing access locations is proposed with the project. As discussed in previous sections of this report, a future change to the on-campus circulation may modify the function of the existing access location, but would not change the location of the existing access points.

5.19 Required Striping & Signing Modifications

No changes to the off-campus roadway pavement markings or signing are necessary as a result of this project. Signing adjustments on-campus will be necessary to accommodate site construction and will be included with the construction.

See previous sections of this report for information relative to future off-campus modifications along the campus frontage. This report recommends consideration of the off-campus improvements after the campus student enrollment reaches 850 students.

5.20 Funding for Roadway Improvements to Accommodate the Development

The MDSO has approved school bond funding to complete the Middle School remodel/expansion along with the associated site construction elements identified within this report. The MDSO and the City of McCall should collaborate on the future timing and funding of improvements to the public roadway improvements along the campus frontage.

5.21 Proposed Roadway Projects in the Project Vicinity

The City of McCall has identified one planned roadway improvement project in the vicinity of the campus property. The project will reconstruct E Deinhard Lane from State Highway 55 to S Samson Trail. No roadway widening is planned with the project. The E Deinhard Lane construction project is scheduled for the 2022 construction season. Construction traffic control from the roadway project may have detours that impact access to the school campus during construction of the Middle School remodel/expansion and associated site work.

6.0 *Conclusions & Recommendations*

The McCall-Donnelly School District (MDSO) Plans to remodel and expand the Payette Lakes Middle School to accommodate five additional classrooms. The Middle School expansion also includes site construction of the following significant features:

- Realign the on-campus intersection of the Barbara Morgan Elementary School access road and the Payette Lakes Middle School north parking lot access road to accommodate better traffic flow to the elementary school
- Regrade the playground area located between the south and east wings of the Payette Lakes Middle School to accommodate a more useful activity area for the students
- Relocate the handicap parking at the Payette Lakes Middle School and expand the available student pick-up and drop-off area
- Modify the existing campus entrance at the intersection of E Deinhard Lane and S Samson Trail (Spring Mountain Blvd) to accommodate pedestrian and bicycle access from the public pathway system to the internal middle school and elementary school pathways

All intersections and school campus approaches to the public roadway system (E Deinhard Ln and S Samson Trail intersection, school campus middle exit and school campus north exit) operate at acceptable levels under the current traffic and school enrollment conditions. On campus traffic circulation and on campus vehicle queueing currently has minimal to no impact on the public roadway system and intersections.

All intersections and school campus approaches to the public roadway system will continue to operate at acceptable levels up to the current school student capacities. Therefore, no campus access improvements are necessary until current enrollment grows by another 120 students, to an enrollment of 850 students on campus. Furthermore, the campus access and circulation plan modifications identified in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report will also operate at acceptable levels up to a student enrollment of 850 students on the campus.

Growing beyond the enrollment of 850 students on the campus will necessitate improvements to the campus access and within the public roadways and intersections. The following improvements will operate at acceptable levels through the Payette Lakes Middle School remodel capacity (adding 125 students to the current campus capacity of 850 students) plus 25% growth of the local traffic:

- Widening the campus north approach to S Samson Trail to accommodate two-way traffic into and out of the Middle School parking lot

- Constructing a physical barrier to restrict left turn movements at the Middle School parking lot approach to the Elementary School access road
- E Deinhard Lane and S Samson Trail intersection
 - Convert the existing southbound to westbound right turn lane to a free running right which operates outside the stop sign requirements on the southbound approach. Merge the free running right receiving lane on E Deinhard Lane with the existing travel lane west of the intersection.
- S Samson Trail
 - Widen S Samson Trail to extend three lanes from the E Deinhard Lane and S Samson Trail intersection to north of the school campus property.

As available ground to the south and east of the school property develops, the development should extend a public roadway to the school property boundary and provide a parking area or turnaround. Bicycle and pedestrian access to/from the school property to the parking area or turnaround should be constructed.

Appendix A
September 22, 2021 Traffic Count Data

Agency: **McCall-Donnelly School District**
 Location: **Payette Lakes Middle School & Barbara Morgan Elementary School Campus**
 Intersection: **S Sampson Trail & North Campus Approach**
 Count Date: **Wednesday, 9/22/2021**

TRAFFIC COUNT DATA

September 22, 2021 AM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						North Campus Approach						15 min Intersection Total	1 hr Intersection Total			
	North Bound			South Bound			East Bound			West Bound							
	left	thru	right	left	thru	right	left	thru	right	left	thru	right					
7:15 - 7:30		9			15							3		0	27	-	
7:30 - 7:45		10			43							15		3	71	-	
7:45 - 8:00		25			46							48		22	141	-	
8:00 - 8:15		21			42							42		13	118	357	
8:15 - 8:30		8			20							7		1	36	366	
8:30 - 8:45															0	295	
8:45 - 9:00															0	154	
9:00 - 9:15															0	36	
PH Totals	0	64	0	0	151	0	0	0	0	0	0	112	0	39			
															Peak Hour Total		366
															Peak Hour		7:30-8:30 am
															Peak Hour Factor		0.65

September 22, 2021 PM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						North Campus Approach						15 min Intersection Total	1 hr Intersection Total			
	North Bound			South Bound			East Bound			West Bound							
	left	thru	right	left	thru	right	left	thru	right	left	thru	right					
2:15 - 2:30		21			33							3		1	58	-	
2:30 - 2:45		22			47							2		0	71	-	
2:45 - 3:00		20			45							1		1	67	-	
3:00 - 3:15		41			29							48		4	122	318	
3:15 - 3:30		31			30							29		3	93	353	
3:30 - 3:35															0	282	
3:35 - 4:00															0	215	
4:00 - 4:15															0	93	
PH Totals	0	114	0	0	151	0	0	0	0	0	0	80	0	8			
															Peak Hour Total		353
															Peak Hour		2:30-3:30 pm
															Peak Hour Factor		0.72

Agency: **McCall-Donnelly School District**
 Location: **Payette Lakes Middle School & Barbara Morgan Elementary School Campus**
 Intersection: **S Sampson Trail & Middle Campus Approach**
 Count Date: **Wednesday, 9/22/2021**

TRAFFIC COUNT DATA

September 22, 2021 AM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						Middle Campus Approach						15 min Intersection Total	1 hr Intersection Total			
	North Bound			South Bound			East Bound			West Bound							
	left	thru	right	left	thru	right	left	thru	right	left	thru	right					
7:15 - 7:30		9			18							0		0	27	-	
7:30 - 7:45		10			50							2		0	62	-	
7:45 - 8:00		15			106							38		13	172	-	
8:00 - 8:15		5			82							58		13	158	419	
8:15 - 8:30		8			25							8		0	41	433	
8:30 - 8:45															0	371	
8:45 - 9:00															0	199	
9:00 - 9:15															0	41	
PH Totals	0	38	0	0	263	0	0	0	0	106	0	26					
															Peak Hour Total		433
															Peak Hour		7:30-8:30 am
															Peak Hour Factor		0.63

September 22, 2021 PM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						Middle Campus Approach						15 min Intersection Total	1 hr Intersection Total			
	North Bound			South Bound			East Bound			West Bound							
	left	thru	right	left	thru	right	left	thru	right	left	thru	right					
2:15 - 2:30		19			36							4		3	62	-	
2:30 - 2:45		23			49							2		0	74	-	
2:45 - 3:00		20			47							2		0	69	-	
3:00 - 3:15		23			78							41		16	158	363	
3:15 - 3:30		31			57							13		1	102	403	
3:30 - 3:35															0	329	
3:35 - 4:00															0	260	
4:00 - 4:15															0	102	
PH Totals	0	97	0	0	231	0	0	0	0	58	0	17					
															Peak Hour Total		403
															Peak Hour		2:30-3:30 pm
															Peak Hour Factor		0.64

Agency: **McCall-Donnelly School District**
 Location: **Payette Lakes Middle School & Barbara Morgan Elementary School Campus**
 Intersection: **S Sampson Trail & Deinhard Lane (South Campus Approach)**
 Count Date: **Wednesday, 9/22/2021**

TRAFFIC COUNT DATA

September 22, 2021 AM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						Deinhard Lane						15 min Intersection Total	1 hr Intersection Total
	North Bound			South Bound			East Bound			West Bound				
	left	thru	right	left	thru	right	left	thru	right	left	thru	right		
7:15 - 7:30	5	4	6	5	2	11	5	13	2				53	-
7:30 - 7:45	17	4	13	12	6	36	5	36	6				135	-
7:45 - 8:00	9	5	49	33	19	86	10	89	10				310	-
8:00 - 8:15	12	4	24	18	24	99	2	44	4				231	729
8:15 - 8:30	10	2	0	1	7	28	6	7	6				67	743
8:30 - 8:45													0	608
8:45 - 9:00													0	298
9:00 - 9:15													0	67
PH Totals	48	15	86	64	56	249	23	176	26	0	0	0		
Peak Hour Total													743	
Peak Hour													7:30-8:30 am	
Peak Hour Factor													0.60	

September 22, 2021 PM TURNING MOVEMENT COUNT

Time Period	S. Samson Trail						Deinhard Lane						15 min Intersection Total	1 hr Intersection Total
	North Bound			South Bound			East Bound			West Bound				
	left	thru	right	left	thru	right	left	thru	right	left	thru	right		
2:15 - 2:30	14	4	4	2	10	28	15	11	6				94	-
2:30 - 2:45	14	6	1	7	4	41	17	29	6				125	-
2:45 - 3:00	13	2	15	10	11	27	17	29	13				137	-
3:00 - 3:15	13	6	7	6	32	74	17	33	12				200	556
3:15 - 3:30	11	8	3	3	13	61	24	6	23				152	614
3:30 - 3:35													0	489
3:35 - 4:00													0	352
4:00 - 4:15													0	152
PH Totals	51	22	26	26	60	203	75	97	54	0	0	0		
Peak Hour Total													614	
Peak Hour													2:30-3:30 pm	
Peak Hour Factor													0.77	

Agency: **McCall-Donnelly School District**
 Location: **Payette Lakes Middle School & Barbara Morgan Elementary School Campus**
 Intersection: **PLMS Parking Lot & BMES Access Road**
 Count Date: **Wednesday, 9/22/2021**

TRAFFIC COUNT DATA

September 22, 2021 AM TURNING MOVEMENT COUNT

Time Period	PLMS Parking Lot						BMES Access Road						15 min Intersection Total	1 hr Intersection Total
	North Bound			South Bound			East Bound			West Bound				
	left	thru	right	left	thru	right	left	thru	right	left	thru	right		
7:15 - 7:30				0		0	13	11			0	0	24	-
7:30 - 7:45				2		1	31	29			3	1	67	-
7:45 - 8:00				2		0	56	114			52	17	241	-
8:00 - 8:15				3		0	38	50			68	9	168	500
8:15 - 8:30				0		0	2	6			8	0	16	492
8:30 - 8:45													0	425
8:45 - 9:00													0	184
9:00 - 9:15													0	16
PH Totals	0	0	0	7	0	1	127	199	0	0	131	27		
													Peak Hour Total	492
(Based on all school intersections)													Peak Hour	7:30-8:30 am
													Peak Hour Factor	0.51

September 22, 2021 PM TURNING MOVEMENT COUNT

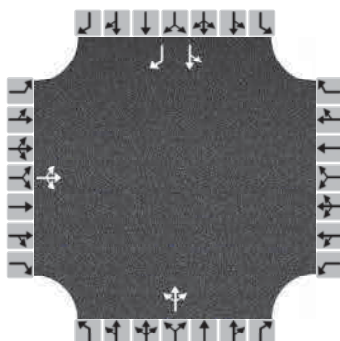
Time Period	PLMS Parking Lot						BMES Access Road						15 min Intersection Total	1 hr Intersection Total
	North Bound			South Bound			East Bound			West Bound				
	left	thru	right	left	thru	right	left	thru	right	left	thru	right		
2:15 - 2:30				0		1	3	14			4	0	22	-
2:30 - 2:45				0		0	5	31			2	1	39	-
2:45 - 3:00				0		0	28	25			2	0	55	-
3:00 - 3:15				0		3	31	17			58	2	111	227
3:15 - 3:30				0		2	6	6			8	3	25	230
3:30 - 3:35													0	191
3:35 - 4:00													0	136
4:00 - 4:15													0	25
PH Totals	0	0	0	0	0	5	70	79	0	0	70	6		
													Peak Hour Total	230
													Peak Hour	2:30-3:30 pm
													Peak Hour Factor	0.52

Appendix B
**Traffic Analysis Worksheets (Current Campus
Capacity and Existing Lane Configuration and
Intersection Control)**

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/11/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.60
Time Analyzed	AM Peak Hour		
Project Description	2021 School Capacity Analysis		

Lanes



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	23	225	26				48	15	110	82	68	300
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	457						288			250	500	
Percent Heavy Vehicles	5						5			5	5	

Departure Headway and Service Time

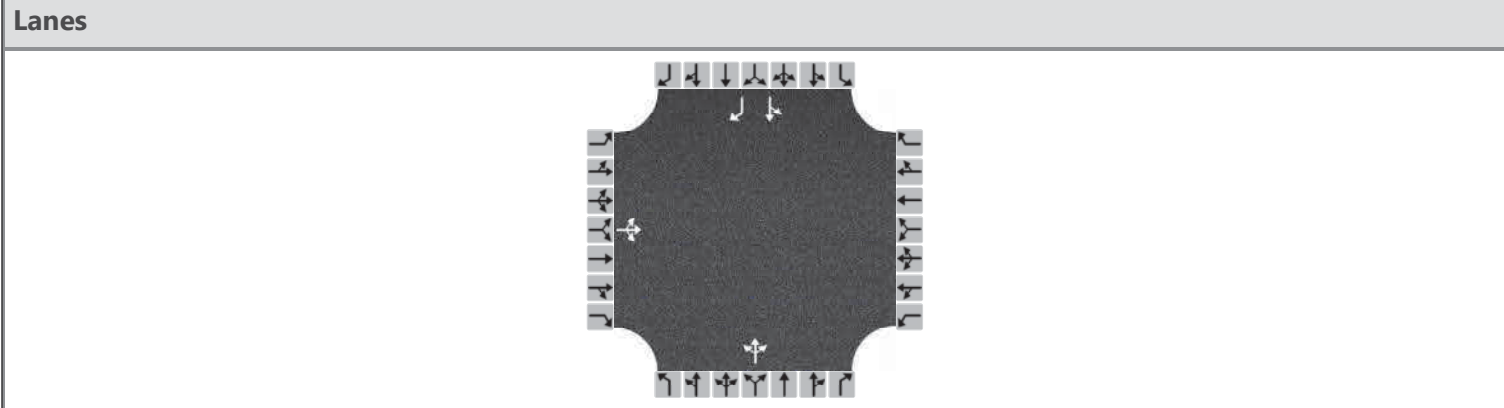
Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.406						0.256			0.222	0.444	
Final Departure Headway, hd (s)	6.34						6.41			7.01	6.02	
Final Degree of Utilization, x	0.804						0.514			0.487	0.835	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	4.34						4.41			4.71	3.72	

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	457						288			250	500	
Capacity	568						561			514	598	
95% Queue Length, Q ₉₅ (veh)	7.9						2.9			2.6	8.9	
Control Delay (s/veh)	30.4						16.0			16.2	32.0	
Level of Service, LOS	D						C			C	D	
Approach Delay (s/veh)	30.4						16.0			26.7		
Approach LOS	D						C			D		
Intersection Delay, s/veh LOS	25.8						D					

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/11/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.77
Time Analyzed	PM Peak Hour		
Project Description	2021 School Capacity Analysis		



Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	75	160	54				51	22	43	43	84	283
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	375						151			165	368	
Percent Heavy Vehicles	5						5			5	5	

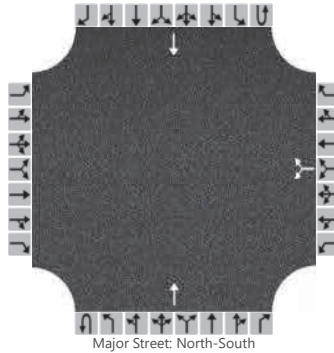
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.334						0.134			0.147	0.327	
Final Departure Headway, hd (s)	5.53						5.80			6.12	5.24	
Final Degree of Utilization, x	0.576						0.243			0.280	0.535	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	3.53						3.80			3.82	2.94	

Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	375						151			165	368	
Capacity	652						620			588	687	
95% Queue Length, Q ₉₅ (veh)	3.7						0.9			1.1	3.2	
Control Delay (s/veh)	15.8						10.7			11.2	13.8	
Level of Service, LOS	C						B			B	B	
Approach Delay (s/veh)	15.8						10.7			13.0		
Approach LOS	C						B			B		
Intersection Delay, s/veh LOS	13.6						B					

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB	Intersection	Samson & Middle Approach				
Agency/Co.	MDSO	Jurisdiction	City of McCall				
Date Performed	11/11/2021	East/West Street	Campus Middle Approach				
Analysis Year	2021	North/South Street	S Samson Trail				
Time Analyzed	AM Peak Hour	Peak Hour Factor	0.63				
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25				
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						128		31			38				322	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)					0											
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

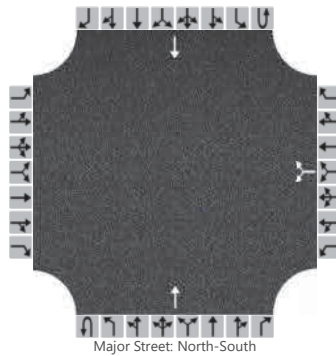
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						252										
Capacity, c (veh/h)						531										
v/c Ratio						0.48										
95% Queue Length, Q ₉₅ (veh)						2.5										
Control Delay (s/veh)						17.8										
Level of Service (LOS)						C										
Approach Delay (s/veh)					17.8											
Approach LOS					C											

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & Middle Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/11/2021			East/West Street	Campus Middle Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.64		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						75		22			97					335
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

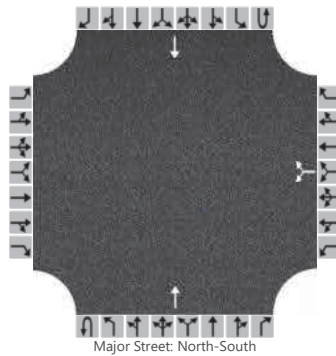
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						152										
Capacity, c (veh/h)						472										
v/c Ratio						0.32										
95% Queue Length, Q ₉₅ (veh)						1.4										
Control Delay (s/veh)						16.2										
Level of Service (LOS)						C										
Approach Delay (s/veh)						16.2										
Approach LOS						C										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/11/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.65		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						153		53			69				169	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

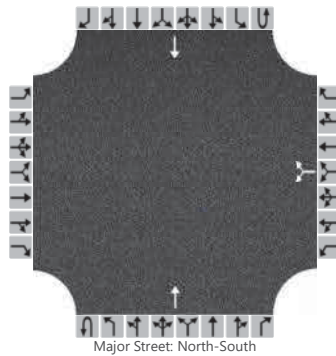
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						317										
Capacity, c (veh/h)						686										
v/c Ratio						0.46										
95% Queue Length, Q ₉₅ (veh)						2.4										
Control Delay (s/veh)						14.7										
Level of Service (LOS)						B										
Approach Delay (s/veh)						14.7										
Approach LOS						B										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/11/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.72		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						167		17			119				168	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

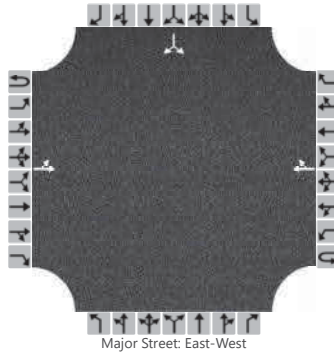
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						256										
Capacity, c (veh/h)						619										
v/c Ratio						0.41										
95% Queue Length, Q ₉₅ (veh)						2.0										
Control Delay (s/veh)						14.8										
Level of Service (LOS)						B										
Approach Delay (s/veh)						14.8										
Approach LOS						B										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	PLMS & BMES		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/11/2021			East/West Street	BMES		
Analysis Year	2021			North/South Street	PLMS		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.51		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		174	241				159	37						8		1
Percent Heavy Vehicles (%)		5												5		5
Proportion Time Blocked																
Percent Grade (%)														0		
Right Turn Channelized																
Median Type Storage					Undivided											

Critical and Follow-up Headways

Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.15												6.45		6.25
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.25												3.55		3.35

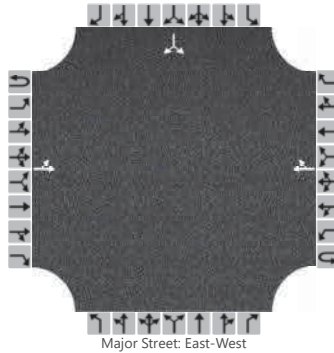
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)		341													18		
Capacity, c (veh/h)		1158													88		
v/c Ratio		0.29													0.20		
95% Queue Length, Q ₉₅ (veh)		1.2													0.7		
Control Delay (s/veh)		9.4													56.1		
Level of Service (LOS)		A													F		
Approach Delay (s/veh)		6.1												56.1			
Approach LOS														F			

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	PLMS & BMES		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/11/2021			East/West Street	BMES		
Analysis Year	2021			North/South Street	PLMS		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.52		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	2021 School Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		146	103				91	13						0		7
Percent Heavy Vehicles (%)		5												5		5
Proportion Time Blocked																
Percent Grade (%)														0		
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.15												6.45		6.25
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.25												3.55		3.35

Delay, Queue Length, and Level of Service

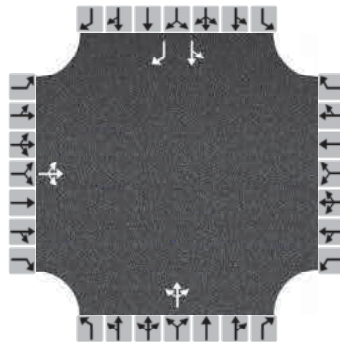
Flow Rate, v (veh/h)		281													13		
Capacity, c (veh/h)		1354													847		
v/c Ratio		0.21													0.02		
95% Queue Length, Q ₉₅ (veh)		0.8													0.0		
Control Delay (s/veh)		8.4													9.3		
Level of Service (LOS)		A													A		
Approach Delay (s/veh)		5.7												9.3			
Approach LOS														A			

Appendix C
Traffic Analysis Worksheets (Current Campus
Capacity Redistributed to Reflect a Campus North
Approach Entry Lane to the Middle School)

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.60
Time Analyzed	AM Peak Hour		
Project Description	2021 Redistribution Capacity Analysis		

Lanes



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	118	131	26				48	61	64	48	68	300
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	458						288			193	500	
Percent Heavy Vehicles	5						5			5	5	

Departure Headway and Service Time

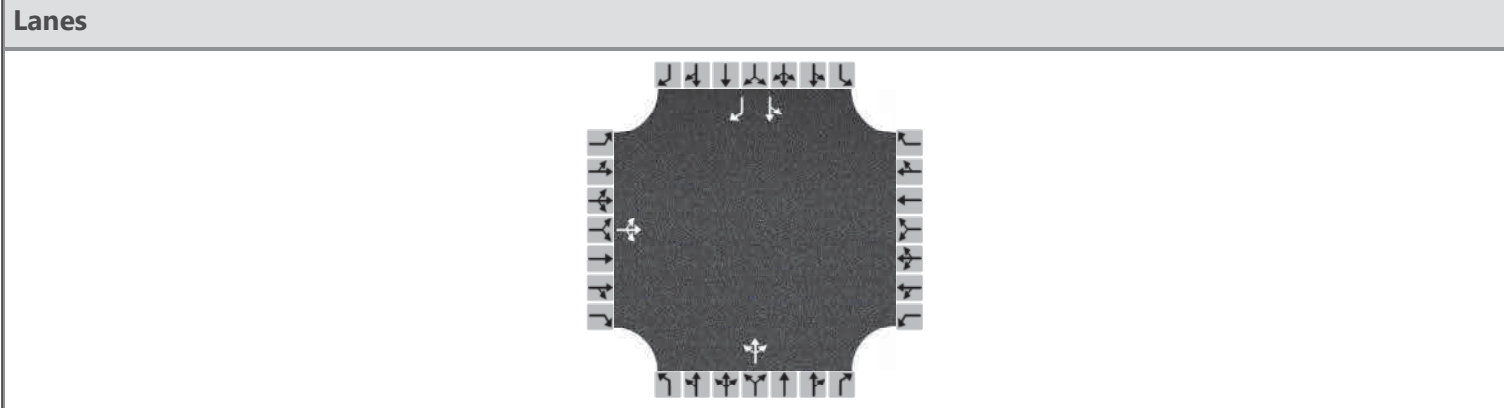
Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.407						0.256			0.172	0.444	
Final Departure Headway, hd (s)	6.40						6.58			6.98	6.05	
Final Degree of Utilization, x	0.815						0.527			0.375	0.841	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	4.40						4.58			4.68	3.75	

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	458						288			193	500	
Capacity	562						547			516	595	
95% Queue Length, Q ₉₅ (veh)	8.1						3.1			1.7	9.0	
Control Delay (s/veh)	31.7						16.7			13.8	32.7	
Level of Service, LOS	D						C			B	D	
Approach Delay (s/veh)	31.7						16.7			27.4		
Approach LOS	D						C			D		
Intersection Delay, s/veh LOS	26.6						D					

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.77
Time Analyzed	PM Peak Hour		
Project Description	2021 Redistribution Capacity Analysis		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	169	66	54				51	48	18	18	84	283
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	375						152			132	368	
Percent Heavy Vehicles	5						5			5	5	

Departure Headway and Service Time

Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.334						0.135			0.118	0.327	
Final Departure Headway, hd (s)	5.56						5.92			6.05	5.25	
Final Degree of Utilization, x	0.579						0.250			0.223	0.536	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	3.56						3.92			3.75	2.95	

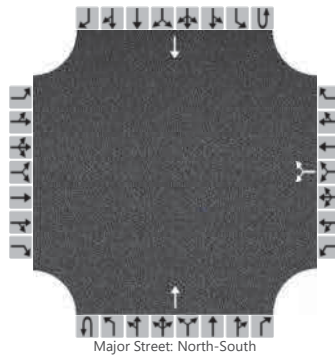
Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	375						152			132	368	
Capacity	648						609			595	685	
95% Queue Length, Q ₉₅ (veh)	3.7						1.0			0.8	3.2	
Control Delay (s/veh)	15.9						10.9			10.5	13.9	
Level of Service, LOS	C						B			B	B	
Approach Delay (s/veh)	15.9						10.9			13.0		
Approach LOS	C						B			B		
Intersection Delay, s/veh LOS	13.7						B					

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Middle Approach
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	Campus Middle Approach
Analysis Year	2021	North/South Street	S Samson Trail
Time Analyzed	AM Peak Hour	Peak Hour Factor	0.63
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	2021 Redistribution Capacity Analysis		

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						128		31			179				288	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

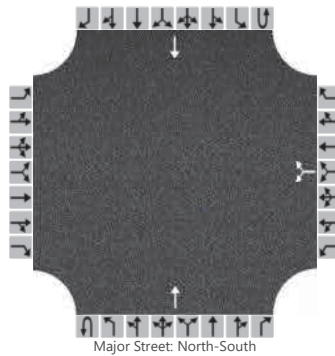
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						252										
Capacity, c (veh/h)						419										
v/c Ratio						0.60										
95% Queue Length, Q ₉₅ (veh)						3.8										
Control Delay (s/veh)						25.7										
Level of Service (LOS)						D										
Approach Delay (s/veh)						25.7										
Approach LOS						D										

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Middle Approach
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	Campus Middle Approach
Analysis Year	2021	North/South Street	S Samson Trail
Time Analyzed	PM Peak Hour	Peak Hour Factor	0.64
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	2021 Redistribution Capacity Analysis		

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes	0	0	0		0	1	0		0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						75		22			216					310
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

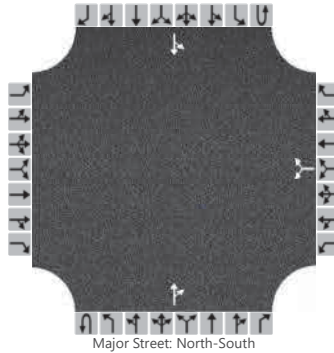
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						152										
Capacity, c (veh/h)						384										
v/c Ratio						0.39										
95% Queue Length, Q ₉₅ (veh)						1.8										
Control Delay (s/veh)						20.3										
Level of Service (LOS)						C										
Approach Delay (s/veh)						20.3										
Approach LOS						C										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.65		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR					TR		LT		
Volume (veh/h)						153		53			69	141		35	135	
Percent Heavy Vehicles (%)						5		5						3		
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

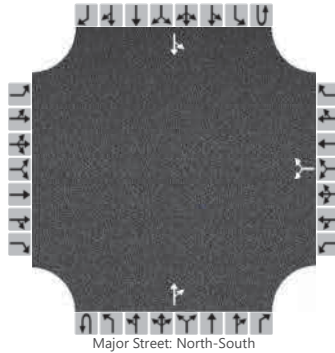
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						317								54		
Capacity, c (veh/h)						537								1231		
v/c Ratio						0.59								0.04		
95% Queue Length, Q ₉₅ (veh)						3.8								0.1		
Control Delay (s/veh)						20.9								8.1		
Level of Service (LOS)						C								A		
Approach Delay (s/veh)						20.9								2.0		
Approach LOS						C										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.72		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes	0	0	0		0	1	0		0	0	1	0	0	0	1	0
Configuration							LR					TR		LT		
Volume (veh/h)						167		17			119	119		26	143	
Percent Heavy Vehicles (%)						5		5						3		
Proportion Time Blocked																
Percent Grade (%)					0											
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

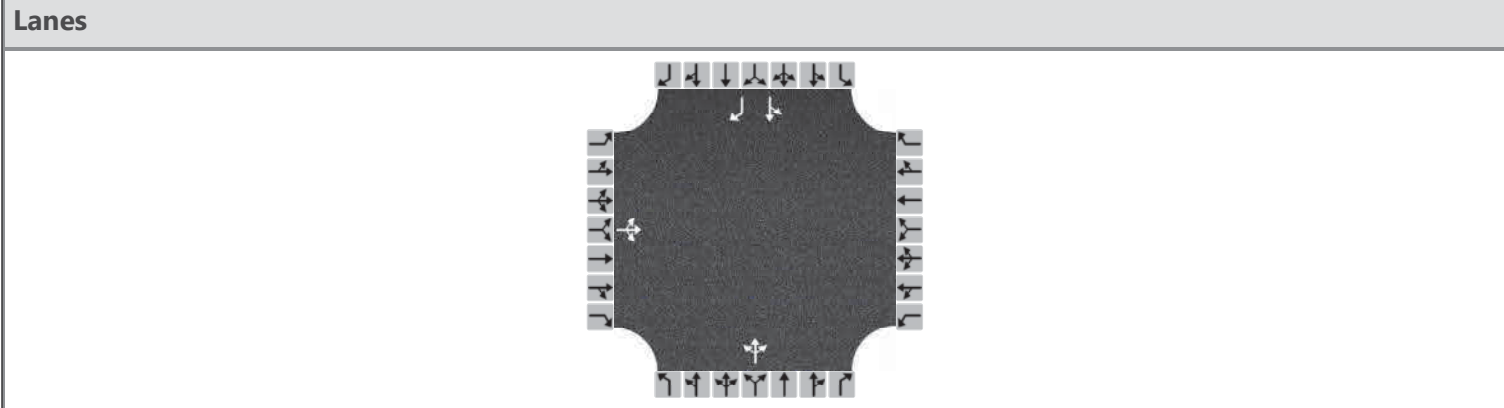
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						256									36	
Capacity, c (veh/h)						512									1223	
v/c Ratio						0.50									0.03	
95% Queue Length, Q ₉₅ (veh)						2.7									0.1	
Control Delay (s/veh)						18.8									8.0	
Level of Service (LOS)						C									A	
Approach Delay (s/veh)					18.8								1.5			
Approach LOS					C											

Appendix D
Traffic Analysis Worksheets (Future Campus
Capacity Redistributed to Reflect a Campus North
Approach Entry Lane to the Middle School)

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.60
Time Analyzed	AM Peak Hour		
Project Description	2021 Redistribution Future Capacity Analysis		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	155	133	33				60	81	65	48	81	358
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	535						343			215	597	
Percent Heavy Vehicles	5						5			5	5	

Departure Headway and Service Time

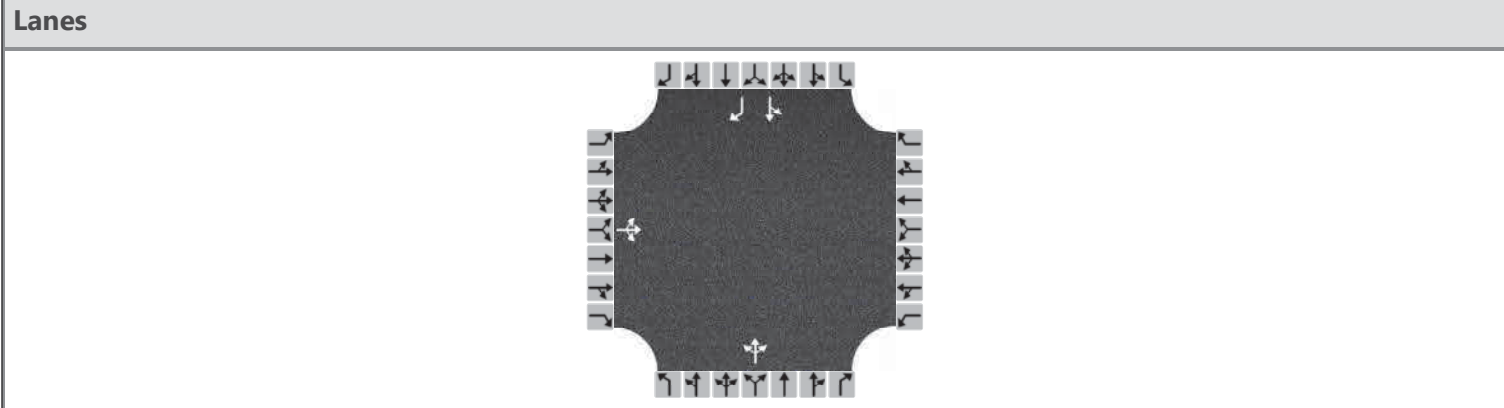
Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.476						0.305			0.191	0.530	
Final Departure Headway, hd (s)	6.69						7.06			7.59	6.67	
Final Degree of Utilization, x	0.994						0.673			0.453	1.106	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	4.69						5.06			5.29	4.37	

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	535						343			215	597	
Capacity	538						510			475	540	
95% Queue Length, Q ₉₅ (veh)	14.0						5.0			2.3	19.0	
Control Delay (s/veh)	63.2						23.4			16.4	95.6	
Level of Service, LOS	F						C			C	F	
Approach Delay (s/veh)	63.2						23.4			74.6		
Approach LOS	F						C			F		
Intersection Delay, s/veh LOS	60.6						F					

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.77
Time Analyzed	PM Peak Hour		
Project Description	2021 Redistribution Future Capacity Analysis		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	216	65	68				64	61	18	18	103	348
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT	R	
Flow Rate, v (veh/h)	453						186			157	452	
Percent Heavy Vehicles	5						5			5	5	

Departure Headway and Service Time

Initial Departure Headway, hd (s)	3.20						3.20			3.20	3.20	
Initial Degree of Utilization, x	0.403						0.165			0.140	0.402	
Final Departure Headway, hd (s)	5.94						6.50			6.49	5.70	
Final Degree of Utilization, x	0.748						0.335			0.283	0.716	
Move-Up Time, m (s)	2.0						2.0			2.3	2.3	
Service Time, ts (s)	3.94						4.50			4.19	3.40	

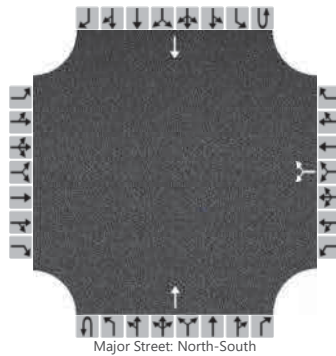
Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	453						186			157	452	
Capacity	606						554			555	632	
95% Queue Length, Q ₉₅ (veh)	6.6						1.5			1.2	6.0	
Control Delay (s/veh)	24.5						12.7			11.7	21.4	
Level of Service, LOS	C						B			B	C	
Approach Delay (s/veh)	24.5						12.7			18.9		
Approach LOS	C						B			C		
Intersection Delay, s/veh LOS	20.0						C					

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & Middle Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus Middle Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.63		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						128		31			236				359	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

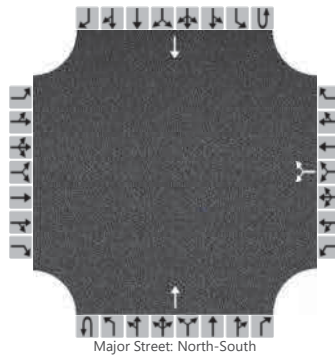
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						252										
Capacity, c (veh/h)						323										
v/c Ratio						0.78										
95% Queue Length, Q ₉₅ (veh)						6.3										
Control Delay (s/veh)						46.6										
Level of Service (LOS)						E										
Approach Delay (s/veh)						46.6										
Approach LOS						E										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & Middle Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus Middle Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.64		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR				T				T	
Volume (veh/h)						75		22			277				394	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2								
Critical Headway (sec)						6.45		6.25								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

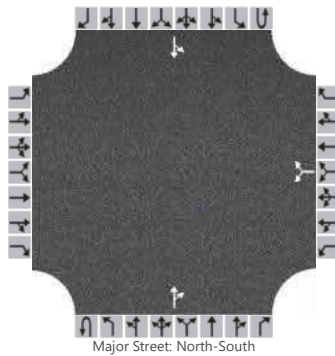
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						152										
Capacity, c (veh/h)						288										
v/c Ratio						0.53										
95% Queue Length, Q ₉₅ (veh)						2.9										
Control Delay (s/veh)						30.6										
Level of Service (LOS)						D										
Approach Delay (s/veh)						30.6										
Approach LOS						D										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.65		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR					TR		LT		
Volume (veh/h)						202		70			79	188		46	157	
Percent Heavy Vehicles (%)						5		5						3		
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

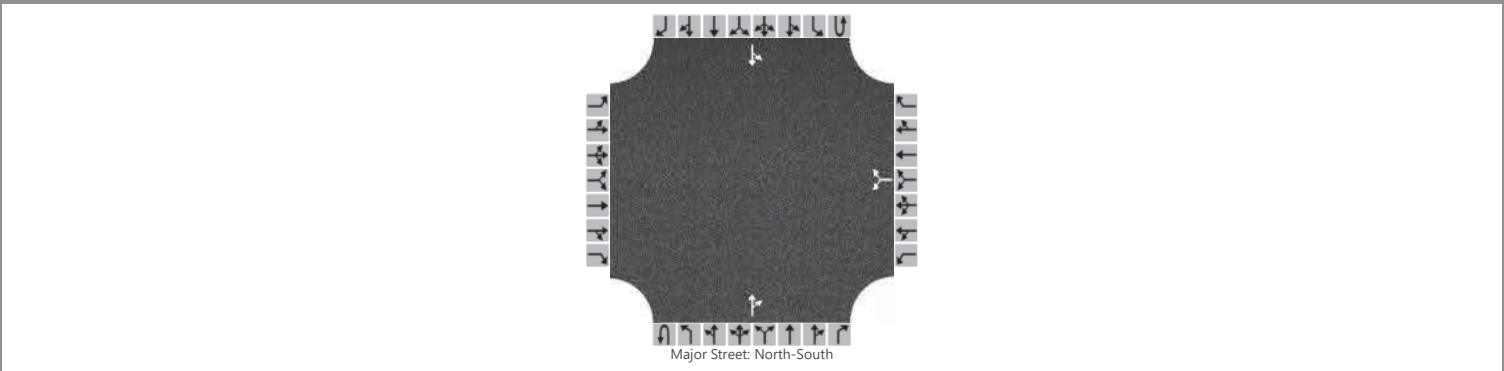
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						418									71	
Capacity, c (veh/h)						455									1143	
v/c Ratio						0.92									0.06	
95% Queue Length, Q ₉₅ (veh)						10.5									0.2	
Control Delay (s/veh)						54.9									8.4	
Level of Service (LOS)						F									A	
Approach Delay (s/veh)						54.9								2.4		
Approach LOS						F										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & N Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.72		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Analysis						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0
Configuration							LR					TR		LT		
Volume (veh/h)						220		22			143	156		34	174	
Percent Heavy Vehicles (%)						5		5						3		
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

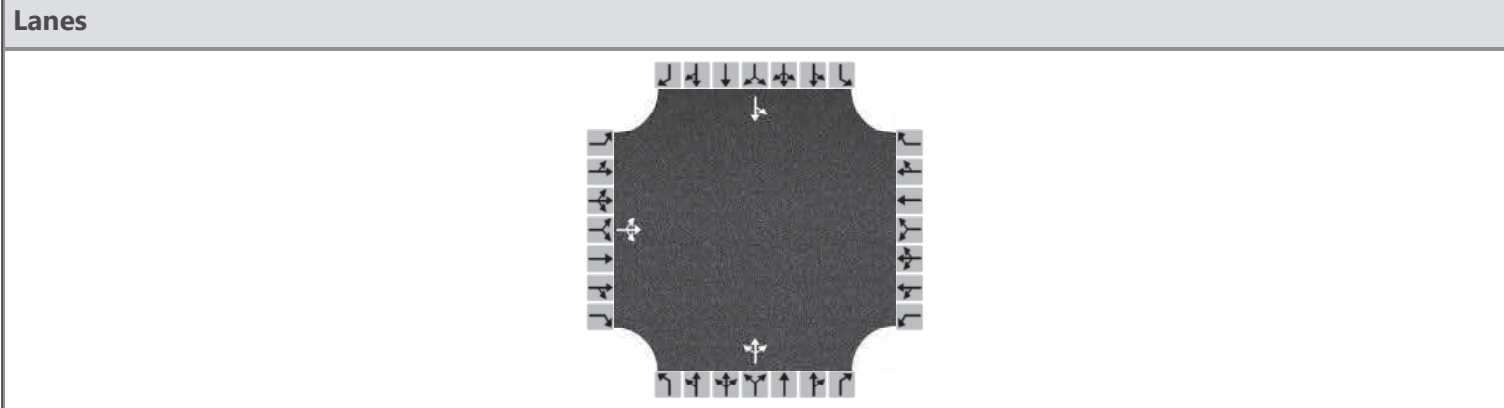
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						336									47	
Capacity, c (veh/h)						429									1138	
v/c Ratio						0.78									0.04	
95% Queue Length, Q ₉₅ (veh)						6.8									0.1	
Control Delay (s/veh)						37.6									8.3	
Level of Service (LOS)						E									A	
Approach Delay (s/veh)						37.6									1.7	
Approach LOS						E										

Appendix E
Traffic Analysis Worksheets (Future Campus
Capacity Redistributed to Reflect a Campus North
Approach Entry Lane to the Middle School and With
Mitigation Along Public Roadways)

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.60
Time Analyzed	AM Peak Hour		
Project Description	2021 Redistribution Future Capacity Mitigation		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	155	133	33				60	81	65	48	81	
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT		
Flow Rate, v (veh/h)	535						343			215		
Percent Heavy Vehicles	5						5			5		

Departure Headway and Service Time

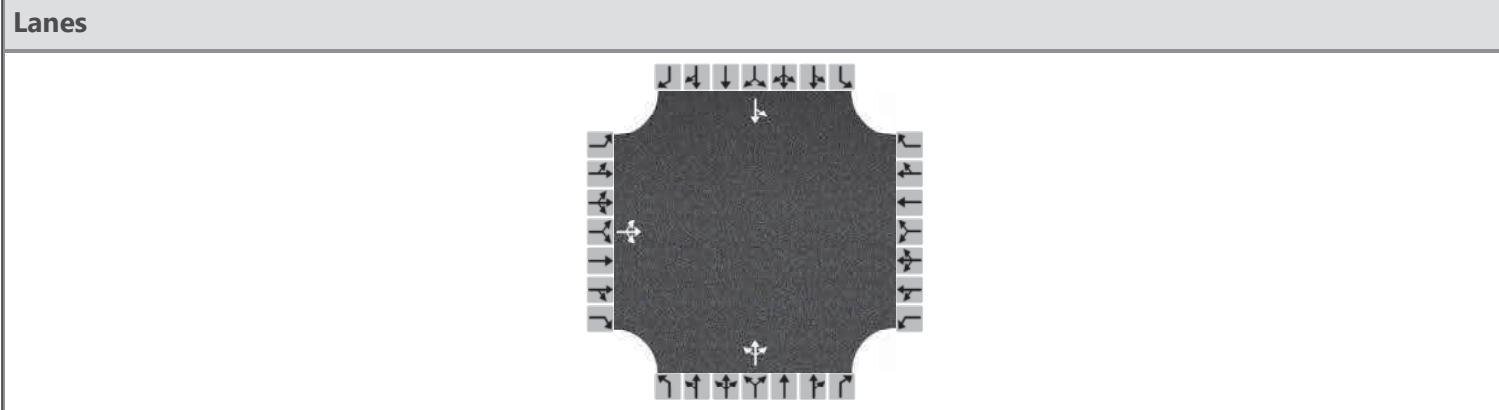
Initial Departure Headway, hd (s)	3.20						3.20			3.20		
Initial Degree of Utilization, x	0.476						0.305			0.191		
Final Departure Headway, hd (s)	5.69						5.95			6.38		
Final Degree of Utilization, x	0.845						0.567			0.381		
Move-Up Time, m (s)	2.0						2.0			2.0		
Service Time, ts (s)	3.69						3.95			4.38		

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	535						343			215		
Capacity	633						605			564		
95% Queue Length, Q ₉₅ (veh)	9.3						3.6			1.8		
Control Delay (s/veh)	32.0						16.5			13.3		
Level of Service, LOS	D						C			B		
Approach Delay (s/veh)	32.0						16.5			13.3		
Approach LOS	D						C			B		
Intersection Delay, s/veh LOS	23.4						C					

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	WJB	Intersection	Samson & Deinhard
Agency/Co.	MDSO	Jurisdiction	City of McCall
Date Performed	11/12/2021	East/West Street	E Deinhard Lane
Analysis Year	2021	North/South Street	S Samson Trail
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.60
Time Analyzed	PM Peak Hour		
Project Description	2021 Redistribution Future Capacity Mitigation		



Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	216	65	68				64	61	18	18	103	
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR						LTR			LT		
Flow Rate, v (veh/h)	582						238			202		
Percent Heavy Vehicles	5						5			5		

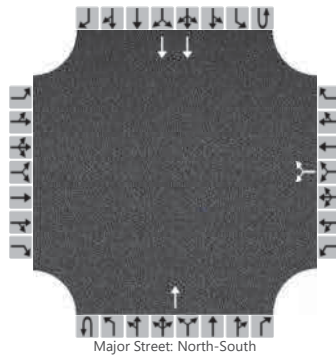
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20						3.20			3.20		
Initial Degree of Utilization, x	0.517						0.212			0.179		
Final Departure Headway, hd (s)	5.33						6.10			6.18		
Final Degree of Utilization, x	0.862						0.404			0.346		
Move-Up Time, m (s)	2.0						2.0			2.0		
Service Time, ts (s)	3.33						4.10			4.18		

Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	582						238			202		
Capacity	675						590			582		
95% Queue Length, Q ₉₅ (veh)	10.0						1.9			1.5		
Control Delay (s/veh)	32.3						13.2			12.4		
Level of Service, LOS	D						B			B		
Approach Delay (s/veh)	32.3						13.2			12.4		
Approach LOS	D						B			B		
Intersection Delay, s/veh LOS	23.9						C					

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & Middle Approach		
Agency/Co.	MDSO			Jurisdiction	City McCall		
Date Performed	11/12/2021			East/West Street	Campus Middle Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.63		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Mitigation						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	2	0
Configuration							LR				T				T	
Volume (veh/h)						128		31			236				359	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)						0										
Right Turn Channelized																
Median Type Storage						Undivided										

Critical and Follow-up Headways

Base Critical Headway (sec)						7.5		6.2								
Critical Headway (sec)						6.90		6.30								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

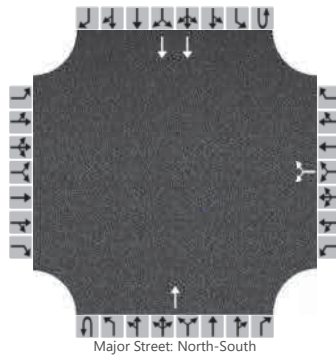
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						252										
Capacity, c (veh/h)						424										
v/c Ratio						0.60										
95% Queue Length, Q ₉₅ (veh)						3.8										
Control Delay (s/veh)						25.3										
Level of Service (LOS)						D										
Approach Delay (s/veh)						25.3										
Approach LOS						D										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & Middle Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus Middle Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.63		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Mitigation						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		0	1	0		0	1	0		0	2	0
Configuration							LR				T				T	
Volume (veh/h)						75		22			277				394	
Percent Heavy Vehicles (%)						5		5								
Proportion Time Blocked																
Percent Grade (%)							0									
Right Turn Channelized																
Median Type Storage							Undivided									

Critical and Follow-up Headways

Base Critical Headway (sec)						7.5		6.2								
Critical Headway (sec)						6.90		6.30								
Base Follow-Up Headway (sec)						3.5		3.3								
Follow-Up Headway (sec)						3.55		3.35								

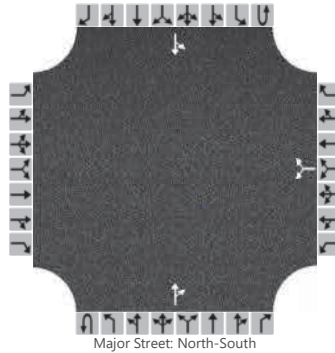
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						154										
Capacity, c (veh/h)						377										
v/c Ratio						0.41										
95% Queue Length, Q ₉₅ (veh)						1.9										
Control Delay (s/veh)						21.0										
Level of Service (LOS)						C										
Approach Delay (s/veh)						21.0										
Approach LOS						C										

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & North Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	AM Peak Hour			Peak Hour Factor	0.65		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Mitigation						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0	
Configuration							LR					TR		LT			
Volume (veh/h)						202		70			79	188		46	157		
Percent Heavy Vehicles (%)						5		5						3			
Proportion Time Blocked																	
Percent Grade (%)						0											
Right Turn Channelized																	
Median Type Storage						Left Only								2			

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

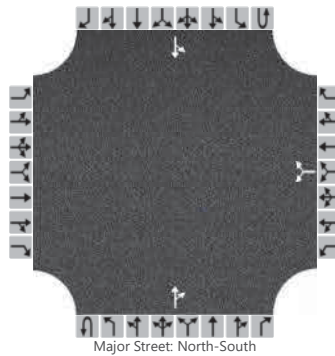
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						418								71			
Capacity, c (veh/h)						600								1143			
v/c Ratio						0.70								0.06			
95% Queue Length, Q ₉₅ (veh)						5.6								0.2			
Control Delay (s/veh)						23.7								8.4			
Level of Service (LOS)						C								A			
Approach Delay (s/veh)						23.7								2.4			
Approach LOS						C											

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	WJB			Intersection	Samson & North Approach		
Agency/Co.	MDSO			Jurisdiction	City of McCall		
Date Performed	11/12/2021			East/West Street	Campus North Approach		
Analysis Year	2021			North/South Street	S Samson Trail		
Time Analyzed	PM Peak Hour			Peak Hour Factor	0.65		
Intersection Orientation	North-South			Analysis Time Period (hrs)	0.25		
Project Description	2021 Redistribution Future Capacity Mitigation						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		0	0	0		0	1	0	0	0	1	0	0	0	1	0	
Configuration							LR					TR		LT			
Volume (veh/h)						220		22			143	156		34	174		
Percent Heavy Vehicles (%)						5		5						3			
Proportion Time Blocked																	
Percent Grade (%)						0											
Right Turn Channelized																	
Median Type Storage						Left Only								2			

Critical and Follow-up Headways

Base Critical Headway (sec)						7.1		6.2							4.1	
Critical Headway (sec)						6.45		6.25							4.13	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.55		3.35							2.23	

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						372									52		
Capacity, c (veh/h)						558									1096		
v/c Ratio						0.67									0.05		
95% Queue Length, Q ₉₅ (veh)						5.0									0.2		
Control Delay (s/veh)						23.4									8.4		
Level of Service (LOS)						C									A		
Approach Delay (s/veh)						23.4								1.8			
Approach LOS						C											

2016 - Samson Trail S of Deinhard

Start Date: 8/17/2016

ADT Values

Complete Days Only: 1,415

Directional Split

Direction 1: 52%

Direction 2: 48%

Total Equivalent Single Axle Load (ESAL)

1,146

Location (Decimal Degrees)

Latitude: 44.8975

Longitude: -116.0854

█ : Peak Volume

Hourly Volumes

Start Time	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	2	0	2	2	1	2	1	1	1	0	0	0	-	-	1	1
1:00 AM	-	-	2	0	1	0	1	0	1	1	0	0	0	0	-	-	1	0
2:00 AM	-	-	3	1	0	0	0	1	0	0	0	1	1	0	-	-	1	1
3:00 AM	-	-	0	1	0	0	0	1	1	1	0	0	0	0	-	-	0	1
4:00 AM	-	-	0	1	0	1	0	1	0	1	0	1	0	2	-	-	0	1
5:00 AM	-	-	1	7	0	8	1	3	1	2	2	7	1	3	-	-	1	5
6:00 AM	-	-	2	14	5	16	4	7	1	6	5	20	7	21	-	-	4	14
7:00 AM	-	-	15	61	16	50	4	25	5	10	15	49	13	31	-	-	11	38
8:00 AM	30	54	37	46	35	61	27	29	34	18	31	42	-	-	-	-	32	42
9:00 AM	47	50	32	49	51	58	26	42	31	40	49	47	-	42	-	40	39	48
10:00 AM	57	52	56	51	80	61	63	73	40	51	53	71	-	-	-	-	58	60
11:00 AM	61	76	46	54	69	71	57	57	45	51	36	48	-	-	-	-	52	60
12:00 PM	70	72	69	55	75	81	78	57	57	60	67	80	-	-	-	-	69	68
1:00 PM	65	65	58	47	60	62	60	61	49	49	80	51	-	61	-	-	62	56
2:00 PM	63	53	48	59	94	66	51	57	62	40	50	45	-	-	-	-	61	53
3:00 PM	73	60	60	47	69	67	63	44	36	34	65	69	-	-	-	-	61	54
4:00 PM	100	59	63	54	75	61	51	52	51	44	88	71	-	52	-	44	71	57
5:00 PM	74	62	79	44	93	59	62	34	54	38	93	55	-	-	-	-	76	49
6:00 PM	60	36	43	29	48	32	52	25	36	18	47	31	-	-	-	-	48	29
7:00 PM	40	22	46	31	37	27	35	21	38	17	42	24	-	-	-	-	40	24
8:00 PM	42	17	40	22	40	27	22	15	26	17	25	10	-	15	-	17	33	18
9:00 PM	25	13	25	17	20	22	24	17	9	6	4	8	-	-	-	-	18	14
10:00 PM	5	0	10	8	11	9	8	2	6	5	1	3	-	-	-	-	7	5
11:00 PM	3	1	4	3	3	1	1	1	1	1	1	3	-	-	-	-	2	2
Total Day	815	692	741	701	884	842	691	627	585	511	755	736	22	57	0	0		
	1507		1442		1726		1318		1096		1491		79		0			
AM Peak Volume	11:00 AM	11:00 AM	10:00 AM	7:00 AM	10:00 AM	11:00 AM	10:00 AM	10:00 AM	11:00 AM	10:00 AM	10:00 AM	10:00 AM	7:00 AM	7:00 AM				
	61	76	56	61	80	71	63	73	45	51	53	71	13	31				
PM Peak Volume	4:00 PM	12:00 PM	5:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	1:00 PM	2:00 PM	12:00 PM	5:00 PM	12:00 PM						
	100	72	79	59	94	81	78	61	62	60	93	80						

2016 - Samson Trail S of Deinhard

Daily Vehicle Classification Count

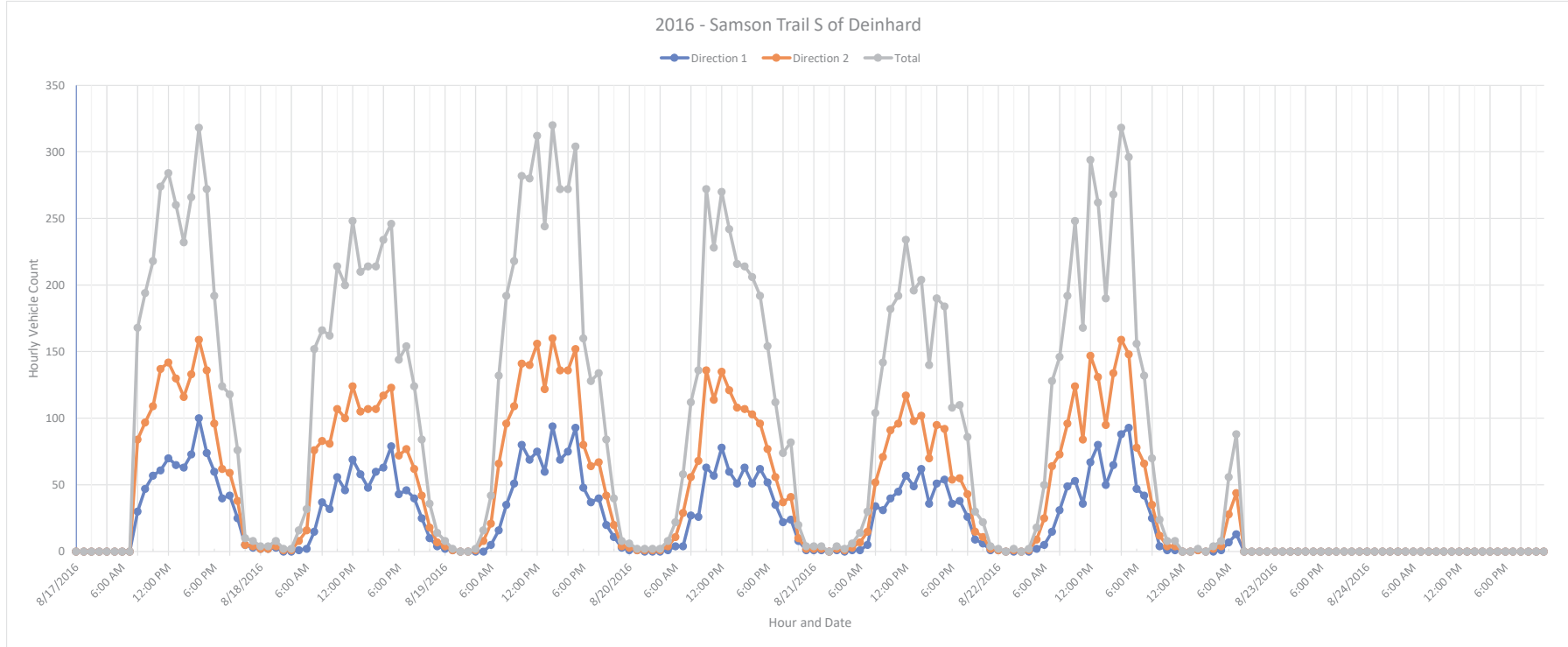
Class	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2		
Motorcycles sum daily %	3	4	3	4	5	6	3	5	1	3	2	5	0	0	0	0	17	27	TF	-
	7 0.5%		7 0.5%		11 0.6%		8 0.6%		4 0.4%		7 0.5%		0 0.0%		0 #DIV/0!		44 0.5%		ESAL	0
Cars & Trailers sum daily %	354	281	317	291	399	335	348	287	322	265	338	298	4	14	0	0	2082	1771	TF	-
	635 42.1%		608 42.2%		734 42.5%		635 48.2%		587 53.6%		636 42.7%		18 22.8%		0 #DIV/0!		3853 44.5%		ESAL	0
2 Axle Long sum daily %	208	164	179	160	191	229	170	169	138	119	187	186	4	13	0	0	1077	1040	TF	-
	372 24.7%		339 23.5%		420 24.3%		339 25.7%		257 23.4%		373 25.0%		17 21.5%		0 #DIV/0!		2117 24.4%		ESAL	0
Buses sum daily %	2	2	1	1	6	5	0	0	0	2	4	3	0	1	0	0	13	14	TF	1.016
	4 0.3%		2 0.1%		11 0.6%		0 0.0%		2 0.2%		7 0.5%		1 1.3%		0 #DIV/0!		27 0.3%		ESAL	27.432
2 Axle 6 Tire sum daily %	134	100	136	111	162	133	98	77	70	63	132	116	4	15	0	0	736	615	TF	0.62
	234 15.5%		247 17.1%		295 17.1%		175 13.3%		133 12.1%		248 16.6%		19 24.1%		0 #DIV/0!		1351 15.6%		ESAL	837.62
3 Axle Single sum daily %	16	3	22	3	14	7	7	0	7	0	21	2	3	2	0	0	90	17	TF	0.903
	19 1.3%		25 1.7%		21 1.2%		7 0.5%		7 0.6%		23 1.5%		5 6.3%		0 #DIV/0!		107 1.2%		ESAL	96.621
4 Axle Single sum daily %	1	0	0	0	1	1	0	0	0	0	1	1	0	0	0	0	3	2	TF	1.052
	1 0.1%		0 0.0%		2 0.1%		0 0.0%		0 0.0%		2 0.1%		0 0.0%		0 #DIV/0!		5 0.1%		ESAL	5.26
<5 Axle Double sum daily %	19	11	14	13	24	13	19	19	15	7	10	10	1	0	0	0	102	73	TF	0.852
	30 2.0%		27 1.9%		37 2.1%		38 2.9%		22 2.0%		20 1.3%		1 1.3%		0 #DIV/0!		175 2.0%		ESAL	149.1
5 Axle Double sum daily %	2	4	1	1	2	2	0	0	0	0	0	2	0	0	0	0	5	9	TF	1.704
	6 0.4%		2 0.1%		4 0.2%		0 0.0%		0 0.0%		2 0.1%		0 0.0%		0 #DIV/0!		14 0.2%		ESAL	23.856
>6 Axle Double sum daily %	0	1	0	1	0	0	1	0	0	0	0	0	0	0	0	0	1	2	TF	1.726
	1 0.1%		1 0.1%		0 0.0%		1 0.1%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		3 0.0%		ESAL	5.178
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
>6 Axi Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.119
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
Bicycles sum daily %	12	28	1	18	1	18	0	12	0	10	0	16	0	2	0	0	14	104	TF	-
	40 2.7%		19 1.3%		19 1.1%		12 0.9%		10 0.9%		16 1.1%		2 2.5%		0 #DIV/0!		118 1.4%		ESAL	0
Not Classed sum daily %	64	94	67	98	79	93	45	58	32	42	60	97	6	10	0	0	353	492	TF	-
	158 10.5%		165 11.4%		172 10.0%		103 7.8%		74 6.8%		157 10.5%		16 20.3%		0 #DIV/0!		845 9.8%		ESAL	0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 1146

2016 - Samson Trail S of Deinhard



2016 - Spring Mntn N of Deinhard (Aug)

Start Date: 8/17/2016

ADT Values

Complete Days Only: 2,511

Directional Split

Direction 1: 54%

Direction 2: 46%

Total Equivalent Single Axle Load (ESAL)

2,046

Location (Decimal Degrees)

Latitude: 44.9001

Longitude: -116.0855

: Peak Volume

Hourly Volumes

Start Time	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	1	0	1	2	0	2	2	3	2	1	0	1	-	-	1	2
1:00 AM	-	-	1	0	1	0	1	2	1	5	0	0	0	2	-	-	1	2
2:00 AM	-	-	2	0	0	1	0	2	1	1	0	2	3	1	-	-	1	1
3:00 AM	-	-	0	1	0	1	1	0	1	3	0	0	1	0	-	-	1	1
4:00 AM	-	-	1	2	0	1	1	2	0	0	1	1	1	1	-	-	1	1
5:00 AM	-	-	5	2	5	4	5	2	3	0	6	0	6	2	-	-	5	2
6:00 AM	-	-	17	11	19	11	10	10	8	6	21	21	18	7	-	-	16	11
7:00 AM	-	-	59	37	38	39	32	22	26	13	53	43	29	25	-	-	40	30
8:00 AM	94	61	77	62	84	68	77	55	68	27	87	50	-	-	-	-	81	54
9:00 AM	93	60	95	64	104	85	71	52	78	60	89	55	-	-	-	-	88	63
10:00 AM	119	80	118	70	124	77	128	111	126	59	98	71	-	-	-	-	119	78
11:00 AM	118	94	106	79	153	103	136	106	86	75	107	90	-	-	-	-	118	91
12:00 PM	123	112	126	102	129	109	123	112	100	108	132	114	-	-	-	-	122	110
1:00 PM	118	89	134	94	118	114	108	92	105	75	122	88	-	-	-	-	118	92
2:00 PM	132	92	122	95	144	139	97	107	112	74	103	86	-	-	-	-	118	99
3:00 PM	123	87	110	105	109	117	124	78	92	61	129	92	-	-	-	-	115	90
4:00 PM	132	123	119	129	149	134	97	80	115	61	129	130	-	-	-	-	124	110
5:00 PM	89	105	113	92	84	108	92	72	76	73	76	114	-	-	-	-	88	94
6:00 PM	80	65	70	76	71	81	75	72	72	41	65	72	-	-	-	-	72	68
7:00 PM	45	56	56	66	54	67	71	54	61	35	57	56	-	-	-	-	57	56
8:00 PM	54	46	62	62	44	66	40	42	30	40	40	29	-	-	-	-	45	48
9:00 PM	23	28	34	38	21	72	30	37	19	16	11	27	-	-	-	-	23	36
10:00 PM	14	11	11	14	9	18	16	20	11	11	6	6	-	-	-	-	11	13
11:00 PM	7	2	6	5	8	7	5	4	1	4	4	4	-	-	-	-	5	4
Total Day	1364	1111	1445	1206	1469	1424	1340	1136	1194	851	1338	1152	58	39	0	0		
	2475		2651		2893		2476		2045		2490		97		0			
AM Peak Volume	10:00 AM	11:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	10:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	7:00 AM	7:00 AM				
	119	94	118	79	153	103	136	111	126	75	107	90	29	25				
PM Peak Volume	2:00 PM	4:00 PM	1:00 PM	4:00 PM	4:00 PM	2:00 PM	3:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	4:00 PM						
	132	123	134	129	149	139	124	112	115	108	132	130						

2016 - Spring Mntn N of Deinhard (Aug)

Daily Vehicle Classification Count

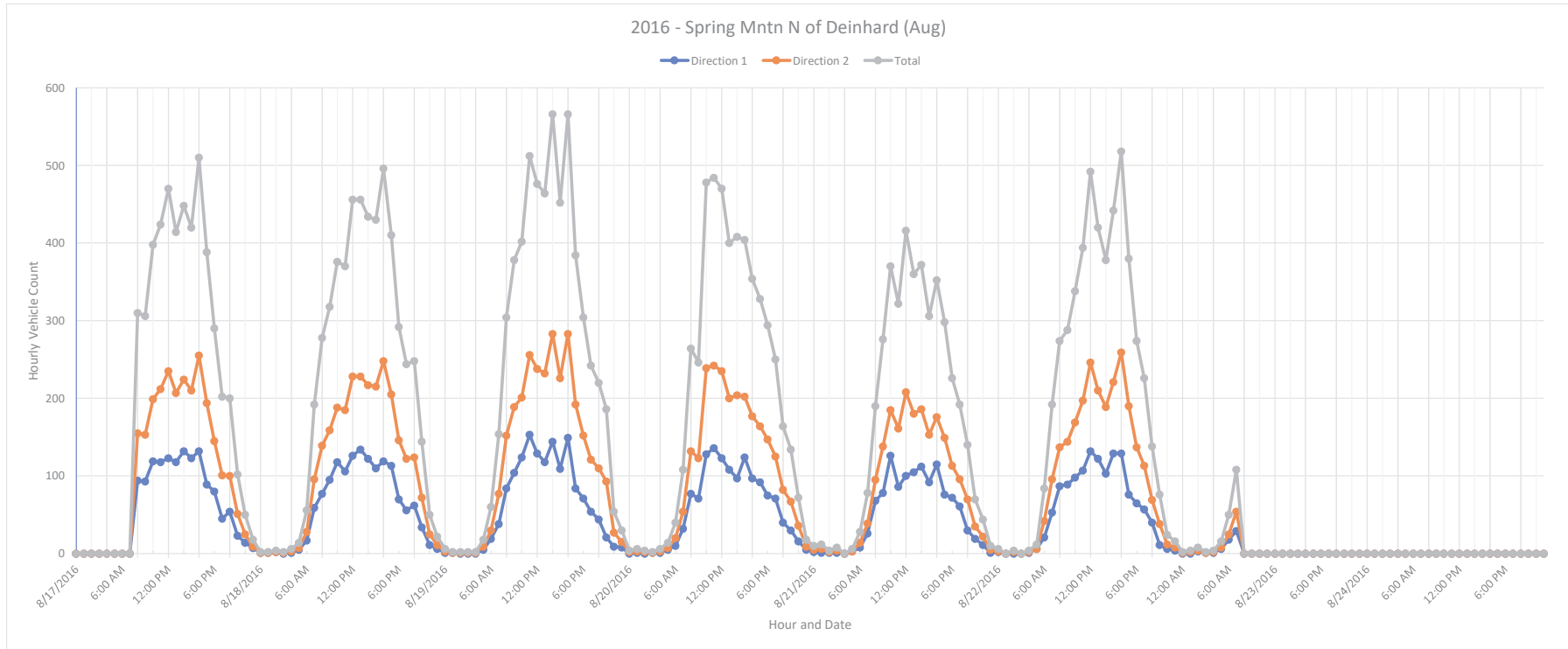
Class	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Totals		Equivalent Axle Loads		
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2			
Motorcycles sum daily %	4	3	9	6	4	3	12	7	11	2	9	5	0	0	0	0	49	26	TF	-	
		7 0.3%		15 0.6%		7 0.2%		19 0.8%		13 0.6%		14 0.6%		0 0.0%		0 #DIV/0!		75 0.5%		ESAL	0
Cars & Trailers sum daily %	568	574	622	637	649	770	637	656	557	495	606	627	27	14	0	0	3666	3773	TF	-	
	1142 46.1%		1259 47.5%		1419 49.0%		1293 52.2%		1052 51.4%		1233 49.5%		41 42.3%		0 #DIV/0!		7439 49.2%		ESAL	0	
2 Axle Long sum daily %	365	333	416	353	397	391	358	296	346	233	378	311	18	13	0	0	2278	1930	TF	-	
	698 28.2%		769 29.0%		788 27.2%		654 26.4%		579 28.3%		689 27.7%		31 32.0%		0 #DIV/0!		4208 27.8%		ESAL	0	
Buses sum daily %	7	4	6	6	5	6	0	0	2	1	4	2	0	0	0	0	24	19	TF	1.016	
	11 0.4%		12 0.5%		11 0.4%		0 0.0%		3 0.1%		6 0.2%		0 0.0%		0 #DIV/0!		43 0.3%		ESAL	43.688	
2 Axle 6 Tire sum daily %	322	138	294	126	328	170	251	104	216	83	281	141	12	7	0	0	1704	769	TF	0.62	
	460 18.6%		420 15.8%		498 17.2%		355 14.3%		299 14.6%		422 16.9%		19 19.6%		0 #DIV/0!		2473 16.3%		ESAL	1533.26	
3 Axle Single sum daily %	20	16	17	18	18	16	8	9	3	0	18	11	0	1	0	0	84	71	TF	0.903	
	36 1.5%		35 1.3%		34 1.2%		17 0.7%		3 0.1%		29 1.2%		1 1.0%		0 #DIV/0!		155 1.0%		ESAL	139.965	
4 Axle Single sum daily %	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	TF	1.052	
	0 0.0%		1 0.0%		1 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		2 0.0%		ESAL	2.104	
<5 Axle Double sum daily %	39	12	44	28	38	27	39	16	45	15	26	25	1	0	0	0	232	123	TF	0.852	
	51 2.1%		72 2.7%		65 2.2%		55 2.2%		60 2.9%		51 2.0%		1 1.0%		0 #DIV/0!		355 2.3%		ESAL	302.46	
5 Axle Double sum daily %	3	2	2	1	2	1	0	0	0	0	0	0	0	0	0	0	7	4	TF	1.704	
	5 0.2%		3 0.1%		3 0.1%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		11 0.1%		ESAL	18.744	
>6 Axle Double sum daily %	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	1	TF	1.726	
	2 0.1%		0 0.0%		1 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		3 0.0%		ESAL	5.178	
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043	
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0	
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508	
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0	
>6 Axi Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.119	
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0	
Bicycles sum daily %	3	0	0	0	2	4	3	1	1	0	1	0	0	0	0	0	10	5	TF	-	
	3 0.1%		0 0.0%		6 0.2%		4 0.2%		1 0.0%		1 0.0%		0 0.0%		0 #DIV/0!		15 0.1%		ESAL	0	
Not Classed sum daily %	32	28	35	30	25	35	32	47	13	22	15	30	0	4	0	0	152	196	TF	-	
	60 2.4%		65 2.5%		60 2.1%		79 3.2%		35 1.7%		45 1.8%		4 4.1%		0 #DIV/0!		348 2.3%		ESAL	0	

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

Total Roadway ESAL **2046**

*TF = Truck Factor

2016 - Spring Mntn N of Deinhard (Aug)



2016 - Spring Mntn N of Deinhard (April)

Start Date: 4/27/2016

ADT Values

Complete Days Only: 1,390

Directional Split

Direction 1: 53%

Direction 2: 47%

Total Equivalent Single Axle Load (ESAL)

967

Location (Decimal Degrees)

Latitude: 44.9

Longitude: -116.0854

 : Peak Volume

Hourly Volumes

Start Time	4/27/2016 Wednesday		4/28/2016 Thursday		4/29/2016 Friday		4/30/2016 Saturday		5/1/2016 Sunday		5/2/2016 Monday		5/3/2016 Tuesday		5/4/2016 Wednesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	0	0	2	0	1	3	1	1	0	2	0	0	0	0	1	1
1:00 AM	-	-	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0
2:00 AM	-	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
3:00 AM	-	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:00 AM	-	-	2	2	1	0	1	0	0	1	1	0	0	0	0	0	1	0
5:00 AM	-	-	6	0	6	1	0	0	0	0	4	2	0	0	0	0	2	0
6:00 AM	-	-	8	12	10	14	8	6	7	2	11	17	0	0	0	0	6	7
7:00 AM	-	-	86	43	81	36	18	14	10	11	90	34	0	0	0	0	41	20
8:00 AM	21	24	73	62	69	53	25	17	39	9	65	64	0	0	0	1	37	29
9:00 AM	55	45	53	53	42	25	49	29	43	25	57	34	0	0	1	0	38	26
10:00 AM	67	63	54	35	54	36	65	40	52	29	66	56	0	0	0	1	45	33
11:00 AM	61	57	86	57	69	45	75	66	49	29	82	64	0	0	-	-	60	45
12:00 PM	79	71	82	83	68	48	64	58	45	77	73	60	0	0	-	-	59	57
1:00 PM	75	61	72	64	73	60	64	50	53	41	67	64	0	0	-	-	58	49
2:00 PM	76	58	80	65	81	64	51	71	50	41	75	51	0	0	-	-	59	50
3:00 PM	90	88	73	78	57	99	65	53	50	47	88	79	0	0	-	-	60	63
4:00 PM	75	69	64	67	69	64	58	54	45	40	20	15	0	0	-	-	47	44
5:00 PM	52	56	78	72	62	72	45	51	44	31	0	0	0	0	-	-	40	40
6:00 PM	55	35	34	42	34	43	40	42	39	38	0	0	0	0	-	-	29	29
7:00 PM	15	33	21	28	21	39	31	35	25	28	0	0	0	0	-	-	16	23
8:00 PM	12	32	13	24	27	30	14	19	16	22	0	0	0	0	-	-	12	18
9:00 PM	14	23	2	16	12	20	6	10	5	12	0	0	0	0	-	-	6	12
10:00 PM	5	3	9	8	3	8	3	10	3	3	0	0	0	0	-	-	3	5
11:00 PM	4	3	3	4	4	3	1	9	1	4	0	0	0	0	-	-	2	3
Total Day	756	721	899	816	845	761	685	637	577	491	699	542	0	0	1	2		
	1477		1715		1606		1322		1068		1241		0		3			
AM Peak Volume	10:00 AM	10:00 AM	7:00 AM	8:00 AM	7:00 AM	8:00 AM	11:00 AM	11:00 AM	10:00 AM	10:00 AM	7:00 AM	8:00 AM	12:00 AM	12:00 AM	9:00 AM	8:00 AM		
	67	63	86	62	81	53	75	66	52	29	90	64			1	1		
PM Peak Volume	3:00 PM	3:00 PM	12:00 PM	12:00 PM	2:00 PM	3:00 PM	3:00 PM	2:00 PM	1:00 PM	12:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM				
	90	88	82	83	81	99	65	71	53	77	88	79						

2016 - Spring Mntn N of Deinhard (April)

Daily Vehicle Classification Count

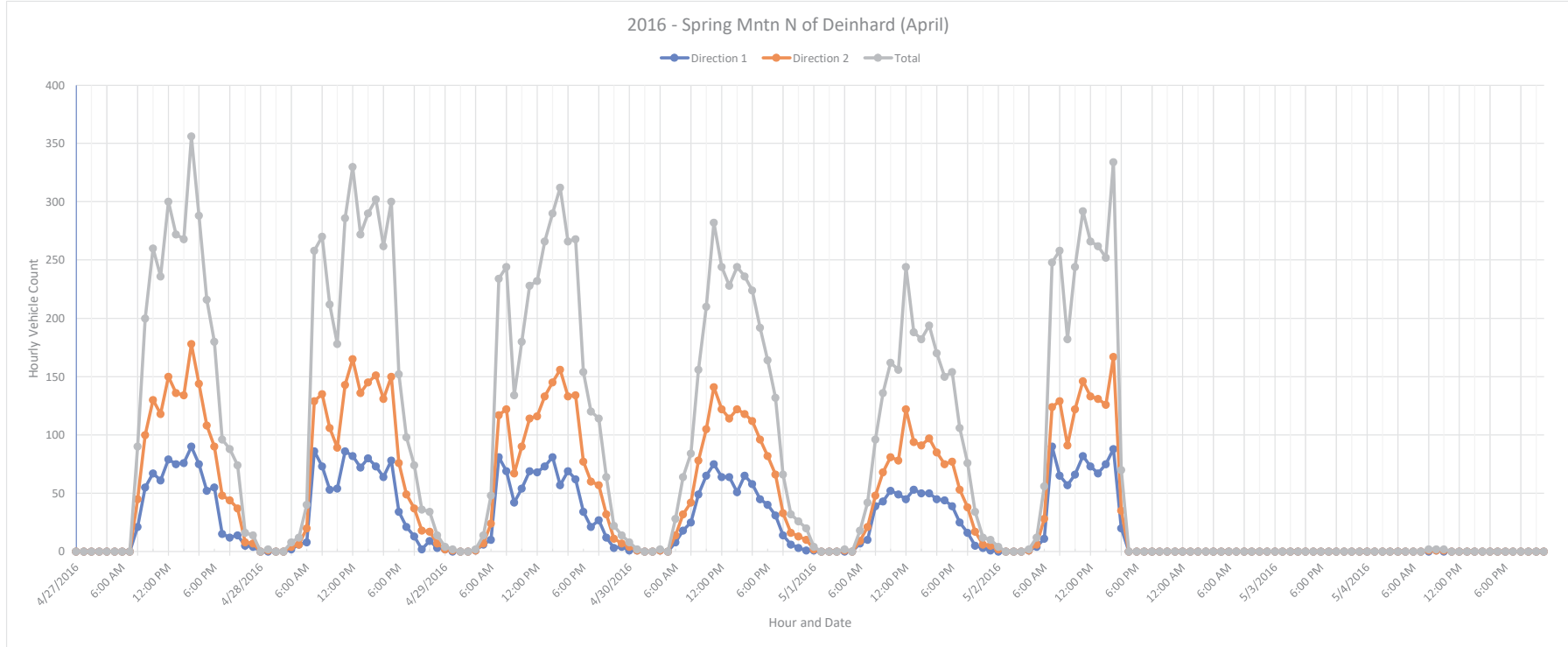
Class	4/27/2016 Wednesday		4/28/2016 Thursday		4/29/2016 Friday		4/30/2016 Saturday		5/1/2016 Sunday		5/2/2016 Monday		5/3/2016 Tuesday		5/4/2016 Wednesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles sum daily %	0	2	1	2	1	0	0	2	2	3	2	2	0	0	0	0	6	11	TF	-
		2 0.1%		3 0.2%		1 0.1%		2 0.2%		5 0.5%		4 0.3%		0 #DIV/0!		0 0.0%		17 0.2%	ESAL	0
Cars & Trailers sum daily %	385	399	461	448	442	431	385	384	300	282	332	266	0	0	0	0	2305	2210	TF	-
	784	53.1%	909	53.0%	873	54.4%	769	58.2%	582	54.5%	598	48.2%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	4515	53.5%	ESAL	0
2 Axle Long sum daily %	210	196	246	221	233	208	191	175	194	150	195	160	0	0	0	0	1269	1110	TF	-
	406	27.5%	467	27.2%	441	27.5%	366	27.7%	344	32.2%	355	28.6%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	2379	28.2%	ESAL	0
Buses sum daily %	1	3	2	6	2	3	3	0	1	1	1	2	0	0	0	0	10	15	TF	1.016
	4	0.3%	8	0.5%	5	0.3%	3	0.2%	2	0.2%	3	0.2%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	25	0.3%	ESAL	25.4
2 Axle 6 Tire sum daily %	104	75	133	90	134	86	86	59	66	45	118	74	0	0	0	0	641	429	TF	0.62
	179	12.1%	223	13.0%	220	13.7%	145	11.0%	111	10.4%	192	15.5%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	1070	12.7%	ESAL	663.4
3 Axle Single sum daily %	25	23	28	29	15	13	6	5	1	1	14	13	0	0	0	0	89	84	TF	0.903
	48	3.2%	57	3.3%	28	1.7%	11	0.8%	2	0.2%	27	2.2%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	173	2.1%	ESAL	156.219
4 Axle Single sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.052
	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	0	0.0%	ESAL	0
<5 Axle Double sum daily %	17	11	14	11	9	9	10	7	7	2	13	10	0	0	0	0	70	50	TF	0.852
	28	1.9%	25	1.5%	18	1.1%	17	1.3%	9	0.8%	23	1.9%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	120	1.4%	ESAL	102.24
5 Axle Double sum daily %	2	1	1	1	1	2	0	0	0	1	0	0	0	0	0	0	4	5	TF	1.704
	3	0.2%	2	0.1%	3	0.2%	0	0.0%	1	0.1%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	9	0.1%	ESAL	15.336
>6 Axle Double sum daily %	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	1	TF	1.726
	0	0.0%	0	0.0%	2	0.1%	0	0.0%	0	0.0%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	2	0.0%	ESAL	3.452
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043
	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	0	0.0%	ESAL	0
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	0	0.0%	ESAL	0
>6 Axi Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.119
	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	0	0.0%	ESAL	0
Bicycles sum daily %	1	3	0	1	1	1	0	1	1	2	1	1	0	0	0	0	4	9	TF	-
	4	0.3%	1	0.1%	2	0.1%	1	0.1%	3	0.3%	2	0.2%	0 #DIV/0!	0 0.0%	0 0.0%	0 0.0%	13	0.2%	ESAL	0
Not Classed sum daily %	11	8	13	7	6	7	4	4	5	4	23	14	0	0	1	2	63	46	TF	-
	19	1.3%	20	1.2%	13	0.8%	8	0.6%	9	0.8%	37	3.0%	0 #DIV/0!	0 0.0%	3 100.0%	109	1.3%	ESAL	0	

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 967

2016 - Spring Mntn N of Deinhard (April)



2016 - Deinhard W of Spring Mntn (Aug)

Start Date: 8/17/2016

ADT Values

Complete Days Only: 3,350

Directional Split

Direction 1: 49%

Direction 2: 51%

Total Equivalent Single Axle Load (ESAL)

2,665

Location (Decimal Degrees)

Latitude: 44.8978

Longitude: -116.0865

█ : Peak Volume

Hourly Volumes

Start Time	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	0	3	2	2	2	2	2	3	2	2	0	1	-	-	1	2
1:00 AM	-	-	0	0	1	1	0	2	1	6	0	0	0	2	-	-	0	2
2:00 AM	-	-	1	1	0	1	2	3	1	1	0	1	2	1	-	-	1	1
3:00 AM	-	-	0	0	1	1	2	0	0	2	0	0	1	0	-	-	1	1
4:00 AM	-	-	1	1	1	1	2	2	1	0	1	0	1	0	-	-	1	1
5:00 AM	-	-	7	2	9	3	6	3	4	0	9	1	7	1	-	-	7	2
6:00 AM	-	-	24	15	25	18	12	11	10	4	31	28	27	19	-	-	22	16
7:00 AM	0	0	87	61	72	67	41	22	28	16	79	71	22	47	-	-	47	41
8:00 AM	129	75	94	83	117	97	88	69	58	38	118	98	-	-	-	-	101	77
9:00 AM	110	86	129	90	140	111	84	56	78	72	114	121	-	-	-	-	109	89
10:00 AM	141	196	136	108	153	139	151	140	119	61	173	164	-	-	-	-	146	135
11:00 AM	156	130	140	117	185	151	155	130	106	85	147	138	-	-	-	-	148	125
12:00 PM	174	165	166	161	154	203	137	166	101	125	184	175	-	-	-	-	153	166
1:00 PM	144	135	154	158	140	163	128	122	115	97	157	181	-	-	-	-	140	143
2:00 PM	156	140	157	131	168	203	117	131	111	104	152	144	-	-	-	-	144	142
3:00 PM	163	135	145	153	151	174	129	121	88	68	156	199	-	-	-	-	139	142
4:00 PM	163	176	138	172	179	181	114	105	112	85	244	291	-	-	-	-	158	168
5:00 PM	109	146	122	136	110	158	98	113	78	103	161	195	-	-	-	-	113	142
6:00 PM	82	113	61	102	86	126	67	101	70	55	90	122	-	-	-	-	76	103
7:00 PM	61	91	71	98	65	86	70	74	65	51	71	93	-	-	-	-	67	82
8:00 PM	40	55	52	74	44	90	38	47	31	58	32	35	-	-	-	-	40	60
9:00 PM	24	38	38	50	31	96	26	46	23	22	15	30	-	-	-	-	26	47
10:00 PM	12	11	12	18	12	27	13	30	8	9	6	3	-	-	-	-	11	16
11:00 PM	4	1	4	6	6	7	5	5	2	5	5	3	-	-	-	-	4	5
Total Day	1668	1693	1739	1740	1852	2106	1487	1501	1212	1070	1947	2095	60	71	0	0		
	3361		3479		3958		2988		2282		4042		131		0			
AM Peak Volume	11:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	10:00 AM	10:00 AM	10:00 AM	10:00 AM	10:00 AM	6:00 AM	7:00 AM				
	156	196	140	117	185	151	155	140	119	85	173	164	27	47				
PM Peak Volume	12:00 PM	4:00 PM	12:00 PM	4:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	4:00 PM	4:00 PM						
	174	176	166	172	179	203	137	166	115	125	244	291						

2016 - Deinhard W of Spring Mntn (Aug)

Daily Vehicle Classification Count

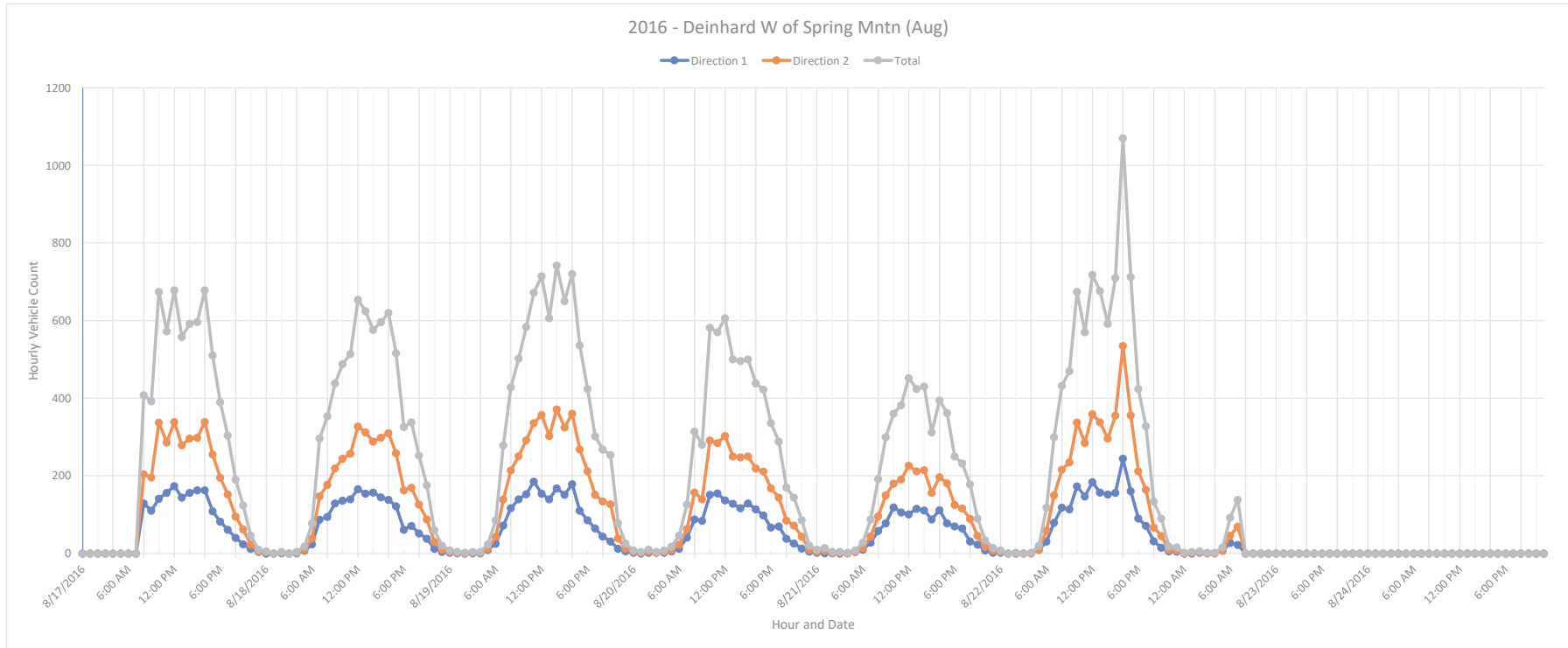
Class	8/17/2016 Wednesday		8/18/2016 Thursday		8/19/2016 Friday		8/20/2016 Saturday		8/21/2016 Sunday		8/22/2016 Monday		8/23/2016 Tuesday		8/24/2016 Wednesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles sum daily %	2	28	4	30	4	47	6	34	2	17	3	42	0	0	0	0	21	198	TF	-
	30 0.9%		34 1.0%		51 1.3%		40 1.3%		19 0.8%		45 1.1%		0 0.0%		0 #DIV/0!		219 1.1%		ESAL	0
Cars & Trailers sum daily %	805	632	869	659	871	744	788	616	631	449	974	715	23	32	0	0	4961	3847	TF	-
	1437 42.8%		1528 43.9%		1615 40.8%		1404 47.0%		1080 47.3%		1689 41.8%		55 42.0%		0 #DIV/0!		8808 43.5%		ESAL	0
2 Axle Long sum daily %	420	338	424	347	462	478	352	356	300	262	485	519	17	7	0	0	2460	2307	TF	-
	758 22.6%		771 22.2%		940 23.7%		708 23.7%		562 24.6%		1004 24.8%		24 18.3%		0 #DIV/0!		4767 23.6%		ESAL	0
Buses sum daily %	8	5	8	3	8	3	2	1	3	0	9	4	0	0	0	0	38	16	TF	1.016
	13 0.4%		11 0.3%		11 0.3%		3 0.1%		3 0.1%		13 0.3%		0 0.0%		0 #DIV/0!		54 0.3%		ESAL	54.864
2 Axle 6 Tire sum daily %	273	197	286	216	312	231	220	152	182	107	306	191	17	8	0	0	1596	1102	TF	0.62
	470 14.0%		502 14.4%		543 13.7%		372 12.4%		289 12.7%		497 12.3%		25 19.1%		0 #DIV/0!		2698 13.3%		ESAL	1672.76
3 Axle Single sum daily %	23	108	18	108	18	104	7	70	3	42	21	123	1	4	0	0	91	559	TF	0.903
	131 3.9%		126 3.6%		122 3.1%		77 2.6%		45 2.0%		144 3.6%		5 3.8%		0 #DIV/0!		650 3.2%		ESAL	586.95
4 Axle Single sum daily %	0	2	0	9	0	8	0	0	0	0	0	3	0	0	0	0	0	22	TF	1.052
	2 0.1%		9 0.3%		8 0.2%		0 0.0%		0 0.0%		3 0.1%		0 0.0%		0 #DIV/0!		22 0.1%		ESAL	23.144
<5 Axle Double sum daily %	30	22	37	24	36	26	30	14	34	9	23	21	1	1	0	0	191	117	TF	0.852
	52 1.5%		61 1.8%		62 1.6%		44 1.5%		43 1.9%		44 1.1%		2 1.5%		0 #DIV/0!		308 1.5%		ESAL	262.416
5 Axle Double sum daily %	4	2	3	7	3	3	0	1	0	3	2	7	0	0	0	0	12	23	TF	1.704
	6 0.2%		10 0.3%		6 0.2%		1 0.0%		3 0.1%		9 0.2%		0 0.0%		0 #DIV/0!		35 0.2%		ESAL	59.64
>6 Axle Double sum daily %	1	0	0	1	0	0	0	0	0	0	1	0	0	0	0	0	2	1	TF	1.726
	1 0.0%		1 0.0%		0 0.0%		0 0.0%		0 0.0%		1 0.0%		0 0.0%		0 #DIV/0!		3 0.0%		ESAL	5.178
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
>6 Axl Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.119
	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		ESAL	0
Bicycles sum daily %	5	63	2	68	9	145	2	106	3	72	13	202	0	6	0	0	34	662	TF	-
	68 2.0%		70 2.0%		154 3.9%		108 3.6%		75 3.3%		215 5.3%		6 4.6%		0 #DIV/0!		696 3.4%		ESAL	0
Not Classed sum daily %	97	296	88	268	129	317	80	151	54	109	110	268	1	13	0	0	559	1422	TF	-
	393 11.7%		356 10.2%		446 11.3%		231 7.7%		163 7.1%		378 9.4%		14 10.7%		0 #DIV/0!		1981 9.8%		ESAL	0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 2665

2016 - Deinhard W of Spring Mntn (Aug)



2016 - Deinhard W of Spring Mountain (April)

Start Date: 4/27/2016

ADT Values

Directional Split

Total Equivalent Single Axle Load (ESAL)

Complete Days Only: 2,098

Direction 1: 51%

1,713

Direction 2: 49%

Location (Decimal Degrees)

Latitude: 44.8979

Longitude: -116.0866

█ : Peak Volume

Hourly Volumes

Start Time	4/27/2016 Wednesday		4/28/2016 Thursday		4/29/2016 Friday		4/30/2016 Saturday		5/1/2016 Sunday		5/2/2016 Monday		5/3/2016 Tuesday		5/4/2016 Wednesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	0	0	2	0	0	6	2	4	0	3	0	0	0	0	1	2
1:00 AM	-	-	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0
2:00 AM	-	-	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0
3:00 AM	-	-	0	0	2	2	0	0	1	0	0	0	0	0	0	0	0	0
4:00 AM	-	-	2	1	2	1	2	0	0	1	2	1	0	0	0	0	1	1
5:00 AM	-	-	7	0	7	1	2	0	3	2	6	2	0	0	0	0	4	1
6:00 AM	-	-	12	23	20	19	11	5	5	2	16	22	0	0	0	0	9	10
7:00 AM	-	-	169	171	146	150	29	16	17	11	173	172	0	0	0	0	76	74
8:00 AM	-	-	135	63	146	81	31	19	37	16	152	73	0	0	1	0	72	36
9:00 AM	79	71	75	64	70	42	57	68	46	31	81	52	0	0	0	1	51	41
10:00 AM	83	70	85	71	78	53	94	64	69	37	90	83	0	0	0	1	62	47
11:00 AM	97	99	118	93	114	94	111	74	54	42	119	105	0	0	-	-	88	72
12:00 PM	111	95	94	107	91	86	85	74	46	77	106	89	0	0	-	-	76	75
1:00 PM	98	88	91	87	103	92	81	80	62	65	100	92	0	0	-	-	76	72
2:00 PM	103	192	98	148	104	145	61	90	49	47	96	135	0	0	-	-	73	108
3:00 PM	55	213	188	105	189	140	77	70	59	52	196	160	0	0	-	-	109	106
4:00 PM	147	76	112	118	85	107	57	60	64	48	10	12	0	0	-	-	68	60
5:00 PM	106	126	99	114	95	104	48	51	54	43	0	0	0	0	-	-	57	63
6:00 PM	79	64	47	71	45	60	44	52	34	49	0	0	0	0	-	-	36	42
7:00 PM	32	44	36	46	23	49	35	40	33	33	0	0	0	0	-	-	23	30
8:00 PM	19	40	24	36	22	41	18	26	24	23	0	0	0	0	-	-	15	24
9:00 PM	40	43	5	13	13	19	8	16	14	16	0	0	0	0	-	-	11	15
10:00 PM	10	6	8	7	4	11	2	9	4	4	0	0	0	0	-	-	4	5
11:00 PM	3	3	4	4	5	3	2	10	2	3	0	0	0	0	-	-	2	3
Total Day	1062	1230	1409	1343	1366	1301	856	830	680	607	1147	1001	0	0	1	2		
	2292		2752		2667		1686		1287		2148		0		3			
AM Peak Volume	11:00 AM	11:00 AM	7:00 AM	7:00 AM	7:00 AM	7:00 AM	11:00 AM	11:00 AM	10:00 AM	11:00 AM	7:00 AM	7:00 AM	12:00 AM	12:00 AM	8:00 AM	9:00 AM		
	97	99	169	171	146	150	111	74	69	42	173	172			1	1		
PM Peak Volume	4:00 PM	3:00 PM	3:00 PM	2:00 PM	3:00 PM	2:00 PM	12:00 PM	2:00 PM	4:00 PM	12:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM				
	147	213	188	148	189	145	85	90	64	77	196	160						

2016 - Deinhard W of Spring Mountain (April)

Daily Vehicle Classification Count

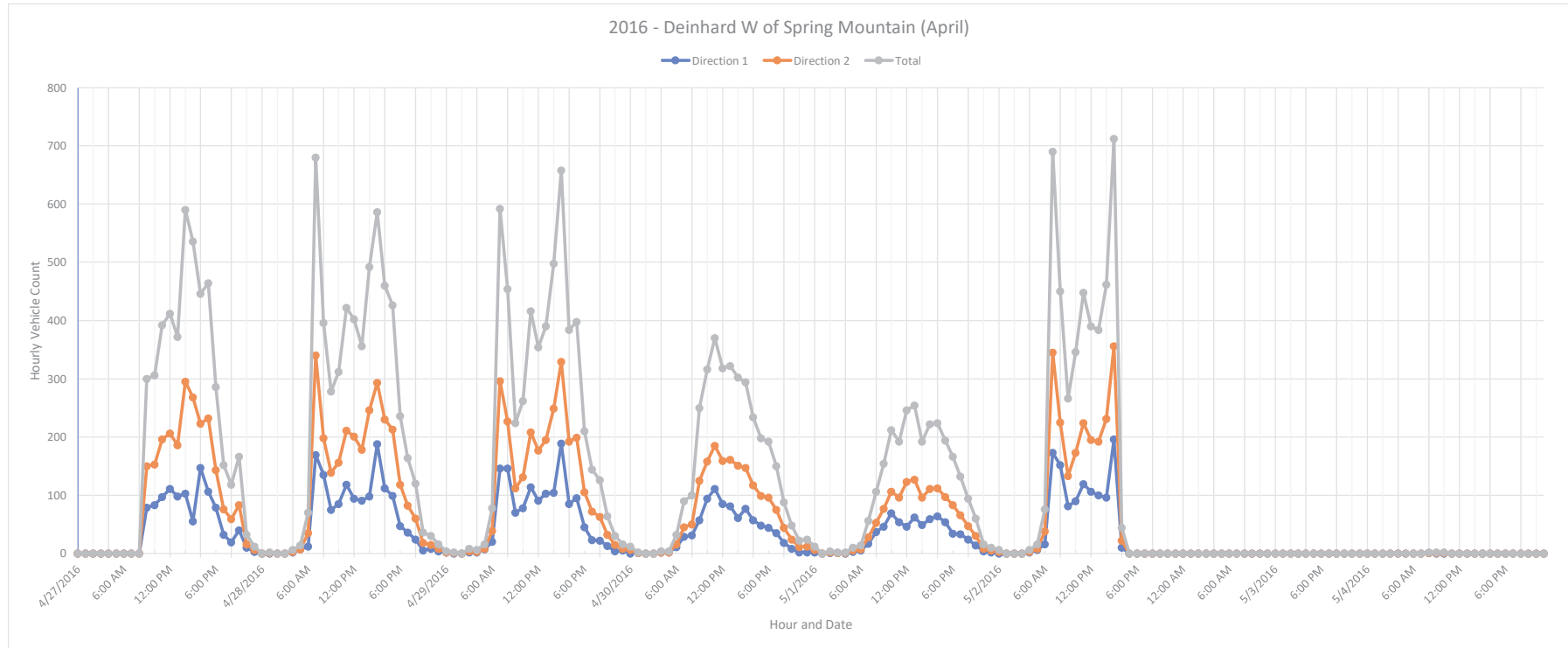
Class	4/27/2016 Wednesday		4/28/2016 Thursday		4/29/2016 Friday		4/30/2016 Saturday		5/1/2016 Sunday		5/2/2016 Monday		5/3/2016 Tuesday		5/4/2016 Wednesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles sum daily %	0	13	4	3	0	0	1	3	5	6	3	2	0	0	0	0	13	27	TF	-
		13 0.6%		7 0.3%		0 0.0%		4 0.2%		11 0.9%		5 0.2%		0 #DIV/0!		0 0.0%		40 0.3%	ESAL	0
Cars & Trailers sum daily %	381	457	724	686	652	605	459	422	347	306	573	462	0	0	0	0	3136	2938	TF	-
		838 36.6%		1410 51.2%		1257 47.1%		881 52.3%		653 50.7%		1035 48.2%		0 #DIV/0!		0 0.0%		6074 47.3%	ESAL	0
2 Axle Long sum daily %	260	280	354	364	385	374	252	224	215	161	309	267	0	0	0	0	1775	1670	TF	-
		540 23.6%		718 26.1%		759 28.5%		476 28.2%		376 29.2%		576 26.8%		0 #DIV/0!		0 0.0%		3445 26.8%	ESAL	0
Buses sum daily %	6	45	13	19	15	19	0	0	1	2	13	17	0	0	0	0	48	102	TF	1.016
		51 2.2%		32 1.2%		34 1.3%		0 0.0%		3 0.2%		30 1.4%		0 #DIV/0!		0 0.0%		150 1.2%	ESAL	152.4
2 Axle 6 Tire sum daily %	190	122	233	197	249	219	114	149	90	115	161	156	0	0	0	0	1037	958	TF	0.62
		312 13.6%		430 15.6%		468 17.5%		263 15.6%		205 15.9%		317 14.8%		0 #DIV/0!		0 0.0%		1995 15.5%	ESAL	1236.9
3 Axle Single sum daily %	14	12	29	27	17	15	9	7	1	4	19	17	0	0	0	0	89	82	TF	0.903
		26 1.1%		56 2.0%		32 1.2%		16 0.9%		5 0.4%		36 1.7%		0 #DIV/0!		0 0.0%		171 1.3%	ESAL	154.413
4 Axle Single sum daily %	0	5	0	1	1	0	0	1	0	0	1	1	0	0	0	0	2	8	TF	1.052
		5 0.2%		1 0.0%		1 0.0%		1 0.1%		0 0.0%		2 0.1%		0 #DIV/0!		0 0.0%		10 0.1%	ESAL	10.52
<5 Axle Double sum daily %	13	8	17	13	15	22	12	9	11	5	13	10	0	0	0	0	81	67	TF	0.852
		21 0.9%		30 1.1%		37 1.4%		21 1.2%		16 1.2%		23 1.1%		0 #DIV/0!		0 0.0%		148 1.2%	ESAL	126.096
5 Axle Double sum daily %	3	1	3	1	1	2	0	0	2	2	0	2	0	0	0	0	9	8	TF	1.704
		4 0.2%		4 0.1%		3 0.1%		0 0.0%		4 0.3%		2 0.1%		0 #DIV/0!		0 0.0%		17 0.1%	ESAL	28.968
>6 Axle Double sum daily %	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	1	TF	1.726
		0 0.0%		0 0.0%		2 0.1%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		2 0.0%	ESAL	3.452
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043
		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		0 0.0%	ESAL	0
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		0 0.0%	ESAL	0
>6 Axl Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.119
		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 #DIV/0!		0 0.0%		0 0.0%	ESAL	0
Bicycles sum daily %	4	29	4	2	1	4	2	2	0	1	7	5	0	0	0	0	18	43	TF	-
		33 1.4%		6 0.2%		5 0.2%		4 0.2%		1 0.1%		12 0.6%		0 #DIV/0!		0 0.0%		61 0.5%	ESAL	0
Not Classed sum daily %	191	258	28	30	29	40	7	13	8	5	48	62	0	0	1	2	312	410	TF	-
		449 19.6%		58 2.1%		69 2.6%		20 1.2%		13 1.0%		110 5.1%		0 #DIV/0!		3 100.0%		722 5.6%	ESAL	0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 1713

2016 - Deinhard W of Spring Mountain (April)



2019 - Spring Mnt Blvd N of Deinhard

Start Date: 7/9/2019

ADT Values

Directional Split

Total Equivalent Single Axle Load (ESAL)

Complete Days Only: 3,396

Direction 1: 55%

4,828

Direction 2: 45%

Location (Decimal Degrees)

Latitude: 44.9001

Longitude: -116.0854

█ : Peak Volume

Hourly Volumes

Start Time	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	1	3	1	2	2	2	8	4	8	2	5	3	2	7	4	3
1:00 AM	-	-	1	1	0	0	2	4	1	2	2	3	1	1	1	3	1	2
2:00 AM	-	-	2	1	0	0	1	1	5	3	2	1	2	1	1	1	2	1
3:00 AM	-	-	1	0	2	0	0	0	1	0	1	2	0	0	0	1	1	0
4:00 AM	-	-	0	1	0	0	0	1	3	0	0	1	1	0	0	1	1	1
5:00 AM	-	-	5	1	4	4	5	2	5	2	2	0	15	4	4	2	6	2
6:00 AM	-	-	21	18	29	33	25	27	22	6	18	6	32	24	35	17	26	19
7:00 AM	-	-	75	47	63	48	58	63	42	30	51	23	86	49	47	30	60	41
8:00 AM	64	44	115	87	107	84	102	78	104	57	88	33	97	83	-	-	97	67
9:00 AM	146	88	120	92	113	94	122	94	130	92	140	68	108	74	-	-	126	86
10:00 AM	128	96	120	92	132	109	161	122	141	99	133	91	145	84	-	-	137	99
11:00 AM	128	106	135	119	135	131	184	136	180	128	130	122	172	123	-	-	152	124
12:00 PM	189	129	152	135	171	123	202	167	183	157	164	116	192	139	-	-	179	138
1:00 PM	162	129	194	118	165	129	173	157	189	162	156	111	182	112	-	-	174	131
2:00 PM	152	113	151	120	154	133	159	135	153	128	130	81	193	147	-	-	156	122
3:00 PM	139	101	150	105	152	121	144	149	149	125	143	127	167	127	-	-	149	122
4:00 PM	136	150	162	118	160	126	179	172	125	115	126	94	165	116	-	-	150	127
5:00 PM	135	131	145	105	132	100	136	153	117	122	122	82	140	137	-	-	132	119
6:00 PM	104	83	90	82	92	105	90	117	98	91	90	72	101	97	-	-	95	92
7:00 PM	78	71	61	74	61	69	105	104	110	66	67	57	68	64	-	-	79	72
8:00 PM	54	49	44	62	67	65	65	92	66	50	74	32	55	48	-	-	61	57
9:00 PM	23	27	32	47	49	60	44	77	41	74	31	41	21	41	-	-	34	52
10:00 PM	11	11	14	19	30	26	25	52	22	44	9	17	12	14	-	-	18	26
11:00 PM	5	3	4	6	8	9	9	14	16	22	6	4	2	1	-	-	7	8
Total Day	1654	1331	1795	1453	1827	1571	1993	1919	1911	1579	1693	1186	1962	1489	90	62		
	2985		3248		3398		3912		3490		2879		3451		152			
AM Peak Volume	9:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	9:00 AM	11:00 AM	11:00 AM	11:00 AM	7:00 AM	7:00 AM		
	146	106	135	119	135	131	184	136	180	128	140	122	172	123	47	30		
PM Peak Volume	12:00 PM	4:00 PM	1:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	4:00 PM	1:00 PM	1:00 PM	12:00 PM	3:00 PM	2:00 PM	2:00 PM				
	189	150	194	135	171	133	202	172	189	162	164	127	193	147				

2019 - Spring Mnt Blvd N of Deinhard

Daily Vehicle Classification Count

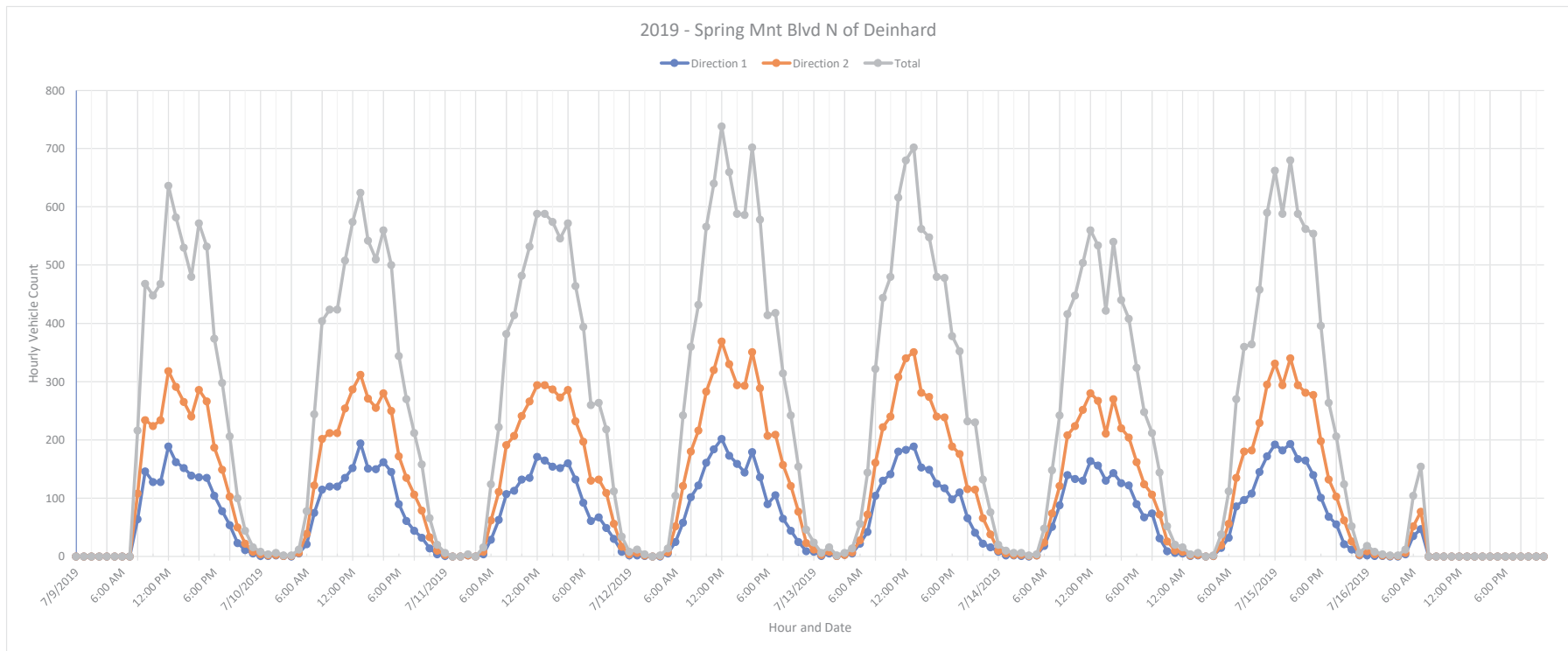
Class	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles sum daily %	8 13 0.4%	5	13 18 0.6%	5	13 20 0.6%	7	12 25 0.6%	13	17 24 0.7%	7	15 21 0.7%	6	11 15 0.4%	4	1 1 0.7%	0	90 137 0.6%	47	TF ESAL	- 0
Cars & Trailers sum daily %	579 1134 38.0%	555	589 1134 34.9%	545	560 1138 33.5%	578	621 1317 33.7%	696	594 1271 36.4%	677	568 1070 37.2%	502	628 1197 34.7%	569	30 55 36.2%	25	4169 8316 35.4%	4147	TF ESAL	- 0
2 Axle Long sum daily %	475 831 27.8%	356	524 890 27.4%	366	570 1008 29.7%	438	620 1153 29.5%	533	723 1193 34.2%	470	623 987 34.3%	364	680 1105 32.0%	425	29 42 27.6%	13	4244 7209 30.7%	2965	TF ESAL	- 0
Buses sum daily %	13 21 0.7%	8	16 24 0.7%	8	24 43 1.3%	19	16 27 0.7%	11	2 4 0.1%	2	1 2 0.1%	1	17 30 0.9%	13	0 0 0.0%	0	89 151 0.6%	62	TF ESAL	1.016 153.416
2 Axle 6 Tire sum daily %	446 757 25.4%	311	513 913 28.1%	400	528 946 27.8%	418	541 1018 26.0%	477	455 792 22.7%	337	398 660 22.9%	262	491 875 25.4%	384	28 50 32.9%	22	3400 6011 25.6%	2611	TF ESAL	0.62 3726.82
3 Axle Single sum daily %	23 38 1.3%	15	25 41 1.3%	16	19 31 0.9%	12	31 63 1.6%	32	1 4 0.1%	3	3 3 0.1%	0	12 16 0.5%	4	0 0 0.0%	0	114 196 0.8%	82	TF ESAL	0.903 176.988
4 Axle Single sum daily %	1 2 0.1%	1	0 3 0.1%	3	1 2 0.1%	1	2 6 0.2%	4	0 0 0.0%	0	0 0 0.0%	0	0 1 0.0%	1	0 0 0.0%	0	4 14 0.1%	10	TF ESAL	1.052 14.728
<5 Axle Double sum daily %	51 76 2.5%	25	57 108 3.3%	51	64 115 3.4%	51	83 164 4.2%	81	57 92 2.6%	35	53 73 2.5%	20	65 119 3.4%	54	1 2 1.3%	1	431 749 3.2%	318	TF ESAL	0.852 638.148
5 Axle Double sum daily %	15 21 0.7%	6	8 14 0.4%	6	5 9 0.3%	4	4 8 0.2%	4	1 2 0.1%	1	1 1 0.0%	0	4 5 0.1%	1	0 0 0.0%	0	38 60 0.3%	22	TF ESAL	1.704 102.24
>6 Axle Double sum daily %	1 2 0.1%	1	2 3 0.1%	1	1 2 0.1%	1	0 1 0.0%	1	0 0 0.0%	0	0 0 0.0%	0	0 1 0.0%	1	0 0 0.0%	0	4 9 0.0%	5	TF ESAL	1.726 15.534
<6 Axle Multi sum daily %	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	TF ESAL	2.043 0
6 Axle Multi sum daily %	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	TF ESAL	1.508 0
>6 Axl Multi sum daily %	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	0 0 0.0%	0	TF ESAL	2.119 0
Bicycles sum daily %	1 5 0.2%	4	2 5 0.2%	3	4 4 0.1%	0	5 8 0.2%	3	5 7 0.2%	2	1 1 0.0%	0	4 4 0.1%	0	0 0 0.0%	0	22 34 0.1%	12	TF ESAL	- 0
Not Classed sum daily %	41 85 2.8%	44	46 95 2.9%	49	38 80 2.4%	42	58 122 3.1%	64	56 101 2.9%	45	30 61 2.1%	31	50 83 2.4%	33	1 2 1.3%	1	320 629 2.7%	309	TF ESAL	- 0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 4828

2019 - Spring Mnt Blvd N of Deinhard



2019 - Samson Trail S of Deinhard

Start Date: 7/9/2019

ADT Values

Complete Days Only: 2,157

Directional Split

Direction 1: 44%

Direction 2: 56%

Total Equivalent Single Axle Load (ESAL)

3,305

Location (Decimal Degrees)

Latitude: 44.8975

Longitude: -116.0855

█ : Peak Volume

Hourly Volumes

Start Time	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	4	1	1	2	1	2	8	2	9	3	2	1	3	1	4	2
1:00 AM	-	-	1	0	0	0	1	1	1	1	1	2	2	1	0	0	1	1
2:00 AM	-	-	1	1	0	1	1	0	1	2	3	1	1	0	0	1	1	1
3:00 AM	-	-	1	1	3	2	1	1	1	0	0	0	0	1	0	1	1	1
4:00 AM	-	-	0	3	0	3	1	7	1	2	0	0	0	1	0	5	0	3
5:00 AM	-	-	3	11	3	10	3	10	5	2	2	4	4	8	3	5	3	7
6:00 AM	-	-	12	27	10	27	10	23	7	22	3	17	16	30	9	31	10	25
7:00 AM	-	-	30	61	22	72	30	56	19	43	10	28	29	79	11	64	22	58
8:00 AM	13	0	37	87	32	89	32	83	30	40	27	68	37	80	-	-	30	64
9:00 AM	60	96	42	103	40	91	46	103	53	91	56	83	41	121	-	-	48	98
10:00 AM	56	102	45	96	54	85	67	124	65	78	51	71	47	84	-	-	55	91
11:00 AM	61	112	74	128	70	113	90	91	66	108	63	86	67	118	-	-	70	108
12:00 PM	77	110	68	108	88	112	75	114	85	92	88	87	70	112	-	-	79	105
1:00 PM	75	119	85	104	68	97	97	114	79	62	72	77	73	107	-	-	78	97
2:00 PM	76	99	91	84	82	74	68	99	73	79	68	79	84	103	-	-	77	88
3:00 PM	68	89	73	88	78	79	95	75	78	84	70	64	97	96	-	-	80	82
4:00 PM	78	107	101	91	97	98	105	95	63	67	64	57	86	99	-	-	85	88
5:00 PM	98	89	114	97	92	100	80	92	50	59	66	52	101	82	-	-	86	82
6:00 PM	67	77	84	76	58	93	66	52	66	59	50	60	68	91	-	-	66	73
7:00 PM	53	44	47	59	65	55	51	43	33	69	34	62	48	61	-	-	47	56
8:00 PM	46	26	31	36	54	34	37	53	32	51	44	23	43	88	-	-	41	44
9:00 PM	23	20	29	19	35	26	34	23	34	32	33	27	16	21	-	-	29	24
10:00 PM	21	10	12	11	16	11	23	15	26	16	7	7	7	14	-	-	16	12
11:00 PM	6	6	5	3	8	2	11	9	13	10	3	2	7	4	-	-	8	5
Total Day	878	1106	990	1295	976	1276	1025	1285	889	1071	824	960	946	1402	26	108		
	1984		2285		2252		2310		1960		1784		2348		134			
AM Peak Volume	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	9:00 AM	7:00 AM	7:00 AM		
	61	112	74	128	70	113	90	124	66	108	63	86	67	121	11	64		
PM Peak Volume	5:00 PM	1:00 PM	5:00 PM	12:00 PM	4:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM	12:00 PM				
	98	119	114	108	97	112	105	114	85	92	88	87	101	112				

2019 - Samson Trail S of Deinhard

Daily Vehicle Classification Count

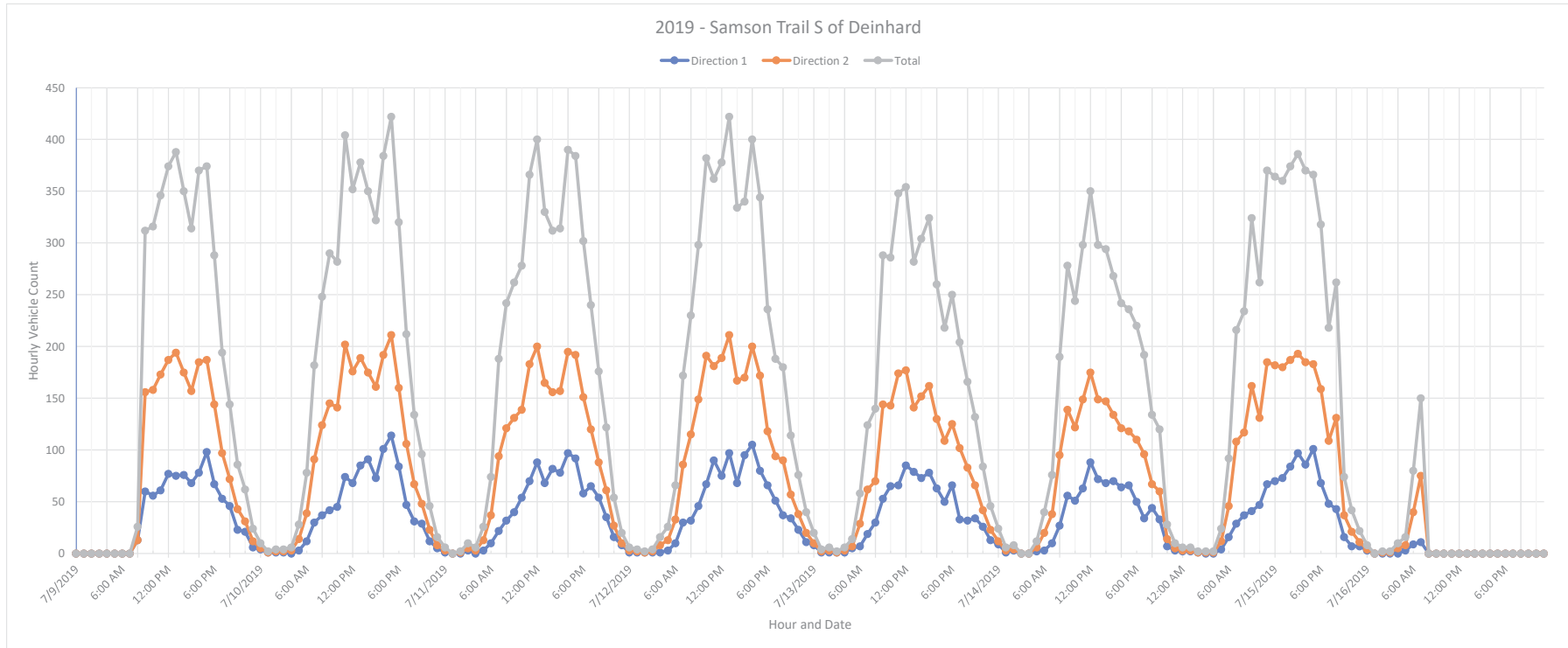
Class	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles sum daily %	3	5	2	8	11	6	2	7	11	12	8	7	6	4	0	0	43	49	TF	-
Cars & Trailers sum daily %	331	409	329	456	339	468	384	497	355	440	329	429	354	517	7	40	2428	3256	TF	-
2 Axle Long sum daily %	213	283	241	317	228	324	214	313	208	256	191	204	214	342	4	28	1513	2067	TF	-
Buses sum daily %	3	6	4	7	3	12	2	8	2	3	1	3	1	7	0	0	16	46	TF	1.016
2 Axle 6 Tire sum daily %	204	313	279	391	235	362	261	374	200	272	195	251	241	382	9	35	1624	2380	TF	0.62
3 Axle Single sum daily %	30	3	38	4	48	11	44	5	20	6	22	2	34	14	2	0	238	45	TF	0.903
4 Axle Single sum daily %	2	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	3	0	TF	1.052
<5 Axle Double sum daily %	21	32	17	39	33	51	45	32	31	32	20	31	33	47	1	4	201	268	TF	0.852
5 Axle Double sum daily %	2	5	7	7	3	6	3	1	3	0	0	1	3	7	0	0	21	27	TF	1.704
>6 Axle Double sum daily %	0	1	1	0	0	1	1	0	0	0	0	0	0	2	0	0	2	4	TF	1.726
<6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	TF	2.043
6 Axle Multi sum daily %	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
>6 Axi Multi sum daily %	1	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	1	2	TF	2.119
Bicycles sum daily %	2	4	5	6	3	6	5	5	1	4	1	5	1	5	0	0	18	35	TF	-
Not Classed sum daily %	66	45	67	60	73	29	63	43	58	46	57	27	59	72	3	1	446	323	TF	-
	111	5.6%	127	5.6%	102	4.5%	106	4.6%	104	5.3%	84	4.7%	131	5.6%	4	3.0%	769	5.1%	ESAL	0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 3305

2019 - Samson Trail S of Deinhard



2019 - E Deinhard W of Spring Mnt Blvd

Start Date: 7/9/2019

ADT Value

Directional Split

Total Equivalent Single Axle Load (ESAL)

Complete Days Only: 4,022

Direction 1: 49%

4,121

Direction 2: 51%

Location (Decimal Degrees)

Latitude: 44.8978

Longitude: -116.0865

█ : Peak Volume

Hourly Volumes

Start Time	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Hourly Average	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
12:00 AM	-	-	0	4	1	3	1	2	9	10	7	11	7	3	0	7	4	6
1:00 AM	-	-	0	1	0	0	2	4	2	3	1	2	1	6	1	2	1	3
2:00 AM	-	-	1	1	0	0	0	0	4	2	1	3	1	1	1	0	1	1
3:00 AM	-	-	1	1	0	2	1	2	0	0	2	1	1	0	0	1	1	1
4:00 AM	-	-	2	1	0	0	0	3	3	0	2	1	2	0	0	1	1	1
5:00 AM	-	-	8	3	7	5	10	4	7	6	3	0	15	5	3	2	8	4
6:00 AM	-	-	30	28	35	35	38	35	22	12	13	6	38	27	39	10	31	22
7:00 AM	-	-	94	63	88	58	79	60	53	44	47	22	110	56	38	27	73	47
8:00 AM	17	13	126	100	120	85	116	96	96	70	80	40	119	102	-	-	96	72
9:00 AM	137	110	149	108	135	142	133	113	162	125	125	74	144	110	-	-	141	112
10:00 AM	147	136	145	122	150	146	181	163	145	127	132	104	161	104	-	-	152	129
11:00 AM	146	139	149	157	140	176	191	188	196	164	128	153	176	168	-	-	161	164
12:00 PM	214	177	164	179	187	193	218	205	194	211	144	155	186	180	-	-	187	186
1:00 PM	183	187	179	152	184	175	176	207	188	222	139	141	197	149	-	-	178	176
2:00 PM	166	141	156	190	180	186	175	176	156	179	127	101	202	183	-	-	166	165
3:00 PM	156	149	153	152	162	161	153	231	155	172	128	151	160	183	-	-	152	171
4:00 PM	163	190	163	174	158	205	188	225	141	138	135	139	167	181	-	-	159	179
5:00 PM	140	192	119	197	124	206	144	210	125	165	117	106	133	217	-	-	129	185
6:00 PM	102	128	113	134	118	125	96	157	77	141	81	93	119	112	-	-	101	127
7:00 PM	77	122	58	94	100	100	85	139	91	100	61	79	65	96	-	-	77	104
8:00 PM	46	76	49	69	45	90	72	122	65	61	58	51	52	66	-	-	55	76
9:00 PM	20	45	29	62	44	86	51	74	42	83	24	67	30	44	-	-	34	66
10:00 PM	8	23	18	22	28	31	34	54	30	55	17	24	12	18	-	-	21	32
11:00 PM	5	6	5	9	5	16	8	22	12	33	4	4	2	5	-	-	6	14
Total Day	1727	1834	1911	2023	2011	2226	2152	2492	1975	2123	1576	1528	2100	2016	82	50		
	3561		3934		4237		4644		4098		3104		4116		132			
AM Peak Volume	10:00 AM	11:00 AM	9:00 AM	11:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	10:00 AM	11:00 AM	11:00 AM	11:00 AM	6:00 AM	7:00 AM		
	147	139	149	157	150	176	191	188	196	164	132	153	176	168	39	27		
PM Peak Volume	12:00 PM	5:00 PM	1:00 PM	5:00 PM	12:00 PM	5:00 PM	12:00 PM	3:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	2:00 PM	5:00 PM				
	214	192	179	197	187	206	218	231	194	222	144	155	202	217				

2019 - E Deinhard W of Spring Mnt Blvd

Daily Vehicle Classification Count

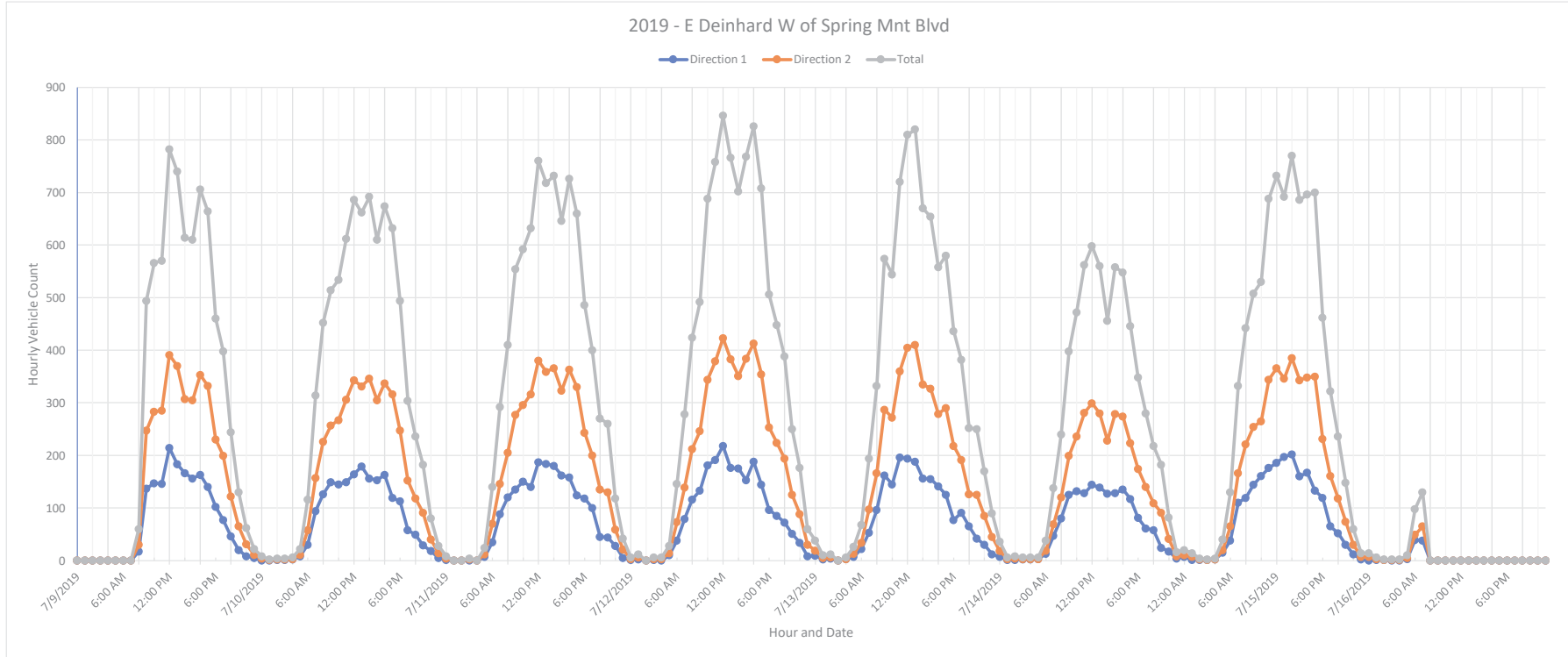
Class	7/9/2019 Tuesday		7/10/2019 Wednesday		7/11/2019 Thursday		7/12/2019 Friday		7/13/2019 Saturday		7/14/2019 Sunday		7/15/2019 Monday		7/16/2019 Tuesday		Totals		Equivalent Axle Loads	
	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
Motorcycles	6	67	6	56	10	79	10	107	11	111	11	85	6	76	1	0	61	581	TF	-
sum daily %	73 2.0%		62 1.6%		89 2.1%		117 2.5%		122 3.0%		96 3.1%		82 2.0%		1 0.8%		642 2.3%		ESAL	0
Cars & Trailers	729	526	777	520	818	586	848	643	857	602	694	437	903	520	36	13	5662	3847	TF	-
sum daily %	1255 35.2%		1297 33.0%		1404 33.1%		1491 32.1%		1459 35.6%		1131 36.4%		1423 34.6%		49 37.1%		9509 34.2%		ESAL	0
2 Axle Long	387	283	458	314	483	314	517	331	468	269	414	181	472	295	20	3	3219	1990	TF	-
sum daily %	670 18.8%		772 19.6%		797 18.8%		848 18.3%		737 18.0%		595 19.2%		767 18.6%		23 17.4%		5209 18.7%		ESAL	0
Buses	9	4	17	1	20	8	15	3	6	1	5	2	16	1	0	0	88	20	TF	1.016
sum daily %	13 0.4%		18 0.5%		28 0.7%		18 0.4%		7 0.2%		7 0.2%		17 0.4%		0 0.0%		108 0.4%		ESAL	109.728
2 Axle 6 Tire	370	210	420	250	444	253	458	249	389	206	301	148	429	228	21	9	2832	1553	TF	0.62
sum daily %	580 16.3%		670 17.0%		697 16.5%		707 15.2%		595 14.5%		449 14.5%		657 16.0%		30 22.7%		4385 15.8%		ESAL	2718.7
3 Axle Single	33	68	19	85	13	115	19	132	4	100	4	72	12	97	0	3	104	672	TF	0.903
sum daily %	101 2.8%		104 2.6%		128 3.0%		151 3.3%		104 2.5%		76 2.4%		109 2.6%		3 2.3%		776 2.8%		ESAL	700.728
4 Axle Single	0	7	0	1	0	3	0	4	0	1	0	0	1	2	0	1	1	19	TF	1.052
sum daily %	7 0.2%		1 0.0%		3 0.1%		4 0.1%		1 0.0%		0 0.0%		3 0.1%		1 0.8%		20 0.1%		ESAL	21.04
<5 Axle Double	39	20	52	32	50	29	48	45	49	24	43	11	53	22	0	1	334	184	TF	0.852
sum daily %	59 1.7%		84 2.1%		79 1.9%		93 2.0%		73 1.8%		54 1.7%		75 1.8%		1 0.8%		518 1.9%		ESAL	441.336
5 Axle Double	14	2	6	5	5	7	4	6	1	1	0	2	4	4	0	0	34	27	TF	1.704
sum daily %	16 0.4%		11 0.3%		12 0.3%		10 0.2%		2 0.0%		2 0.1%		8 0.2%		0 0.0%		61 0.2%		ESAL	103.944
>6 Axle Double	1	3	1	1	1	2	2	0	0	0	0	0	0	1	0	0	5	7	TF	1.726
sum daily %	4 0.1%		2 0.1%		3 0.1%		2 0.0%		0 0.0%		0 0.0%		1 0.0%		0 0.0%		12 0.0%		ESAL	20.712
<6 Axle Multi	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	2.043
sum daily %	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		ESAL	0
6 Axle Multi	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TF	1.508
sum daily %	0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		0 0.0%		ESAL	0
>6 Axl Multi	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	TF	2.119
sum daily %	0 0.0%		0 0.0%		0 0.0%		1 0.0%		1 0.0%		0 0.0%		0 0.0%		0 0.0%		2 0.0%		ESAL	4.238
Bicycles	10	102	12	102	11	122	17	144	19	117	4	87	16	105	0	2	89	781	TF	-
sum daily %	112 3.1%		114 2.9%		133 3.1%		161 3.5%		136 3.3%		91 2.9%		121 2.9%		2 1.5%		870 3.1%		ESAL	0
Not Classed	129	542	143	656	156	708	214	827	171	690	100	503	188	665	4	18	1105	4609	TF	-
sum daily %	671 18.8%		799 20.3%		864 20.4%		1041 22.4%		861 21.0%		603 19.4%		853 20.7%		22 16.7%		5714 20.5%		ESAL	0

Note: The first 3 classes, bicycles, and not classed are negligible in ESAL calculations.

*TF = Truck Factor

Total Roadway ESAL 4121

2019 - E Deinhard W of Spring Mnt Blvd



STORMWATER APPLICATION
City of McCall

Fill in all information. Submit one copy of signed application and three copies of Stormwater Management Plan/Report to the City Engineer.

1. Project Name: Payette Lakes Middle School addition
Location: 111 S. Samson Trail

2. Owner's Name: McCall- Donnelly School District
Street: 120 Idaho St. City: McCall
State: Idaho Zip Code: 83638 Phone: 208-634-2162

3. Project Description: 12,000 square foot classroom and multipurpose space building addition to the Payette Lakes Middle School.

 - a. Total property area, in acres. 39.6 acres
 - b. Proposed impervious surface (asphalt, rooftop, concrete, sidewalk, etc.) in square feet. approximately 8,000 sf of new impervious surface

 - c. Describe existing vegetation present on site. Existing turf grass and natural undeveloped vegetation.

 - d. Start date of construction. March 2022
 - e. Estimated length of time to complete improvements. 12 months

4. Stormwater Management Plan/Report attached? Yes No

5. Circle the section of the Stormwater Management Plan/Report Checklist which are applicable to project.
A B C D E F

6. Party responsible for operation and maintenance of project, including maintenance of temporary and permanent Best Management Practices:

CM Company

Name	Title	Signature	Date

Address	Daytime Phone	After Hours Phone	

Do not write below this line.

This Stormwater Management Plan/Report is:

Approved: _____

Not Approved: _____

Approved, with conditions: _____

By The City of McCall

Representative	Title	Signature	Date
----------------	-------	-----------	------



City of McCall

PUBLIC WORKS

www.mccall.id.us

216 East Park Street
McCall, Idaho 83638

Phone 208-634-5580

Main 208-634-7142

Fax 208-634-4170

November 10, 2021

Michael Schlager
Design West Architects
216 SW 5th Ave,
Meridian, ID 83642

Re: CUP 21-03 – 111 S Samson Trail - Engineering Review #1

Dear Michael,

This letter provides the Public Works Department's comments for CUP 21-03 based on the materials submitted to date as well as the site visit conducted on 11/5/21 with the City, MDSD and the Design West project team.

Transportation and Traffic Analysis:

The Traffic Flow site plan was provided in your submittal and the City also had on file the draft Traffic Flow Analysis (Draft, 6/21/2019 by Paragon Consulting). We incorporated both documents into our review. We understand the draft Analysis explains how the proposed circulation plan (Exhibit 7 2020-2021 School Year Changes Map) was established. Below we have outlined our comments and concerns with the proposed plan:

1. The existing campus is missing key pedestrian and bike routes to get students to and from the public Samson Trail and Deinhard public rights-of-way, particularly for students approaching the campus from the south and west. There is not a proposed multi-modal route connects pedestrians and bikes and brings them efficiently to the schools' entrances without having to walk and bike through vehicular areas or use excessively long routing that would discourage use. Reading through the comments that were provided by parents during pick up and drop off, the students and parents going through the parking lots and drive aisles causes delays, congestion and safety concerns. A pedestrian and bike path that parallels the main entrance driveway (or similar alternative) should be incorporated that provides a more direct connection from the Deinhard and Samson Trail intersection.

Creating a safer route for pedestrians and bikes, will help encourage students to utilize these modes of transportation which may be considered currently unsafe and will ultimately reduce vehicular congestion within the campus.

2. The proposed circulation plan shows a separation of buses from the general drop off/pickup route. It is assumed this will improve circulation. Please provide quantitative information in your traffic flow analysis on how this change improves or provides accommodations for the existing and anticipated higher traffic volumes to the other entrances of the school. Please also address the concerns listed below for the two entrances off Samson Trail, specifically:
 - a. North Samson Trail Entrance – at this location busses entering the campus will be interacting with vehicles entering and exiting the middle school:
 - i. Turning movement conflicts for buses and staged vehicles leaving the campus – will lane widening be needed both on Samson Trail and/or the campus drive lanes?

-
- ii. Due to the convergence of the majority of both vehicle and bus traffic entering/exiting at this intersection, is a southbound left turn lane warranted on Samson Trail? What backups on Samson Trail (southbound) are anticipated with this route change?
 - b. Samson Trail/Deinhard Lane Entrance:
 - i. It is understood that the majority of traffic coming to the campus (during peak morning/evening hours) are utilizing Deinhard Lane. How does this proposed circulation plan increase/decrease the historical traffic volumes at the Samson Trail/Deinhard Lane intersection, especially considering an increase in traffic as the campus's capacity increases?
 - ii. What improvements to Deinhard/Samson Trail intersection are necessary to mitigate the higher peak hour traffic volumes as described in the draft Analysis, especially considering traffic will increase as campus capacity increases.
 3. With respect to #2 above, pick-up and drop-off times are identified as the cause of most of the traffic that occurs in the mornings and afternoons. More information should be collected on the number of vehicles that are queuing at these times and a quantitative analysis of how the proposed circulation lane will mitigate queuing times should be provided.
 4. There does not appear to be an area dedicated for pick-up and drop-offs for the middle school.
 - a. Are pick-ups/drop offs expected to occur within the drive lanes of the parking lot (presumably at the school's main entrance)?
 - b. Will this cause congestion within the parking lot drive lanes and/or impacts to vehicles trying to exit the campus?
 5. Some expansion of the elementary school parking lot for staging is shown, but there is not information and data supporting that this increase in area will be sufficient to alleviate the problems that are being seen. Separating these idle vehicles from areas where traffic needs to flow would help with the congestion in the school internally and the intersection. Please provide information on the number of vehicles that participate in pick-up and drop-offs and where all those vehicles will be staged and located on the campus.
 6. Has the project team discussed having pick-ups/drop offs park in a designated location and walking to pick up their students? This could help mitigate idling/queuing vehicles impacting drive lanes within the campus and resulting in backups on Deinhard and Samson Trail. This could also address safety concerns with requiring students to walk to vehicles unsupervised.
 7. Currently, limiting access to the campus from the two entrances off Samson Trail creates a bottleneck for traffic at Samson Trail/Deinhard intersection during the City's busiest morning traffic (7-9 AM), at the end of the school day, and during major campus activities (athletic events, assemblies, etc.). We understand there are many vehicles delivering students coming to the campus from the north and south, despite a lack of vehicular access from these sides of the campus. Additionally, it is expected that significant future residential development to the east of the campus will occur in the near future (500-1500 units based on current/future zoning).

Providing or planning for an alternative vehicular and multi-modal access to the campus from the east could mitigate inefficient traffic flows from the neighborhoods surrounding the campus (i.e. Woodlands, Fox Ridge, and future development to the east) as well as reduce traffic volumes at the Deinhard/Samson Trail and North Samson Trail entrances. The City was presented (by a potential developer) with a concept of extending Deinhard Lane through the MSDS campus as a potential option to improve access to the campus as well as potential future development east of the campus. Our understanding is this concept has not been endorsed by MDS to date, and you indicated that

backups into the proposed Deinhard Lane as well as space constraints along the east side of the campus were concerns.

- a. Can you please elaborate on the benefits and impacts of extending Deinhard Lane through the campus on circulation and congestion in the vicinity of the campus?
 - b. Does the drop off/pickup parking lot concept in #6 above provide a potential solution?
 - c. Can you provide a recommended alternative to the Deinhard Lane extension that still provides a future easterly campus entrance for those future neighborhoods?
8. The draft traffic flow analysis will need to be revised to address the comments above. We understand that you have completed recent traffic counts during Fall 2021 and that you will be providing this subsequent data and traffic impact analysis.
 9. A signage and striping plan will be required to be submitted. This could be combined as part of other roadway and/or site plans submitted as part of the final civil design package.
 10. Any roadway, driveway or pathway widening or other construction located within the public right-of-way will require civil construction plans designed by a licensed professional engineer to be reviewed and approved by the City Engineer and permitted for construction via a public works permit. We assume these plans will be provided in the final civil plans for the project.

Stormwater and Drainage:

1. A Stormwater management plan for the campus was approved in 2011 with the Payette Lakes Middle School & Saballa Field Site Improvements Project. Please confirm that current stormwater facilities can accommodate the additional runoff generated by the proposed building addition and new impervious areas (parking, drive lanes, sidewalks/pathways, etc.) in accordance with the City's Drainage Management Guidelines. If enhancements/modifications to the current stormwater management facilities are proposed, please include this information in the proposed civil plans set and provide a revision to the 2011 approved Stormwater Drainage Report.

Utilities:

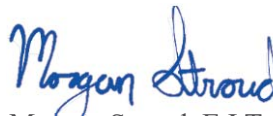
1. Per our 11/5/21 discussions, we understand that the proposed building expansion will not require increasing water meter sizing or any other significant new utilities to serve the expansion.
2. Please provide confirmation from the McCall Fire District of the required fire flow volumes needed to support fire protection for the enlarged Middle School building so we can confirm that required volumes are available at the adjacent City water hydrants.

In summary, we do not have significant concerns regarding the proposed middle school building expansion. However, more information needed regarding the traffic analysis completed, impacts and mitigation measures proposed (particularly future access from the east), and improvements to multimodal facilities along the southern side of the MDSD campus., That said, we believe as the design evolves, the project could comply with City's design standards. Please feel free to contact us if you have any questions or need further clarification as you prepare additional documents and work towards finalizing your construction documents.

Sincerely,

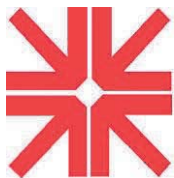


Nathan Stewart, P.E.
Public Works Director, City Engineer



Morgan Stroud, E.I.T
McCall Staff Engineer

Cc: Project file – CUP21-03
Brian Parker – City Planner
Jason Clay – McCall Donnelly School District



DESIGN
WEST

November 17, 2021

Nathan Stewart, Public Works Director
Morgan Stroud, Staff engineer
City of McCall, Public Works Department
216 East Park Street
McCall, ID 83638

Re: CUP 21-03 – 111 S Samson Trail - Engineering Review #1

Dear Nathan & Morgan,

We are in receipt of your November 10, 2021 letter which outlines the Public Works Department's comments for CUP 21-03. Below please find a copy of your comments along with our responses and information presented in *Blue Italics*. Comment responses have been addressed by Joe Barton, Paragon Engineering and Dion Zimmerman, Design West Architects. Both will be available for follow up questions during our CUP hearing on December 7th. Additionally, please reference the November 17, 2021 Traffic Impact Study for this project for additional information and details.

1. The existing campus is missing key pedestrian and bike routes to get students to and from the public Samson Trail and Deinhard public rights-of-way, particularly for students approaching the campus from the south and west. There is not a proposed multi-modal route connects pedestrians and bikes and brings them efficiently to the schools' entrances without having to walk and bike through vehicular areas or use excessively long routing that would discourage use. Reading through the comments that were provided by parents during pick up and drop off, the students and parents going through the parking lots and drive aisles causes delays, congestion and safety concerns. A pedestrian and bike path that parallels the main entrance driveway (or similar alternative) should be incorporated that provides a more direct connection from the Deinhard and Samson Trail intersection.

Creating a safer route for pedestrians and bikes, will help encourage students to utilize these modes of transportation which may be considered currently unsafe and will ultimately reduce vehicular congestion within the campus.

The site plans being developed include modifications to the campus entry road at E Deinhard Lane & S Samson Trail to incorporate bicycle and pedestrian access to the on-campus bike and pedestrian facilities.

Figure 2 of the referenced Traffic Impact Study shows the campus bicycle and pedestrian access plan, which includes the connection to the public pathway at E Deinhard Lane and S Samson Trail. See the referenced Traffic Impact Study for additional bicycle and pedestrian information.

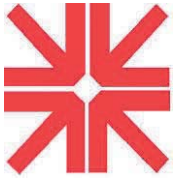
2. The proposed circulation plan shows a separation of buses from the general drop off/pickup route. It is assumed this will improve circulation. Please provide quantitative information in your traffic flow analysis on how this change improves or provides accommodations for the existing and anticipated higher traffic volumes to the other entrances of the school. Please also address the concerns listed below for the two entrances off Samson Trail, specifically:

The July 26, 2019, Payette Lakes Middle School & Barbara Morgan

MERIDIAN, IDAHO
PULLMAN, WASHINGTON
KENNEWICK, WASHINGTON
ONTARIO, OREGON

DESIGN WEST
ARCHITECTS, P.A.

216 SW FIFTH AVENUE
MERIDIAN, IDAHO 83642
TEL. 208-888-1768
www.designwestpa.com



Elementary School Traffic Flow Analysis report did include some suggestions for separating the campus bus traffic from the general traffic. These recommendations were based on bus route and time logistics in that separating the bus traffic reduced the potential for buses to get hung up in the general traffic, therefore throwing off route times. Separating bus traffic from general traffic on the internal campus site is not anticipated to have an impact to traffic conditions on the public roadway intersections.

Approximately twelve school buses entered and exited the school campus in the peak hour traffic counts completed on September 22, 2021, representing a small percentage of total traffic on the campus.

See additional information related to the timing of implementing a change to site circulation and note that modifying the campus north approach to two-way traffic is not included in the current design plans, associated with the Middle School remodel/expansion.

- a. North Samson Trail Entrance – at this location busses entering the campus will be interacting with vehicles entering and exiting the middle school:

Note the current bus and traffic circulation has bus traffic interacting with Middle School exit traffic at the campus north approach. Therefore, modifying the traffic circulation plan would not change the campus exit interaction between buses and general traffic.

Additionally, under the current traffic circulation the buses entering campus are intermixed with all general traffic entering the campus. Under the future modification that would convert the campus north approach to two-way traffic, bus traffic entering the campus would only be interacting with general traffic entering the Middle School.

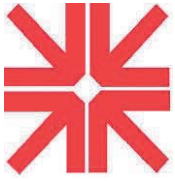
- i. Turning movement conflicts for buses and staged vehicles leaving the campus – will lane widening be needed both on Samson Trail and/or the campus drive lanes?

No future lane widening on the campus north approach or S Samson Trail would be necessary for the exiting bus traffic. This approach already accommodates exiting vehicle and bus traffic.

Upon converting the campus north approach to two-way traffic, the campus approach would be widened to accommodate the additional lane and the entry radius on the campus approach should be constructed to accommodate the bus turning radius. Therefore, no widening on S Samson Trail would be necessary.

- ii. Due to the convergence of the majority of both vehicle and bus traffic entering/exiting at this intersection, is a southbound left turn lane warranted on Samson Trail? What backups on Samson Trail (southbound) are anticipated with this route change?

If the north campus approach is converted to two-way traffic, no turn lanes are necessary on S Samson Trail. The conflicting northbound traffic volume is insufficient to cause traffic backup



for the southbound left movement.
The referenced Traffic Impact Study recommends improvements on S Samson Trail (See Figure 8 and associated text within the Traffic Impact Study) after reaching a campus student enrollment of 850 students. That is an increase of ± 120 students over current enrollment on the campus.

b. Samson Trail/Deinhard Lane Entrance:

- i. It is understood that the majority of traffic coming to the campus (during peak morning/evening hours) are utilizing Deinhard Lane. How does this proposed circulation plan increase/decrease the historical traffic volumes at the Samson Trail/Deinhard Lane intersection, especially considering an increase in traffic as the campus's capacity increases?

The traffic redistribution would reduce the total intersection traffic volume at E Deinhard Lane and S Samson Trail by redistributing the traffic entering the Middle School to the campus north approach, effectively removing all traffic coming from the north and going to the Middle School from the intersection. Redistributing the Middle School entering traffic to the north campus approach would reduce the traffic at the E Deinhard Lane and S Samson Trail intersection by ± 33 vehicles in the AM peak traffic hour, representing about a 3.5% reduction in traffic at the intersection. Similar intersection traffic reduction would also be seen in the afternoon peak hour traffic.

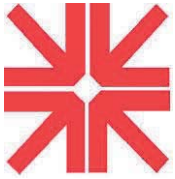
- ii. What improvements to Deinhard/Samson Trail intersection are necessary to mitigate the higher peak hour traffic volumes as described in the draft Analysis, especially considering traffic will increase as campus capacity increases.

The referenced Traffic Impact Study shows that no improvements to the intersection of E Deinhard Lane and S Samson Trail are necessary until the campus student enrollment reaches 850 students. The analysis indicates this holds true for the existing traffic circulation layout and the proposed future modifications to the traffic circulation. See Figure 8 and the associated text within the referenced Traffic Impact Study for recommended improvements at the intersection once campus student enrollment exceeds 850 students.

3. With respect to #2 above, pick-up and drop-off times are identified as the cause of most of the traffic that occurs in the mornings and afternoons. More information should be collected on the number of vehicles that are queueing at these times and a quantitative analysis of how the proposed circulation lane will mitigate queuing times should be provided.

See Section 3.3 of the referenced Traffic Impact Study for information pertaining to vehicle queueing at the campus schools.

4. There does not appear to be an area dedicated for pick-up and drop-offs for the



middle school.

The current plans for construction with the Middle School remodel/expansion include relocating the existing handicap parking and significantly expanding the area for student drop-off and pick-up at the Middle School. See the referenced Traffic Impact Study for additional information.

- a. Are pick-ups/drop offs expected to occur within the drive lanes of the parking lot (presumably at the school's main entrance)?

No, the drop-off and pick-up will be at the curb along the east side of the parking lot where there is sufficient width of the drop-off and pick-up lane, a drive isle and room to access the parking stalls.

- b. Will this cause congestion within the parking lot drive lanes and/or impacts to vehicles trying to exit the campus?

The proposed modifications to the drop-off and pick-up area are expected to significantly increase efficiency and therefor reduce the drop-off and pick-up queue time. This along with the available drive isle widths will reduce parking lot congestion and remove the drop-off and pick-up vehicle conflict with entering and exiting vehicles.

5. Some expansion of the elementary school parking lot for staging is shown, but there is not information and data supporting that this increase in area will be sufficient to alleviate the problems that are being seen. Separating these idle vehicles from areas where traffic needs to flow would help with the congestion in the school internally and the intersection. Please provide information on the number of vehicles that participate in pick-up and drop-offs and where all those vehicles will be staged and located on the campus.

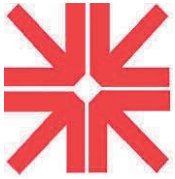
Improvements at the Elementary School are not planned with the site construction associated with the Middle School remodel/expansion. The School District is still in the process of building options for the Elementary School site. It is unlikely that the parking lot concept shown in the July 26, 2019, Payette Lakes Middle School & Barbara Morgan Elementary School Traffic Flow Analysis report will be the final solution that moves to construction.

One concept that is being considered is extending a drop-off and pick-up lane up the south school property boundary and looping in at the traffic circle, therefore separating the drop-off and pick-up traffic from traffic accessing the existing parking stalls.

Vehicle queueing at the Elementary School has no impact to the campus approaches or the public roadway system.

6. Has the project team discussed having pick-ups/drop offs park in a designated location and walking to pick up their students? This could help mitigate idling/queueing vehicles impacting drive lanes within the campus and resulting in backups on Deinhard and Samson Trail. This could also address safety concerns with requiring students to walk to vehicles unsupervised.

As indicated in the referenced Traffic Impact Study, the vehicle queueing on



the internal campus roadways and parking lots do not result in traffic backups on the public roadway system. The left turn into the Middle School parking lot from the Elementary School access road was identified as a location that could cause traffic backup during slick roadway conditions. The proposed future modifications to the campus north approach in combination with restricting left turns at the Middle School parking lot approach eliminates the potential for this condition. Additionally, the upgrades to the Middle School drop-off and pick-up area will increase queueing efficiency at the Middle School, reducing the potential for any queue there to influence through traffic on the Elementary School access road.

Some parents do park at alternate locations around the campus and have students walk to their vehicles, although there is no designated area for this parking. It would require significant on-campus changes to implement a specific park and walk system.

7. Currently, limiting access to the campus from the two entrances off Samson Trail creates a bottleneck for traffic at Samson Trail/Deinhard intersection during the City's busiest morning traffic (7-9 AM), at the end of the school day, and during major campus activities (athletic events, assemblies, etc.). We understand there are many vehicles delivering students coming to the campus from the north and south, despite a lack of vehicular access from these sides of the campus. Additionally, it is expected that significant future residential development to the east of the campus will occur in the near future (500-1500 units based on current/future zoning).

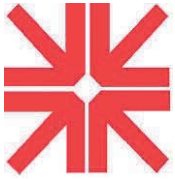
As observed during the September 2021 traffic count and supported by the analysis contained in the referenced Traffic Impact Study, the campus access traffic does not create a "bottleneck" at the E Deinhard Lane and S Samson Trail intersection.

Major campus activities (i.e. football games) occur six to ten times per year. It is fiscally impractical and often physically impossible to design facilities for such rare occasions. Consider all the major event centers around the Boise valley. Facilities around these event centers are in no way designed to accommodate event traffic at a high level of service.

Providing or planning for an alternative vehicular and multi-modal access to the campus from the east could mitigate inefficient traffic flows from the neighborhoods surrounding the campus (i.e. Woodlands, Fox Ridge, and future development to the east) as well as reduce traffic volumes at the Deinhard/Samson Trail and North Samson Trail entrances. The City was presented (by a potential developer) with a concept of extending Deinhard Lane through the MSDS campus as a potential option to improve access to the campus as well as potential future development east of the campus. Our understanding is this concept has not been endorsed by MDSO to date, and you indicated that backups into the proposed Deinhard Lane as well as space constraints along the east side of the campus were concerns.

- a. Can you please elaborate on the benefits and impacts of extending Deinhard Lane through the campus on circulation and congestion in the vicinity of the campus?

See Section 4.2 of the referenced Traffic Impact Study



- b. Does the drop off/pickup parking lot concept in #6 above provide a potential solution?

See Section 4.2 of the referenced Traffic Impact Study and the response to Comment 5 above.

- c. Can you provide a recommended alternative to the Deinhard Lane extension that still provides a future easterly campus entrance for those future neighborhoods?

See Section 4.2 of the referenced Traffic Impact Study

8. The draft traffic flow analysis will need to be revised to address the comments above. We understand that you have completed recent traffic counts during Fall 2021 and that you will be providing this subsequent data and traffic impact analysis.

The Traffic Impact Study for this project is being submitted with this letter.

9. A signage and striping plan will be required to be submitted. This could be combined as part of other roadway and/or site plans submitted as part of the final civil design package.

Signing and pavement marking plans for the campus site work will be prepared and submitted. The site work associated with the Middle School remodel/expansion does not include any construction within the public right-of-way.

10. Any roadway, driveway or pathway widening or other construction located within the public right- of-way will require civil construction plans designed by a licensed professional engineer to be reviewed and approved by the City Engineer and permitted for construction via a public works permit. We assume these plans will be provided in the final civil plans for the project.

Site civil plans for the campus site work associated with the Middle School remodel/expansion are being design by and will be stamped by a licensed professional engineer. The site work associated with the Middle School remodel/expansion does not include any construction within the public right-of-way.

Stormwater and Drainage:

1. A Stormwater management plan for the campus was approved in 2011 with the Payette Lakes Middle School & Saballa Field Site Improvements Project. Please confirm that current stormwater facilities can accommodate the additional runoff generated by the proposed building addition and new impervious areas (parking, drive lanes, sidewalks/pathways, etc.) in accordance with the City's Drainage Management Guidelines. If enhancements/modifications to the current stormwater management facilities are proposed, please include this information in the proposed civil plans set and provide a revision to the 2011 approved Stormwater Drainage Report.

The Middle School remodel/expansion and associated site work will result in a decrease in total hard surface area and will not change the campus drainage patterns. Therefore, no update to the campus stormwater plan is necessary.



Utilities:

1. Per our 11/5/21 discussions, we understand that the proposed building expansion will not require increasing water meter sizing or any other significant new utilities to serve the expansion.

Correct, the only utility changes anticipated include relocating a fire hydrant and adjusting water valve cans to grade.

2. Please provide confirmation from the McCall Fire District of the required fire flow volumes needed to support fire protection for the enlarged Middle School building so we can confirm that required volumes are available at the adjacent City water hydrants.

The current building square footage is 55,056 square ft. The new additions bring the total gross square footage to 64,723 square ft. The international fire code table B105.1 require a fire flow of 4500 gpm for a duration of 4 hours. By using the allowable reduction listed in table B105.2 of .25% of the required, the actual gallons per minute fire flow is 1125 gpm. However, code minimum does not permit less than 1500 gpm, therefore, the required fire flow for Payette Lakes Middle School should be calculated at 1500 gallon per minute.

Please let us know if you have any questions or need additional information relative to our above responses.

Sincerely,

Michael Schlager, Project Manager

Cc: Jason Clay – McCall Donnelly School District

PLACEHOLDER FOR ENGINEERING REVIEW #2

From: [Garrett de Jong](#)
To: [Brian Parker](#)
Subject: RE: City of McCall Request for Comment - CUP-21-03 - 111 South Samson Trail
Date: Friday, October 8, 2021 10:12:06 AM
Attachments: [image004.png](#)
[image005.png](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Brian,

Fire sprinkler and alarm system plans will need to be submitted to the Idaho State Fire Marshal's Office for plan review.

The traffic plan, including any gates, will need to be approved by the fire district.

Thank you,

Garrett

Garrett de Jong
Fire Chief
McCall Fire & EMS
201 Deinhard Lane
McCall, ID 83638
208-634-7070



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This message has been sent to you as official business of the McCall Fire Protection District. If you have a concern about the authenticity of this communication, including any attachments, please contact the sender directly for confirmation, either by telephone or separate e-mail. Unencrypted e-mail is inherently insecure and should be treated with caution.

Electronic Privacy Notice. This e-mail, and any attachments, contains information that is, or may be, covered by the Electronic Communications Privacy Act, 18 U.S.C. 2510-2521, and is also confidential and proprietary in nature. If you are not the intended recipient, please be advised that you are legally prohibited from retaining, using, copying, distributing, or otherwise disclosing this information in any manner. Instead, please reply to the sender that you have received this communication in error, and then immediately delete it. Thank you in advance for your cooperation.

From: Brian Parker <barker@mccall.id.us>

Sent: Monday, October 4, 2021 12:09 PM

To: Bob Fodrea <bob@fodrealandgroup.com>; Chip Bowers <chip@bowerslandssurveys.com>; Chris Curtin <ccurtin@mccall.id.us>; Cynda Herrick <cherrick@co.valley.id.us>; Dale Caza <dcaza@plrwsd.org>; David Simmonds <dsimmonds50@gmail.com>; Delta James <djames@mccall.id.us>; Diane Evans Mack - Fish and Game <diane.evansmack@idfg.idaho.gov>; Garrett de Jong <garrett@mccallfire.com>; Gene Trantham <gtrantham@mccall.id.us>; IDL Jurisdictional Inbox <IDL_jurisdictional@idl.idaho.gov>; ITD Development Services <D3Development.Services@itd.idaho.gov>; ITD District 3 Permits <ITDD3Permits@itd.idaho.gov>; Jasen King, IDL <jking@idl.idaho.gov>; Jeff Bateman <jbateman@plrwsd.org>; John Powell <jpowell@mccall.id.us>; jthrower@gmail.com; Kurt Wolf <kwolf@mccall.id.us>; Lance Holloway, DEQ <lance.holloway@deq.idaho.gov>; Laura Shealy BPLWQAC <idchik5@gmail.com>; Laurie Frederick, Valley Co Cartographer <lfr frederick@co.valley.id.us>; Levi Brinkley <lbrinkley@mccall.id.us>; Linda Stokes <lstokes@mccall.id.us>; Lori Hunter (lhunter@co.valley.id.us) <lhunter@co.valley.id.us>; Mark Wasdahl, ITD <mark.wasdahl@itd.idaho.gov>; Meredith Todd <mtodd@mccall.id.us>; Michelle Groenevelt <mgroenevelt@mccall.id.us>; Mike Reno <mreno@cdh.idaho.gov>; Morgan Stroud <mstroud@mccall.id.us>; Nathan Stewart <nstewart@mccall.id.us>; Richard Stein <rstein@mccall.id.us>; Sabrina Sims <ssims@mccall.id.us>; Sarah Arjona <Sarah.Arjona@itd.idaho.gov>; Scott Corkill, IDL <scorkill@idl.idaho.gov>; Sheri Staley - Idaho Power <sstaley@idahopower.com>; Steve Moser, Idaho Power <smoser@idahopower.com>; Valley County Road Dept <roaddept@co.valley.id.us>

Subject: City of McCall Request for Comment - CUP-21-03 - 111 South Samson Trail

All,

Please provide comment on the attached application by October 18th.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
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Keeping citizens informed.

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Meredith Todd

From: Brian Parker
Sent: Tuesday, November 30, 2021 4:10 PM
To: Meredith Todd
Subject: FW: CUP School Expansion and Extension of Deinhard Ln.

For the packet.

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038

Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)

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-----Original Message-----

From: Tom Kilgore <tkilgore@frontiernet.net>
Sent: Tuesday, November 30, 2021 4:09 PM
To: Brian Parker <bparker@mccall.id.us>
Subject: CUP School Expansion and Extension of Deinhard Ln.

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Sir,

As residents of The Woodlands, we are concerned about any future development to the northeast and south of our neighborhood, in particular, ingress and egress roads for access to any new development. As you know, the developer of Pine Creek Ranch is proposing over 500 units to the northeast and south of the Woodlands. It has come to our attention, the McCall School District plans to expand portions of the Middle School and Elementary School and reconfigure Deinhard Ln. to the east and south. It is our understanding, the original CUP to build the schools included a right of way to extend Deinhard Ln. should there be any future development to the east.

That future development is now being proposed by Pine Creek Ranch.

It is imperative the Planning and Zoning Commission and the City Council honor the 1994 CUP and allow the right of way so Deinhard Ln. can be extended into any new development.

Without the extension of Deinhard and other ingress/egress roads, Woodland Dr. and other current roads in the Woodlands would bear the brunt of increased traffic due to Pine Creek Ranch. The way Pine Creek Ranch is currently configured, virtually all new traffic would use Woodlands drive in addition to the current residents of The Woodlands. The extension of Deinhard Ln. would relieve, at least, a little of that traffic. Also, it would provide an additional emergency ingress and egress in case of a wild land fire or other incident.

We respectfully request, the 1994 CUP right of way be used for the extension of Deinhard Ln. should the school expansion plans be approved.

Sincerely,

Tom & Kathy Kilgore

674 Koski Dr.

--

This email has been checked for viruses by AVG.

<https://gcc02.safelinks.protection.outlook.com/?url=https%3A%2F%2Fwww.avg.com%2F&data=04%7C01%7Cmto dd%40mccall.id.us%7C0fb59af4e1174f0e206008d9b4568468%7C3fcc645557cd446686b48d7a5b85d741%7C0%7C0%7C 637739105955980390%7CUnknown%7CTWFpbGZsb3d8eyJWlloiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJ XVCI6Mn0%3D%7C3000&data=sSGVyT3YWMw0%2Fmw7lGe6i3x8e382DQUuFVC4jnlqTkM%3D&reserved=0>

From: [Brian Parker](#)
To: [Christina Nemeć](#); [Liz Rock](#); robertlyons@highmountaincountry.com; [Ryan Kinzer](#); jscott.tunnell@gmail.com; [Tony Moss](#)
Cc: [Meredith Todd](#)
Subject: FW: MDSĐ expansion comment
Date: Wednesday, December 1, 2021 8:56:30 AM
Attachments: [image001.png](#)
[image002.png](#)

Commissioners,

Please see the comment below regarding CUP-21-03 located at 111 North Samson Trail.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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From: DEBRA SCHWARK <wdschwark@comcast.net>
Sent: Tuesday, November 30, 2021 8:39 PM
To: Brian Parker <bparker@mccall.id.us>
Subject: MDSĐ expansion comment

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

The Schools agreed in the 1994 CUP to grant a ROW to extend Deinhard in the event there is development to the East. We believe this obligation should be repeated in the current CUP and that the ROW be required to be dedicated to the City and recorded as was originally contemplated.

The traffic flow situation around the schools must be addressed by providing a major thoroughfare artery that can accommodate both residential (from Pine Creek Ranch subdivision to the east/south) and school volume traffic (both daily and special event)...currently, parents routinely drop students off in the parking lot of the

Woodlands Pool & tennis court areas in order to avoid the congestion near the school...this naturally creates congestion and safety issues on Woodlands Dr.

Woodlands Dr. should in no way be used as a connector to the Pine Creek Ranch subdivision.. it is far too narrow, steep and curvy to accomodate any additional traffic(in summer or winter)...and as the sole ingress/egress for The Woodlands subdivision it already exceeds the the recommended units/access points for emergency vehicle access and wildfire evacuation....

thanks

Wade & Debra Schwark
600 Woodlands Dr

McCall Area Planning and Zoning Commission

Staff Report

CUP-21-04, DR-21-38

Cloud 9 Events Center

201 Rio Vista Boulevard

December 7, 2021

Applicant: James and Susan Evans
Representative: None
Application: Conditional Use Permit and Design Review
Zoning District: R1 - Residential
Jurisdictional Area: City Limits

Description

An application for a Conditional Use Permit and Design Review to expand the use of the Bed & Breakfast/Short Term Rental and the property to be an Event/Activities Venue. The Conditional Use Permit and Design Review applications propose to do a high level of landscaping on the property as part of the Event Venue, and expand the size of the Carport, attached to the main building, from 1 car to 3 cars for a total building square footage of approximately 5,600 square feet.

Staff Analysis

Project Information

Zoning District: R1 - Residential

Comprehensive Plan Designation: Low Density Residential

Project Acreage: 4.86-acres

Proposed Use: Expansion of existing bed and breakfast facility an events venue.

Dimensional Standards:

	Proposed	Required
Front Setback	43.5-feet	30-feet
Rear Setback	368-feet	20-feet
West Side Yard Setback	137-feet	15-feet
East Side Yard Setback	17-feet	15-feet
Lot Coverage	14,410 square-feet (67% of allowed)	21,170 square-feet (10% of total)
Area to be Plowed	21,571 square-feet	N/A
Snow Storage Area	8,828 square-feet (41% of area to be plowed)	6,473 square-feet (33% of area to be plowed)

Code Sections of Interest

- McCall City Code Section 3.2.02: Meanings of Terms or Words:
 BED AND BREAKFAST: See definition of Dwelling, Rooming House.

 DWELLING, ROOMING HOUSE (BOARDING HOUSE, DORMITORY, BED AND BREAKFAST): A dwelling or part thereof, other than a hotel, motel or restaurant, where lodging and, occasionally, meals are provided for compensation for three (3) or more unrelated persons, where no cooking or dining facilities are provided in the individual rooms.
- McCall City Code Section 3.8.063: Bicycle Parking:
 Table 3.8.063 does not identify event venues or similar use, thus the minimum number of short and long-term bicycle parking spaces is “determined through conditional use and design review.”
- McCall City Code Section 3.13.03(B): Findings for Granting Permit:
 A conditional use permit shall be granted only if the commission finds that the use, as applied for, in fact will:
 1. Constitute a conditional use authorized in the zone involved.
 2. Be harmonious with and in accord with the general objectives and with any specific objectives of the comprehensive plan and/or this title.
 3. Be designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or likely character of the neighborhood, and that such use will not change the essential character of the surrounding area.
 4. Not be detrimental to the health, safety and general welfare of persons residing or working in the neighborhood of such proposed use.

5. Not cause any substantially harmful environmental consequences to any land or waters within the planning jurisdiction.

6. Not create excessive additional public cost for public facilities and services, and will not be detrimental to the economic welfare of the community.

7. Be served adequately by essential public facilities and services including highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools. The applicant may be required, as a condition of approval, to mitigate any deficient public service.

8. Not involve uses, activities, processes, materials, equipment or conditions of operation that will cause unreasonable production of traffic, noise, smoke, fumes, glare, odors or other forms of pollution.

9. Have vehicular approaches to the property so designed as not to create a detrimental interference with traffic on surrounding public or private thoroughfares, or adversely affect the pedestrian environment.

10. Not result in the destruction, loss or damage of an important natural, scenic or historic feature.

11. Be on a site of sufficient size to accommodate the proposed use, including the yards, open spaces, snow storage, walls, fences, parking areas, loading zones and design standards applicable.

12. Have a minimal negative economic impact on the neighborhood or surrounding community.

- McCall City Code Section 3.13.09: Permit Standards for Dwelling, Short-Term Rentals:

(A) All short-term rentals shall comply with this section and file with the Administrator, a signed declaration affirming that the property will be managed to adhere to the following requirements:

1. Parking: all parking for the unit is contained on the site and not more than one (1) parking space per bedroom is provided. All trailers shall be parked on a surfaced area, if space is provided, and shall not park on the right-of way.

2. Occupancy: Short-term rentals shall contain no more than four (4) people per bedroom. Total maximum occupancy of the short-term rental shall be based on the number of bedrooms times four (4).

3. Noise: Quiet hours from 10 pm to 8 am the following day are enforced.

4. Safety: Smoke, propane gas, and carbon monoxide detectors are installed within the premises.

5. A posting of the requirements set forth in 1-3 are posted in a visible spot on the premises and a copy provided to all renters.

6. Contact Information: The name and contact information for the property manager is identified and has been sent to all property owners within 300 feet of the location of the short-term rental.

7. No events are held that include additional guests beyond those staying at the short-term rental without first obtaining a conditional use permit.

8. If the short-term rental is located on a parcel that contains an accessory dwelling unit and a primary residence, then one of the dwellings must be owner occupied or a local housing, non-short-term rental, dwelling unit.

9. Camping shall comply with conditions set forth in 3.8.01 and shall not be used to exceed the number of tenants specified on the short-term rental business license.

10. Trash Service: Bear proof solid waste collection facilities shall be available on the site and adequate for the occupancy of the short-term dwelling unit.

(B) Short-term rentals shall comply with the provisions set forth in this section by March 1, 2020.

(C) No short-term rentals within the City of McCall shall operate without obtaining a business license. Short-term rentals with a local property manager who maintains a physical office in the City of McCall or Impact Area may operate under the business license held by that local property manager.

(D) Enforcement of the requirements set forth in this section shall follow the provisions pursuant to Chapter 17 ENFORCEMENT of this title.

- McCall City Code Section 3.16.07: Design Review Criteria:

The commission or administrator shall determine the following before approval is given:

(A) The project is in general conformance with the comprehensive plan.

(B) The project does not jeopardize the health, safety or welfare of the public.

(C) The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance as enumerated in section 3.16.01 of this chapter.

(D) The project will have no substantial impact on adjacent properties or on the community at large.

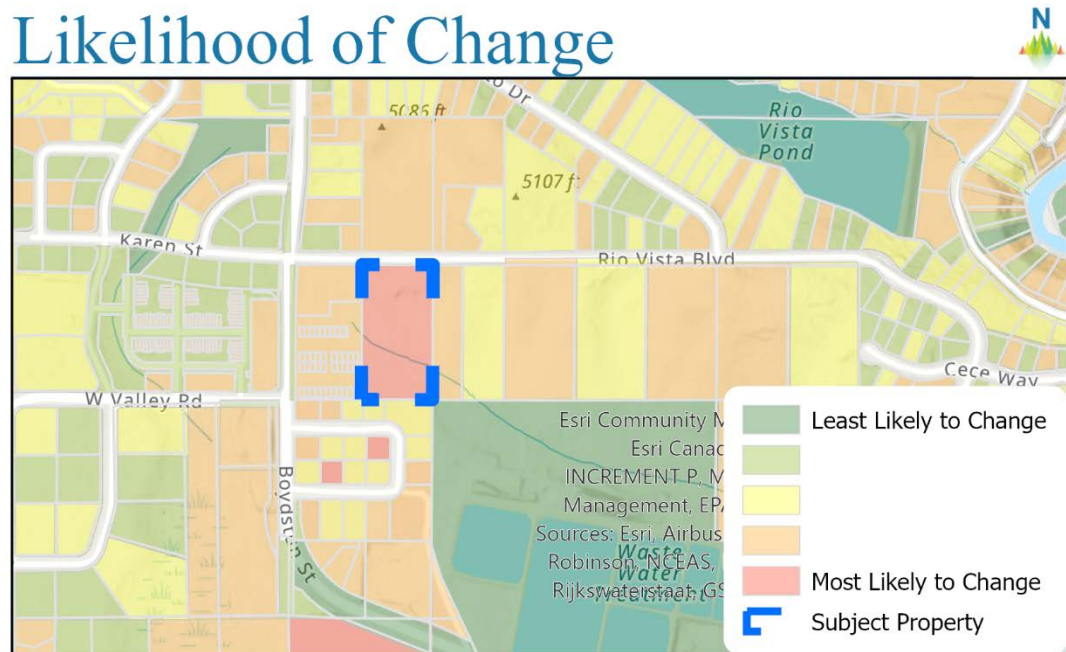
(E) If applicable, a subdivision design review document has been reviewed and approved by the commission in lieu of the design guidelines.

(F) For projects in the shoreline and river environs zone, the project will not have an unreasonable and adverse impact on the visual quality of its setting or the water quality.

(G) For projects in the scenic route zone, the project will preserve and enhance the scenic quality of the street or highway.

Comprehensive Plan Sections of Interest

- Vision in Motion – Our Vision:
McCall is a diverse, small town united to maintain a safe, clean, healthy, and attractive environment. It is a friendly, progressive community that is affordable and sustainable.
- Deep Dive – Future Land Use Designations – Low Density Residential:
This land use designation is intended to provide for the development of low-density single-family residential neighborhoods. These neighborhoods consist of larger home sites.
- Deep Dive – Map 5.6 – Likelihood of Change:



Staff Discussion

- The applicant is proposing to expand an existing carport from one (1) car to three cars (3), bringing the total structure size up to approximately 5,600 square-feet. The applicant is also proposing to utilize the 4.86-acre site as an outdoor events venue. The applicant is requesting a maximum event occupancy of 110 guests.
- Event venues are not included in the Table of Permitted and Conditionally Permitted Uses within Residential Zones included within McCall City Code Section 3.3.02. As a result, a conditional use permit is required pursuant to McCall City Code Section 3.3.02(A).
- The applicant is proposing to provide thirty (30) on-site parking spaces. McCall City Code Section 3.8.062 does not identify the minimum amount of required parking for event venues. The most similar land use in Table 3.8.062 is “Auditoriums, sports arenas, theaters and similar uses” which require one (1) parking space per five (5) seats. McCall City Code Section 3.13.09(A)(1) establishes a maximum of one (1) parking space per bedroom for short-term rentals. By this standard the applicant is providing adequate parking for the proposed uses as a 110-person event venue would require 22 parking spaces and an eight (8) bedroom short term rental would

require eight (8) parking spaces, for a total of thirty (30) parking spaces. Additionally, the applicant is proposing to construct a bus stop on the property along Rio Vista Boulevard, and to provide a shuttle service for events that conclude after the end of public bus service to the site.

- The proposed landscape plan, date stamped by the City on October 26, 2021, does not identify any bicycle parking located on site. As event venues are not included in the table of Minimum Required Bicycle Parking Spaces within McCall City Code Section 3.8.063, the appropriate number of short and long term bicycle parking spaces is to be determined through the conditional use permitting and design review process.

As the applicant is proposing bicycle use as a component of their parking demand management strategy, and the City of McCall's Comprehensive Plan and Transportation Master Plan encourage bicycle use, the bicycle parking should be ample. Prior to the issuance of a building permit, the applicant should provide a revised landscape plan identifying a minimum of twelve (12) short-term and four (4) long-term bicycle parking spaces. The required bicycle parking should be in conformance with McCall City Code Section 3.8.063(A)-(F) and the Association of Pedestrian and Bicycle Professionals' *Essentials of Bike Parking*.

- The proposed landscape plan, date stamped by the City on October 26, 2021, does not identify species of plants proposed. While the landscaping appears to be adequate, prior to the issuance of a building permit, the applicant should provide a revised landscape plan which includes a schedule of plant species to be utilized. The revised landscape plan should be reviewed and approved by staff.

Comments

Agency –

McCall Public Works

See attached letter dated September 23, 2021.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

See attached letter dated November 16, 2021.

Central District Health (CDH)

In an email dated November 4, 2021, CDH requested clarification regarding water and sewer service on the subject property. Upon receipt of a clarifying email from the applicant, CDH had no objections to the application

McCall Fire & EMS

In an email dated November 5, 2021, the Fire Chief had the following comments:

The building at 201 Rio Vista Blvd was likely built under the residential code. Idaho's adopted fire code correlates with the Building Code, in which commercial buildings are constructed. The fire code and building code do not apply to one- and two-family dwellings built under the residential code, which this home originally was. The city building official determines occupancy classification at the time of construction and can change it if the use of the building changes.

The fire code defines occupancies containing sleeping units where the occupants are primarily transient in nature as being classified as R-1, which includes boarding houses with more than ten occupants, congregate living facilities with more than ten occupants, hotels, and motels. A

traditional home, where the occupants are primarily permanent in nature, is classified as R-3. The main difference between the two classifications regarding fire code is that R-1 occupancies require automatic fire sprinkler systems, but R-3 does not. In my opinion, based on the 2018 fire code, any building that sleeps over ten occupants, transient in nature, should have an automatic fire sprinkler system installed.

Because of the use classification, my comments are recommendations to the City of McCall Planning and Zoning and the City of McCall Building Official based on applying the 2018 International Fire Code to the intended use of the building, R-1. They are recommendations as the fire district does not have the authority to inspect or enforce fire code on one-and-two family dwellings.

Recommendations:

- An automatic sprinkler system installed in accordance with Section 903.3 shall be provided throughout the building. (IFC2018 903.2.8 Group R). The building fire alarm system shall monitor the automatic fire extinguishing system in accordance with NFPA 72 (IFC2018 904.3.5).
- Per (IFC2018 [A] 102.3 Change of use or occupancy), a building code analysis should be done to bring the structure up to building code standards, as determined by the city building official, including smoke detectors and carbon monoxide detectors installed following current building code and for the intended use.
- A parking plan for guest vehicles should be developed to make the driveway accessible for emergency vehicles (IFC2018 403.12.2).

McCall Building Department

In an email dated November 18, 2021, the Building Official had the following comments:

As a follow-up to Garrett's email, I agree with him. Here is a summary of how the project relates to the building code requirements:

1. Bed & Breakfast – Currently approved for 19 occupants. If the applicant wishes to increase the number of occupants the occupancy type will change to R-1 and fire sprinklers will be required, along with other potential upgrades.
2. Event Center – An event center is a different occupancy type. It would be classified as an Assembly Occupancy. This would also activate the requirement for fire sprinklers and fire walls, etc. If the applicant wishes to use the grounds as an event center, the Bed & Breakfast structure may not be used as part of the Event. (Unless a code study and code upgrades are made to comply with an A-3 Occupancy.) Additionally, should the kitchen be used for Events, the kitchen would need to be upgraded to a commercial kitchen.

McCall City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I would like to express our concern regarding the noxious weed problems this property has and encourage planning and zoning to make it a condition of approval that these issues be addressed. It would be our recommendation that they have a mitigation plan to control and manage this issue both during construction and into the future.

Public –

Letter received on November 30, 2021, from Kent Graefe and Tammy L. Buck expressing concern.

Staff Recommendation

Staff recommends approval of this application with the Conditions of Approval listed below.

Conditions of Approval

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. Prior to the issuance of a building permit, the applicant shall provide a revised site plan identifying a minimum of twelve (12) short-term and four (4) long-term bicycle parking spaces. The required bicycle parking shall be in conformance with McCall City Code Section 3.8.063(A)-(F) and the Association of Pedestrian and Bicycle Professionals' *Essentials of Bike Parking*.
3. Prior to the issuance of a building permit, the applicant shall provide a letter of acceptance from Treasure Valley Transit regarding the location and design of the bus stop and the pathway from the parking area to the bus stop.
4. Prior to the issuance of a building permit, the applicant shall provide elevations of the proposed bus stop and trash bins.
5. Prior to the issuance of a building permit, the applicant shall provide a revised landscape plan which includes a schedule of plant species to be utilized. The revised landscape plan shall be reviewed and approved by staff.
6. Prior to the issuance of a building permit, the applicant shall submit a noxious weed control plan to be reviewed and approved by the City Arborist.
7. Prior to the issuance of a building permit, the applicant shall provide an outdoor lighting plan in conformance with McCall City Code Title III, Chapter 14.
8. Prior to the issuance of a certificate of occupancy, the applicant shall reseed all disturbed areas with native grasses or wildflowers.
9. Quiet hours shall be observed between 10:00 P.M. and 8:00 A.M.
10. The maximum occupancy of the bed and breakfast/short term rental is nineteen (19) individuals.
11. The maximum event occupancy for the property is 110 individuals.
12. No parking on Rio Vista Boulevard is permitted.
13. No indoor assemblies are permitted.
14. Adequate temporary restroom facilities shall be provided for all outdoor events.

15. Pursuant to McCall City Code Section 3.13.034(B), conditional use permit approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.
16. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

IN RE:)
)
CLOUD 9 EVENTS CENTER)
)
Conditional Use Permit &)
Design Review)
)
Application Number:)
CUP-21-04, DR-21-38)

**McCALL AREA PLANNING AND ZONING COMMISSION
FINDINGS OF FACT, CONCLUSIONS OF LAW, AND
DECISION**

FINDINGS OF FACTS

Applicant: James and Susan Evans

Representative(s): None

Application: An application for a Conditional Use Permit and Design Review to expand the use of the Bed & Breakfast/Short Term Rental and the property to be an Event/Activities Venue. The Conditional Use Permit and Design Review applications propose to do a high level of landscaping on the property as part of the Event Venue, and expand the size of the Carport, attached to the main building, from 1 car to 3 cars for a total building square footage of approximately 5,600 square feet.

Location: Situated in the E1/2 of the SW1/4 of the NW1/4 of the NW1/4 of Section 17, T18N, R3E, B.M., City of McCall, Valley County, Idaho.

Property Address: 201 Rio Vista Boulevard, McCall

Public Notices: Newspaper: The Notice of Hearing was published in the *Star News* on November 18, 2021
Mailing: The Notice of Hearing was mailed by the applicant to property owners within 300 feet on November 19, 2021.
Posting: The Notice of Hearing was posted by the applicant on the subject property on November 19, 2021.

Procedural History: A neighborhood meeting was held on October 17, 2021. A preliminary development plan review was conducted by the Planning and Zoning Commission on October 5, 2021. A public hearing before the Planning and Zoning Commission was held on December 7, 2021.

Zoning: R1 - Residential

Property Size: 4.86-acres

Lot Coverage: 14,410 square-feet (67% of allowed)

Proposed/Existing Setbacks: The required setbacks within the CV - Civic zone are as follows:

	Proposed	Required
Front Setback	43.5-feet	30-feet
Rear Setback	368-feet	20-feet
West Side Yard Setback	137-feet	15-feet
East Side Yard Setback	17-feet	15-feet

Parking Spaces: The applicant is proposing to provide thirty (30) on-site parking spaces. McCall City Code Section 3.8.062 does not identify the minimum amount of required parking for event venues. The most similar land use in Table 3.8.062 is “Auditoriums, sports arenas, theaters and similar uses” which require one (1) parking space per five (5) seats. McCall City Code Section 3.13.09(A)(1) establishes a maximum of one (1) parking space per bedroom for short-term rentals. By this standard the applicant is providing adequate parking for the proposed uses as a 110-person event venue would require 22 parking spaces and an eight (8) bedroom short term rental would require eight (8) parking spaces, for a

total of thirty (30) parking spaces. Additionally, the applicant is proposing to construct a bus stop on the property along Rio Vista Boulevard, and to provide a shuttle service for events that conclude after the end of public bus service to the site.

APPROVAL STANDARDS

Title 3, Chapter 13

Conditional Use Permit

A Conditional Use permit shall be granted only if the Commission finds that the use, as applied for, will:

1. Constitute a conditional use authorized in the zone involved.

Event venues are not included in the Table of Permitted and Conditionally Permitted Uses within Public Zones included within McCall City Code Section 3.3.02. As a result, a conditional use permit is required pursuant to McCall City Code Section 3.3.02(A).

2. Be harmonious with and in accord with the general objectives and with any specific objectives of the comprehensive plan and/or this title.

The proposed use is in accord with the general objectives of McCall City Code Title III and the McCall Comprehensive Plan. Conditions of approval may be placed on the development to ensure compatibility with the specific objectives of the comprehensive plan and McCall City Code Title III.

3. Be designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or likely character of the neighborhood, and that such use will not change the essential character of the surrounding area.

The proposed development will be appropriately designed, constructed, operated, and maintained.

4. Not be detrimental to the health, safety and general welfare of persons residing or working in the neighborhood of such proposed use.

The proposed development is not anticipated to have significant health, safety, or general welfare impacts on the existing neighborhood.

5. Not cause any substantially harmful environmental consequences to any land or waters within the planning jurisdiction.

The proposed development is not anticipated to cause undue harm to any land or waters within the planning jurisdiction.

6. Not create excessive additional public cost for public facilities and services, and will not be detrimental to the economic welfare of the community.

The proposed events center is not anticipated to generate excessive additional public costs or be detrimental to the economic welfare of the community.

7. Be served adequately by essential public facilities and services including highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools. The applicant may be required, as a condition of approval, to mitigate any deficient public service.

The proposed development is located adjacent to a public transit route and is adequately served by public facilities.

8. Not involve uses, activities, processes, materials, equipment, or conditions of operation that will cause unreasonable production of traffic, noise, smoke, fumes, glare, odors or other forms of pollution.

No use involving smoke, fumes, glare, or odors is proposed. The applicant is required to observe quiet hours from 10:00 P.M. to 8:00 A.M. to prevent unreasonable noise.

- 9. Have vehicular approaches to the property so designed as not to create a detrimental interference with traffic on surrounding public or private thoroughfares, or adversely affect the pedestrian environment.**

The proposed vehicular approaches are unlikely to create a detrimental interference on traffic or the quality of the pedestrian environment as they are adequately spaced from existing approaches.

- 10. Not result in the destruction, loss or damage of an important natural, scenic or historic feature.**

No impacts on natural, scenic, or historic features are anticipated to occur from this development.

- 11. Be on a site of sufficient size to accommodate the proposed use, including the yards, open spaces, snow storage, walls, fences, parking areas, loading zones and design standards applicable.**

Adequate space exists on the property to accommodate the proposed use.

- 12. Have a minimal negative economic impact on the neighborhood or surrounding community.**

No substantial negative impact on the neighborhood or surrounding community is anticipated.

Title 3, Chapter 16.07

Design Review Criteria

The commission shall determine the following before approval is given:

- (A) The project is in general conformance with the comprehensive plan.**

The project is in general conformance with the McCall Comprehensive Plan. Conditions of approval may be placed on the development to ensure compatibility with the specific objectives of the comprehensive plan.

- (B) The project does not jeopardize the health, safety or welfare of the public.**

The proposed development is not anticipated to have significant health, safety, or general welfare impacts on the existing neighborhood.

- (C) The project conforms to the applicable requirements of the zoning ordinance and subdivision ordinance as enumerated in section [3.16.01](#) of this chapter.**

The proposed use is in accord with the general objectives of McCall City Code Title III and the McCall Comprehensive Plan. Conditions of approval may be placed on the development to ensure compatibility with the specific objectives of the comprehensive plan and McCall City Code Title III.

- (D) The project will have no substantial impact on adjacent properties or on the community at large.**

No substantial negative impact on adjacent properties or the community at large is anticipated with this development.

- (E) If applicable, a subdivision design review document has been reviewed and approved by the commission in lieu of the design guidelines.**

N/A

- (F) For projects in the shoreline and river environs zone, the project will not have an unreasonable and adverse impact on the visual quality of its setting or the water quality.**

N/A

- (G) For projects in the scenic route zone, the project will preserve and enhance the scenic quality of the street or highway.**

N/A

DEPARTMENT/AGENCY/PUBLIC COMMENTS

Agency –

McCall Public Works

See attached letter dated September 23, 2021.

Payette Lakes Recreational Water and Sewer District (PLRWSD)

See attached letter dated November 16, 2021.

Central District Health (CDH)

In an email dated November 4, 2021, CDH requested clarification regarding water and sewer service on the subject property. Upon receipt of a clarifying email from the applicant, CDH had no objections to the application

McCall Fire & EMS

In an email dated November 5, 2021, the Fire Chief had the following comments:

The building at 201 Rio Vista Blvd was likely built under the residential code. Idaho's adopted fire code correlates with the Building Code, in which commercial buildings are constructed. The fire code and building code do not apply to one- and two-family dwellings built under the residential code, which this home originally was. The city building official determines occupancy classification at the time of construction and can change it if the use of the building changes.

The fire code defines occupancies containing sleeping units where the occupants are primarily transient in nature as being classified as R-1, which includes boarding houses with more than ten occupants, congregate living facilities with more than ten occupants, hotels, and motels. A traditional home, where the occupants are primarily permanent in nature, is classified as R-3. The main difference between the two classifications regarding fire code is that R-1 occupancies require automatic fire sprinkler systems, but R-3 does not. In my opinion, based on the 2018 fire code, any

building that sleeps over ten occupants, transient in nature, should have an automatic fire sprinkler system installed.

Because of the use classification, my comments are recommendations to the City of McCall Planning and Zoning and the City of McCall Building Official based on applying the 2018 International Fire Code to the intended use of the building, R-1. They are recommendations as the fire district does not have the authority to inspect or enforce fire code on one-and-two family dwellings.

Recommendations:

- An automatic sprinkler system installed in accordance with Section 903.3 shall be provided throughout the building. (IFC2018 903.2.8 Group R). The building fire alarm system shall monitor the automatic fire extinguishing system in accordance with NFPA 72 (IFC2018 904.3.5).
- Per (IFC2018 [A] 102.3 Change of use or occupancy), a building code analysis should be done to bring the structure up to building code standards, as determined by the city building official, including smoke detectors and carbon monoxide detectors installed following current building code and for the intended use.
- A parking plan for guest vehicles should be developed to make the driveway accessible for emergency vehicles (IFC2018 403.12.2).

McCall Building Department

In an email dated November 18, 2021, the Building Official had the following comments:

As a follow-up to Garrett's email, I agree with him. Here is a summary of how the project relates to the building code requirements:

1. Bed & Breakfast – Currently approved for 19 occupants. If the applicant wishes to increase the number of occupants the occupancy type will change to R-1 and fire sprinklers will be required, along with other potential upgrades.
2. Event Center – An event center is a different occupancy type. It would be classified as an Assembly Occupancy. This would also activate the requirement for fire sprinklers and fire walls, etc. If the applicant wishes to use the grounds as an event center, the Bed & Breakfast structure may not be used as part of the Event. (Unless a code study and code upgrades are made to comply with an A-3 Occupancy.) Additionally, should the kitchen be used for Events, the kitchen would need to be upgraded to a commercial kitchen.

McCall City Arborist

In an email dated November 26, 2021, the McCall City Arborist had the following comments:

I would like to express our concern regarding the noxious weed problems this property has and encourage planning and zoning to make it a condition of approval that these issues be addressed. It would be our recommendation that they have a mitigation plan to control and manage this issue both during construction and into the future.

Public –

Letter received on November 30, 2021, from Kent Graefe and Tammy L. Buck expressing concern.

CONCLUSIONS OF LAW

1. The City of McCall has provided for the processing of Conditional Use Permits, authorized by Section 67-6512, Idaho Code, pursuant to Title 3, Chapter 13 of McCall City Code.
2. Adequate notice of the December 7, 2021 public hearing was provided, pursuant to Section 67-6512, Idaho Code and Title 3, Chapter 15 of McCall City Code.

3. Upon compliance with the conditions noted below, the application meets the Conditional Use Permit Standards set forth in Title 3, Chapter 13 of McCall City Code.

DECISION

THEREFORE, the McCall Area Planning and Zoning Commission hereby **recommends** this Conditional Use Permit application for **approval**, provided that the following conditions are met (conditions removed by the Commission in strikethrough, additions underlined):

1. Prior to the issuance of a building permit, the applicant shall receive final engineering approval.
2. Prior to the issuance of a building permit, the applicant shall provide a revised site plan identifying a minimum of twelve (12) short-term and four (4) long-term bicycle parking spaces. The required bicycle parking shall be in conformance with McCall City Code Section 3.8.063(A)-(F) and the Association of Pedestrian and Bicycle Professionals' *Essentials of Bike Parking*.
3. Prior to the issuance of a building permit, the applicant shall provide a letter of acceptance from Treasure Valley Transit regarding the location and design of the bus stop and the pathway from the parking area to the bus stop.
4. Prior to the issuance of a building permit, the applicant shall provide elevations of the proposed bus stop and trash bins.
5. Prior to the issuance of a building permit, the applicant shall provide a revised landscape plan which includes a schedule of plant species to be utilized. The revised landscape plan shall be reviewed and approved by staff.
6. Prior to the issuance of a building permit, the applicant shall provide an outdoor lighting plan in conformance with McCall City Code Title III, Chapter 14.

7. Prior to the issuance of a certificate of occupancy, the applicant shall reseed all disturbed areas with native grasses or wildflowers.
8. Quiet hours shall be observed between 10:00 P.M. and 8:00 A.M.
9. The maximum occupancy of the bed and breakfast/short term rental is nineteen (19) individuals.
10. The maximum event occupancy for the property is 110 individuals.
11. No parking on Rio Vista Boulevard is permitted.
12. No indoor assemblies are permitted.
13. Adequate temporary restroom facilities shall be provided for all outdoor events.
14. Pursuant to McCall City Code Section 3.13.034(B), conditional use permit approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.
15. Pursuant to McCall City Code Section 3.16.08, design review approval shall lapse and become void whenever the applicant has not applied for a building permit within one year from the date of initial approval.

Findings of Fact **adopted** this 7th Day of December, 2021.

Robert Lyons, Chair
McCall Area Planning and Zoning Commission

Attest:

Brian Parker, City Planner
City of McCall

City of McCall
216 East Park Street
McCall, Idaho 83638
P.208.634.7142

LAND USE APPLICATION



Date Received: _____

Fees Paid: _____

NOTICE OF ADDITIONAL FEES

Land use applications may be subject to engineering and legal review for purpose of addressing compliance and conformance issues. The City of McCall reserves the right to contract these services to private firms. The costs of these reviews are passed on to the applicant. These fees are separate, and in addition to, the City's application and permit fees. Completion of this application signifies consent to these fees.

Please check all that apply:

- # _____ Record of Survey (ROS) - \$420
- # _____ Design Review (DR) - \$300 + \$25/1,000 sq. ft. of new construction (rounded to the nearest 1,000)
- # _____ Scenic Route (SR) - \$300
- # _____ Shoreline or River Environs (SH) - \$300
- # _____ Conditional Use Permit (CUP) - \$600
- # _____ Development Agreement - \$500
- # _____ Planned Unit Development (PUD) General Plan - \$2,000 + \$75/lot or unit
- # _____ Planned Unit Development (PUD) Final Plan - \$500 + \$75/lot or unit
- # _____ Subdivision (SUB) Preliminary Plat - \$2,500 + \$75/lot or unit
- # _____ Subdivision (SUB) Final Plat - \$1000 + \$75/lot or unit
- # _____ Minor Plat Amendment - \$1,000
- # _____ Variance (VAR) - \$1,000
- # _____ Rezone (ZON) - \$1,500
- # _____ Zoning Code Amendment (CA) - \$750/title
- # _____ Annexation - \$3,000
- # _____ Vacation (VAC) - \$750

Incomplete applications cannot be accepted by the City. Unless otherwise exempted by the Administrator, all Application Requirements must be provided at the time of submission. Please refer to specific application info sheets for more details.

PROPERTY OWNER INFORMATION

Property Owner 1: James Evans Email: jim@evergreengems.com
Mailing Address: 420 Wilhelm Creek Ct. McCall, ID 83638 Phone: 610-547-5877
Property Owner 2 (If Applicable): _____ Email: _____
Mailing Address: _____ Phone: _____

AGENT/AUTHORIZED REPRESENTATIVE INFORMATION

Applicant/Representative: Susan Evans Email: susan@evergreengems.com
Mailing Address: 201 Rio Vista Blvd. Phone: 208-631-0576

PROPERTY INFORMATION

Address(es) of Property: 201 Rio Vista Blvd
Legal Description of Property: _____
Zoning District of Property: R1 Project Sq. Footage (If Applicable): _____
Impact Area City Limits Residential Commercial

LAND USE APPLICATION CONTINUED

Payette Lakes Water and Sewer District or Septic System or not applicable

PROJECT DESCRIPTION

Explain the general nature of what is proposed: (please attach supplemental information if needed)

See attached

201 RIO VISTA BLVD

NEIGHBORHOOD MEETING HELD 10/17/21 5:30 - 6:30 PM

SIGNATURES

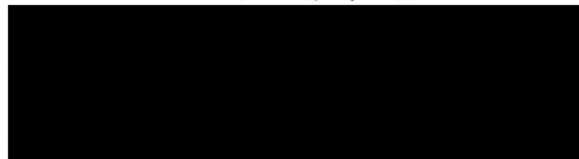
The Applicant hereby agrees to pay reasonable attorney fees, including attorney fees on appeal and expenses of the City of McCall, in the event of a dispute concerning the interpretation or enforcement of the Land Use Application in which the City of McCall is the prevailing party.

I certify that I have reviewed and understand the procedures and requirements of the McCall City Code. I give permission for City staff and/or Planning & Zoning Commission members to view and enter the subject property in order to fully review this application. I understand that failure to provide complete and accurate information on this application may lead to denial of this application.

The submittal items identified in the application info sheets are the minimum required materials for the City to accept applications. The City Planner may require additional information based on the specific circumstances of each proposal.

JAMES EVANS

Property Owner 1

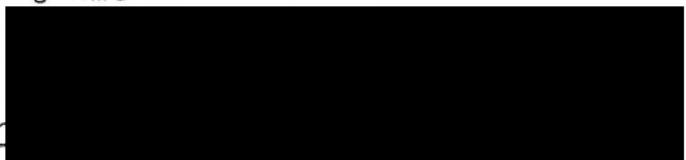


Property Owner 2 (If Applicable)



Agent/Authorized Representative

Signature



FOR RECORD OF SURVEY APPLICATIONS ONLY: STATEMENT OF EASEMENT DISCLOSURE

Surveyor

Signature

I hereby certify that I have performed a thorough search for all relevant easements that relate to the subject property and have indicated or referenced these by their instrument number on the provided survey.



Limited Edition Vacation Rentals & Property Management

Cloud 9

Conditional Use Permit Application

Evergreen Gems Property Management

Project Description

10/25/2021

The Lodge on Cloud 9 (formerly The Northwest Passage Bed & Breakfast CUP-18-11 which we would like to retain) is also currently operated as a short-term rental property. Our proposal is to continue to host as a Bed & Breakfast during the off seasons, as a short term rental the majority of the time year-round and to enhance the natural environment/acreage surrounding the lodge to periodically host guests for outdoor day use events such as weddings/receptions, family reunions, corporate retreats, etc.

The lodge itself has nine bedrooms and ten bathrooms with sleeping capacity for up to 26 individuals in beds which we would like to enable (we currently allow a maximum of 19). With a mid-level pricing plan, we expect the majority of the events held will consist of fewer than one hundred guests (probably run between 50-75) with a maximum of one hundred ten guests a couple of times a year.

The comprehensive plan has identified this area for density higher than R1(R2/4) based on its proximity to the industrial park to the southeast, the storage facility and apartments to the southwest and the single-family homes on R4 property in the process of being built directly across the street to the northwest. We believe that this proposed use is a great segue from industrial and multifamily use to R4 single family homes across the street to the north and for R1 homes further to the northeast.

To meet the needs of an event facility and to be considerate of our neighbors with regards to traffic, noise and safety, we have completed a comprehensive survey of the property and contracted a landscape architect to develop a high-level plan (see attached) to address the 3.13.03 standards required for conditional use.

Proposed/Existing Setbacks:

The current lodge meets setback requirements. We do propose to add small arbor type structures and some storage shed facilities that will be well within the setback requirements.



Limited Edition Vacation Rentals & Property Management

Event Transportation & Parking:

Use of the McCall Free Transit system will be encouraged for guests attending daytime events, but not staying at the property. (Current stop is at the corner of Rio and Boydston). We will also offer a drop point at the property with a path to a bike parking area and the event space. The fixed line route is a flag stop system, and anyone who needs to use the service may flag the bus anywhere along the route where it is safe to do so. Harlow's Shuttle Service will be hired for events that last into the evening beyond 7pm.

Current parking is six (6) paved and seven (7) gravel spaces for a total of thirteen (13) spaces. Per CUP-18-11 (approved for the Bed and Breakfast at this property) - one parking space is required for each sleeping room or nine (9) spaces for overnight guests. The current plan shows a total of about 30 parking spaces. For events, we expect half of those vehicles to bring (2) guests and the other half to bring four (4) guests. There is also parking for bicycles, the option for guests to use the McCall transit system during the day or guests can hire Harlow's private shuttle service.

Title 3, Chapter 13 Conditional Use Permit Standards:

A – Public Hearing: 1) The Neighborhood meeting was held at 201 Rio Vista Blvd. on October 17th from 5:30 – 6:30 pm. Comments and concerns have been addressed in the overall description and the findings below. The next public meeting notices will be mailed and posted no later than November 22nd.

B – Findings:

1. Constitute a conditional use authorized in the zone involved. *(Please see introduction)*
2. Be harmonious with and in accord with the general/specific objectives of the comprehensive plan and/or this title.

Character:

Utilize unique development tools to create a transition from commercial development to residential development and provide open space.

Honor the strong sense of pride in the City's history and heritage throughout McCall.

Emphasize residents' quality of life through a strategy that considers the needs of the various constituencies (full-time residents, visitors, and second homeowners).

Economy:

Ensure a strong and sustainable year-round economy of local businesses.

Strengthen the year-round economy with the addition of conference facilities and boutique hotel venues.

Connections:

Support and utilize public transportation, both within McCall and to the surrounding cities in the West Central Mountain region.



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3. Be designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or likely character of the neighborhood, and that such use will not change the essential character of the surrounding area. *The existing structure is currently used as a vacation rental and a Bed and Breakfast. Many extended families and friends look for the opportunity to stay together on a single property (there are few that can accommodate larger groups). The proposed additional usage upgrades should only enhance the character of the area.*
4. Not be detrimental to the health, safety and general welfare of persons residing or working in the neighborhood of such proposed use. *The use should not be detrimental to the health, safety, and general welfare of the neighborhood. An Evergreen Gems manager will be onsite for all events to make sure everything flows smoothly.*
5. Not cause any substantially harmful environmental consequences to any land or waters within the planning jurisdiction. *The plan is to **actually improve** the environment by adding a significant number of additional indigenous trees, shrubs, grasses, wildflowers, etc.*
6. Not create excessive additional public cost for public facilities and services and will not be detrimental to the economic welfare of the community. *The use will not create any additional cost for public facilities and services as it is already served. It will also contribute to the economic welfare of the community through the hiring of private services.*
7. Be served adequately by essential public facilities and services including highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools. The applicant may be required, as a condition of approval, to mitigate any deficient public service. *The proposed event venue is within the City Limits and is served by existing water and sewer services, garbage services, has police and fire protection, well maintained local streets and is just off a major collector. The property has a separate well for irrigation of the enhanced landscape, will bring in contracted Honey Dippers “porta-potties” and additional trash receptacles as required.*
8. Not involve uses, activities, processes, materials, equipment or conditions of operation that will cause unreasonable production of traffic, noise, smoke, fumes, glare, odors or other forms of pollution. *The use and activities proposed are commensurate with the property size, location and distance from neighbors and should not produce any excessive forms of pollution. Quiet hours will be enforced from 10pm – 8am and any music or noise during regular hours will be directed towards the industrial area and buffered by the house and water features. Traffic will be minimal as cars will be directed to the major collector*



Limited Edition Vacation Rentals & Property Management

(opposite and away from the neighborhood to the east) and the use of hired or free public transportation will be encouraged.

9. Have vehicular approaches to the property so designed as not to create a detrimental interference with traffic on surrounding public or private thoroughfares, or adversely affect the pedestrian environment. *The updated approaches will improve the process of entering and exiting the property. Current departure is difficult due to steep inclines and lack of good visuals up and down the street. There will also be bicycle parking and a pedestrian path from the shuttle drop point to gain access to the property.*
10. Not result in the destruction, loss or damage of an important natural, scenic or historic feature. *The current lodge structure will be maintained. We have added a special "shrine" commemorating the association with the filming of "Northwest Passage". We have already cleared out many dangerous dry and dead debris around the area to mitigate potential fire hazard. Many indigenous species trees/plants will be added to the landscape. More trees and plants = better air!*
11. Be on a site of sufficient size to accommodate the proposed use, including the yards, open spaces, snow storage, walls, fences, parking areas, loading zones and design standards applicable. *The site is approximately 4.8 acres and is of sufficient size to accommodate the proposed use with enhanced gardens, open spaces, snow storage, parking areas, loading zones, etc.*
12. Have a minimal negative economic impact on the neighborhood or surrounding community. *The project as proposed will have a positive economic impact on the surrounding community as guests will use many private services to help enable these businesses to grow.*



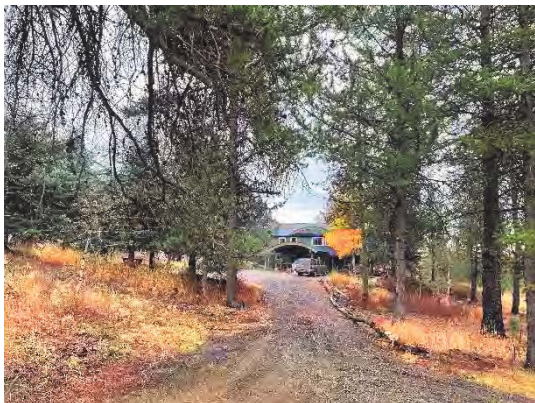
Current entry from street (too steep)



new entry



Limited Edition Vacation Rentals & Property Management



New entry split to house



main house parking area



Main house carport entry and snow storage



future carport site (bldg. approved)



Future gravel parking event (snow mobile trailers)



small parking slots – no trees removed



Limited Edition Vacation Rentals & Property Management



Future catering access and parking



parking/catering



Dining Tent site



Dining Tent example (temporary structure)



Future cascading water feature and garden site with deck promenade to ceremony space



Limited Edition Vacation Rentals & Property Management



Water garden ideas – don't be fooled by palm trees!



Ceremony site



just an example of what a set up might look like



example of lighting in parking area (all lighting will follow city/dark sky requirements)

Meredith Todd

From: Brian Parker
Sent: Tuesday, November 23, 2021 1:34 PM
To: Meredith Todd
Subject: FW: FW: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



Keeping citizens informed.

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From: James Evans <jim@evergreengems.com>
Sent: Monday, November 22, 2021 10:08 AM
To: Brian Parker <bparker@mccall.id.us>
Cc: Susan Evans <susan@evergreengems.com>; John Powell <jpowell@mccall.id.us>; Garrett de Jong <garrett@mccallfire.com>
Subject: Re: FW: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Brian,

Thank you for the clarifications. Please adjust our application as follows:

1. We withdraw our request to increase the Short Term Rental/Bed & Breakfast (the structure/building) occupancy limit from 19 to 26.
2. To clarify, our request in the CUP application is about allowing Outdoor events on the grounds. The STR/B&B structure/building will not be used as part of the outdoor event. That was never our intention.
 - A. Accordingly, the 1.5 'public' bathrooms (that are not en-suite) within the structure will no longer be made available for event support. Instead, we will rent the number of porta potties required to support the event's attendance.

B. Also, the kitchen(s) will not be used in support of the event. It has always been our intention to hire a licensed catering company to provide food and drink for the event.

Please let me know if any of my clarifying statements above raises other questions or uncertainties that I may not have anticipated.

Sincerely,

Jim Evans, Partner
Evergreen Gems Vacation Rentals
610 547-5877



On Mon, Nov 22, 2021 at 8:40 AM Brian Parker <bparker@mccall.id.us> wrote:

Jim,

After discussing with John, here is what is going to be required from the Building Official:

1. Bed & Breakfast – Currently approved for 19 occupants. If the applicant wishes to increase the number of occupants the occupancy type will change to R-1 and fire sprinklers will be required, along with other potential upgrades.
2. Event Center – An event center is a different occupancy type. It would be classified as an Assembly Occupancy. This would also activate the requirement for fire sprinklers and fire walls, etc. If the applicant wishes to use the grounds as an event center, the Bed & Breakfast structure may not be used as part of the Event. (Unless a code study and code upgrades are made to comply with an A-3 Occupancy.) Additionally, should the kitchen be used for Events, the kitchen would need to be upgraded to a commercial kitchen.

Please clarify the intended use of the building based on these requirements.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us

Blog: mccallcitysource.com

Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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From: James Evans <jim@evergreengems.com>
Sent: Wednesday, November 10, 2021 2:06 PM
To: Brian Parker <bparker@mccall.id.us>
Cc: Susan Evans <susan@evergreengems.com>; John Powell <jpowell@mccall.id.us>
Subject: Re: FW: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

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Brian,

Well the message from the Fire Marshall just got my attention! We are going to need your guidance here as I'm now quite confused.

Some facts and observations I'd like to share -

1. The home was originally built in the 1930s. It was expanded and remodeled in the early 90s. It was then remodeled again into its current configuration in 2018/2019 by Colby Peters, the previous owner. This last remodel was down to the studs and of course, fully authorized and approved by McCall with both smoke detectors and CO Detectors. On our

own initiative, we have added explosive gas detectors as the kitchen range uses propane. We do this in all our applicable properties.

2. There is an existing and active CUP for the property obtained by Colby as a Bed and Breakfast, circa 2019. To our knowledge, there is no mention in the existing B&B CUP concerning occupancy restrictions. I read it and don't believe the subject is even mentioned. There is certainly no Sprinkler System requirement mentioned.

3. The property currently has an approved business license as a Short Term Rental (STR) allowing occupancy up to 19 per McCall's STR Ordinance. With 9 bedrooms and 10 bathrooms, it easily handles 19, all in bedrooms and beds. Our CUP proposes an increase to 26 as that was and is the existing bed sleep capacity of the property when we purchased it.

4. The current zoning of the 5 acre property is R1 with R4 across the street. The future land use plan calls for a change to R2/4.

5. I think it is safe to say there is no requirement in the McCall STR Ordinance (that allows occupancy up to 19) for sprinkler systems. While I don't know the current count of STRs in McCall, I do know it is measured in the many hundreds. I wouldn't be surprised if 20% or more can legally sleep more than 10.

6. Earlier this year, we applied for and gained permission to move our driveway to improve Guest, Service and Emergency Vehicle access to the property. That change is mostly in place today and will be completed in Spring.

I hope this is a helpful set of facts and observations that makes your job easier.

Please advise on way forward,

Sincerely,

Jim Evans, Partner

Evergreen Gems Vacation Rentals

610 547-5877



On Wed, Nov 10, 2021 at 3:01 PM Brian Parker <bparker@mccall.id.us> wrote:

Jim and Susan,

Please see the comments below from the McCall Fire Chief.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us

Blog: mccallcitysource.com

Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



[Please click to sign up for CodeRED!](#)

From: Garrett de Jong <garrett@mccallfire.com>

Sent: Friday, November 5, 2021 10:53 AM

To: Brian Parker <bparker@mccall.id.us>

Cc: John Powell <jpowell@mccall.id.us>

Subject: RE: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

RE: CUP-21-04 & DR-21-38: 201 Rio Vista Blvd – Susan Evans of Evergreen Gems and Cloud 9 Lodge

Hi Brian,

The building at 201 Rio Vista Blvd was likely built under the residential code. Idaho's adopted fire code correlates with the Building Code, in which commercial buildings are constructed. The fire code and building code do not apply to one- and two-family dwellings built under the residential code, which this home originally was. The city building official determines occupancy classification at the time of construction and can change it if the use of the building changes.

The fire code defines occupancies containing sleeping units where the occupants are primarily transient in nature as being classified as R-1, which includes boarding houses with more than ten occupants, congregate living facilities with more than ten occupants, hotels, and motels. A traditional home, where the occupants are primarily permanent in nature, is classified as R-3. The main difference between the two classifications regarding fire code is that R-1 occupancies require automatic fire sprinkler systems, but R-3 does not. In my opinion, based on the 2018 fire code, any building that sleeps over ten occupants, transient in nature, should have an automatic fire sprinkler system installed.

Because of the use classification, my comments are recommendations to the City of McCall Planning and Zoning and the City of McCall Building Official based on applying the 2018 International Fire Code to the intended use of the building, R-1. They are recommendations as the fire district does not have the authority to inspect or enforce fire code on one-and-two family dwellings.

Recommendations:

- An automatic sprinkler system installed in accordance with Section 903.3 shall be provided throughout the building. (IFC2018 903.2.8 Group R). The building fire alarm system shall monitor the automatic fire extinguishing system in accordance with NFPA 72 (IFC2018 904.3.5).
- Per (IFC2018 [A] 102.3 Change of use or occupancy), a building code analysis should be done to bring the structure up to building code standards, as determined by the city building official, including smoke detectors and carbon monoxide detectors installed following current building code and for the intended use.
- A parking plan for guest vehicles should be developed to make the driveway accessible for emergency vehicles (IFC2018 403.12.2).

Thank you,

Garrett

Garrett de Jong

Fire Chief

McCall Fire & EMS

201 Deinhard Lane

McCall, ID 83638

208-634-7070



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This message has been sent to you as official business of the McCall Fire Protection District. If you have a concern about the authenticity of this communication, including any attachments, please contact the sender directly for confirmation, either by telephone or separate e-mail. Unencrypted e-mail is inherently insecure and should be treated with caution.

Electronic Privacy Notice. This e-mail, and any attachments, contains information that is, or may be, covered by the Electronic Communications Privacy Act, 18 U.S.C. 2510-2521, and is also confidential and proprietary in nature. If you are not the intended recipient, please be advised that you are legally prohibited from retaining, using, copying, distributing, or otherwise disclosing this

information in any manner. Instead, please reply to the sender that you have received this communication in error, and then immediately delete it. Thank you in advance for your cooperation.

From: Brian Parker <bparker@mccall.id.us>

Sent: Thursday, November 4, 2021 2:21 PM

To: Bob Fodrea <bob@fodrealandgroup.com>; Chip Bowers <chip@bowerslandssurveys.com>; Chris Curtin <ccurtin@mccall.id.us>; Cynda Herrick <cherrick@co.valley.id.us>; Dale Caza <dcaza@plrwsd.org>; David Simmonds <dsimmonds50@gmail.com>; Delta James <djames@mccall.id.us>; Garrett de Jong <garrett@mccallfire.com>; Gene Trantham <gtrantham@mccall.id.us>; IDL Jurisdictional Inbox <IDL_jurisdictional@idl.idaho.gov>; ITD Development Services <D3Development.Services@itd.idaho.gov>; ITD District 3 Permits <ITDD3Permits@itd.idaho.gov>; Jasen King, IDL <jking@idl.idaho.gov>; Jeff Bateman <jbateman@plrwsd.org>; jennifer.schildgen@itd.idaho.gov; John Powell <jpowell@mccall.id.us>; Jordan Messner <jordan.messner@idfg.idaho.gov>; jthrower@gmail.com; Kurt Wolf <kwolf@mccall.id.us>; Lance Holloway, DEQ <lance.holloway@deq.idaho.gov>; Laura Shealy BPLWQAC <idchik5@gmail.com>; Laurie Frederick, Valley Co Cartographer <lfrederick@co.valley.id.us>; Levi Brinkley <lbrinkley@mccall.id.us>; Linda Stokes <lstokes@mccall.id.us>; Lori Hunter (lhunter@co.valley.id.us) <lhunter@co.valley.id.us>; Lorraine Brush <lbrush@plrwsd.org>; Mark Wasdahl, ITD <mark.wasdahl@itd.idaho.gov>; Meredith Todd <mtodd@mccall.id.us>; Michelle Groenevelt <mgroenevelt@mccall.id.us>; Mike Reno <mreno@cdh.idaho.gov>; Morgan Stroud <mstroud@mccall.id.us>; Nathan Stewart <nstewart@mccall.id.us>; Regan Berkley <regan.berkley@idfg.idaho.gov>; Richard Stein <rstein@mccall.id.us>; Sabrina Sims <ssims@mccall.id.us>; Sarah Arjona <Sarah.Arjona@itd.idaho.gov>; Scott Corkill, IDL <scorkill@idl.idaho.gov>; Sheri Staley - Idaho Power <sstaley@idahopower.com>; Steve Moser, Idaho Power <smoser@idahopower.com>; Valley County Road Dept <roaddept@co.valley.id.us>

Subject: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

All,

Please provide comment on the attached application by December 7th, 2021.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us

Blog: mccallcitysource.com

Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



[Please click to sign up for CodeRED!](#)

Topographic Survey for Evergreen Gems

Situate in a Portion of the NW 1/4 of the NW 1/4 of Section 17
Township 18 North, Range 3 East, Boise Meridian
City of McCall, Valley County, Idaho
2021

RECEIVED
10/26/2021



Legend:

	FOUND V-P MONUMENT
	FOUND 2" MONUMENT
	SURVEILY BENCHMARK
	CALCULATED POINT, NOTHING FOUND OR SET
	WATER VALVE
	WATER METER
	FIRE HYDRANT
	FROST FREE HYDRANT / SPICOT
	WELL
	SANITARY SEWER MANHOLE
	PRESSURE IRRIGATION VALVE
	POWER POLE
	SIX WIRE
	STREETLIGHT
	ELECTRIC METER
	DECIDUOUS TREE
	CONIFEROUS TREE
	PARCELS BOUNDARY LINE
	ADJACENT PROPERTY LINE
	ROADWAY CENTERLINE
	EASEMENT LINE
	EDGE OF SHOULDER
	EDGE OF PAVEMENT
	FENCE LINE
	RIGHT-OF-WAY LINE
	SANITARY SEWER LINE
	WATER LINE
	OVERHEAD POWER LINE
	UNDERGROUND POWER LINE
	EXISTING BUILDING
	EXISTING GROUND CONTOUR

Topographic Survey Evergreen Gems Cloud 9

201 Rio Vista Blvd
McCall, ID 83638

Referenced Survey Table:

R1.	PLAT OF MCCALL VALLEY STORAGE CONDOMINIUM, BOOK 13, PAGE 64, INSTRUMENT # 63465, RECORDS OF VALLEY COUNTY.
R2.	PLAT OF MCCALL INDUSTRIAL PARK, BOOK 8, PAGE 11, INSTRUMENT # 156826, RECORDS OF VALLEY COUNTY.
R3.	RECORD OF SURVEY, INSTRUMENT # 303542, RECORDS OF VALLEY COUNTY.
R4.	RECORD OF SURVEY, INSTRUMENT # 288234, RECORDS OF VALLEY COUNTY.

- Keynotes:**
- UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE LOCATIONS SHOWN HEREON ARE BASED UPON ABOVE GROUND EVIDENCE AND UTILITY COMPANY FACILITY MAPS. THE SURVEYOR MAKES NO GUARANTEE OF THE ACCURACY OF LOCATION OF UNDERGROUND UTILITIES.
 - BOUNDARY INFORMATION SHOWN HEREON IS BASED ON RECORDS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY AND POINT MONUMENTS ON THE GROUND. ENCROACHMENTS AND OTHER ENCUMBRANCES WHICH MAY BE APPARENT BY CONDUCTING A FULL TITLE SEARCH, ARE NOT NECESSARILY DEPICTED ON THIS SURVEY.
 - THE BASIS OF BEARING OF THIS MAP IS GRID NORTH ON THE IDAHO STATE PLANE COORDINATES SYSTEM (NAD 83), WEST ZONE, AS DETERMINED BY GLOBAL POSITIONING SYSTEMS METHODS. ANY DISTANCES SHOWN ARE HORIZONTAL GROUND DISTANCES IN U.S. SURVEY FEET.

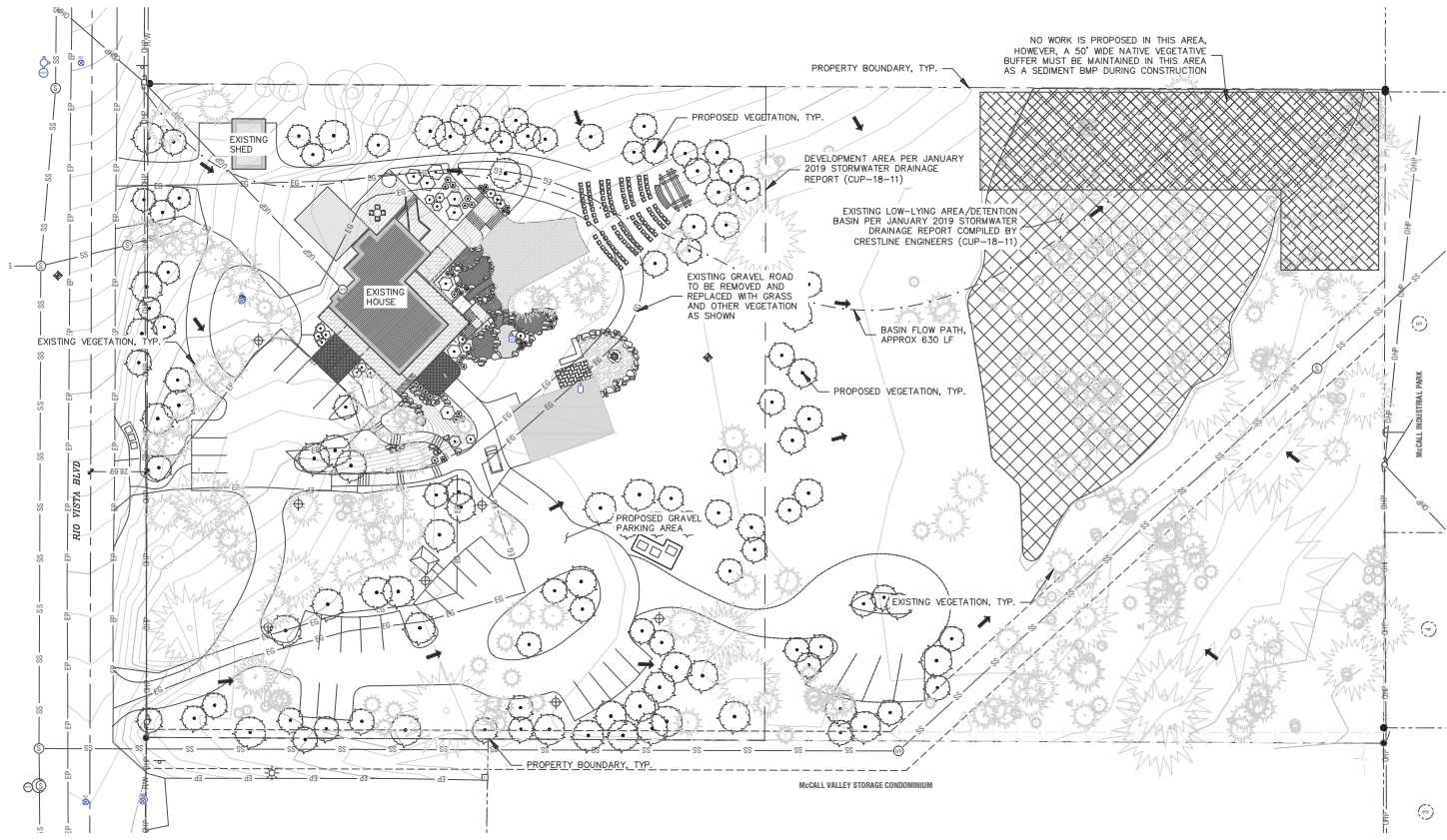
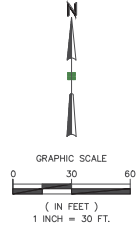


Topographic Survey

Horizontal Scale: 1"=30'



RECEIVED
By Meredith Todd at 9:56 am, Oct 26, 2021



- LEGEND**
- FLOW PATH
 - - - - - DEVELOPMENT AREA BOUNDARY
 - - - - - PROPERTY BOUNDARY
 - [Hatched Box] EXISTING LOW-LYING/DETENTION AREA
 - [Cross-hatched Box] EXISTING LOW-LYING/DETENTION AREA
 - FLOW ARROW

- NOTES**
1. ALL SITE GRADING ADJACENT TO BUILDINGS SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING AT A MINIMUM OF 1/8" IN HARDSCAPE AREAS AND 4% IN LANDSCAPE AREAS.
 2. DRIVEWAY AND GRAVEL PARKING AREAS SHALL BE GRADED IN A WAY TO NOT SUBSTANTIALLY ALTER EXISTING DRAINAGE PATTERNS AS ILLUSTRATED IN THIS GRADING AND DRAINAGE PLAN.
 3. AREAS WITHIN THE PROPERTY BOUNDARY SHALL BE SLOPED TO RETAIN RUNOFF ON-SITE. IF NECESSARY, SWALES SHALL BE CONSTRUCTED TO PREVENT RUNOFF FROM FLOWING ONTO ADJOINING PROPERTIES.
 4. REVEGETATION AND STABILIZATION OF DISTURBED PROJECT AREAS SHALL BE IN ACCORDANCE WITH THE PROJECT LANDSCAPE PLAN.
 5. A MINIMUM OF A 50' WIDE VEGETATIVE BUFFER SHALL BE RETAINED ON THE DOWNHILL PORTION OF THE SITE AS A SEDIMENT CONTROL BMP, AS SHOWN ON THE PLAN VIEW.

CLOUD 9 – GRADING, DRAINAGE & STORM WATER MANAGEMENT PLAN
 PROJECT NO. _____
 SCALE: 1"=30'
 MCGILL

DESIGNED BY: N/A	CHECKED BY: N/A
DRAWN BY: AME	PLOT DATE: 10/23/21
DATE: _____	DESCRIPTION: _____
SHEET	
1 OF 1	

P:\PROJECTS\2021\CLOUD 9 - GRADING, DRAINAGE & STORM WATER MANAGEMENT PLAN\DWG\CLOUD 9 - GRADING, DRAINAGE & STORM WATER MANAGEMENT PLAN.DWG

← Rio Vista →

ESS

40
110

STORAGE
25
18

SPA

DOOR

42
30

PATIO
50
25

10
60

10
20

10
10

10
10

10
10

10
10

10
10

10
10

10
10

24
50'

HOUSING

8x60
4x60

8x50

5x15

40x220

BBQ OUTDOOR
KITCHEN/
BAR

VIEWING DECK

LOADING
ZONE

7 PARKING
SPOTS

TRASH

LIGHTING

House - 4728+960: 5688

Uncovered - 3850

Driveway - 19,420

1" = 30'

70 Lot Coverage

4.87 ac = 212,137 SF

6.8%

Total Area @ 70%

100% - 5688

50% - 1925

35% - 6797

14,410 SF

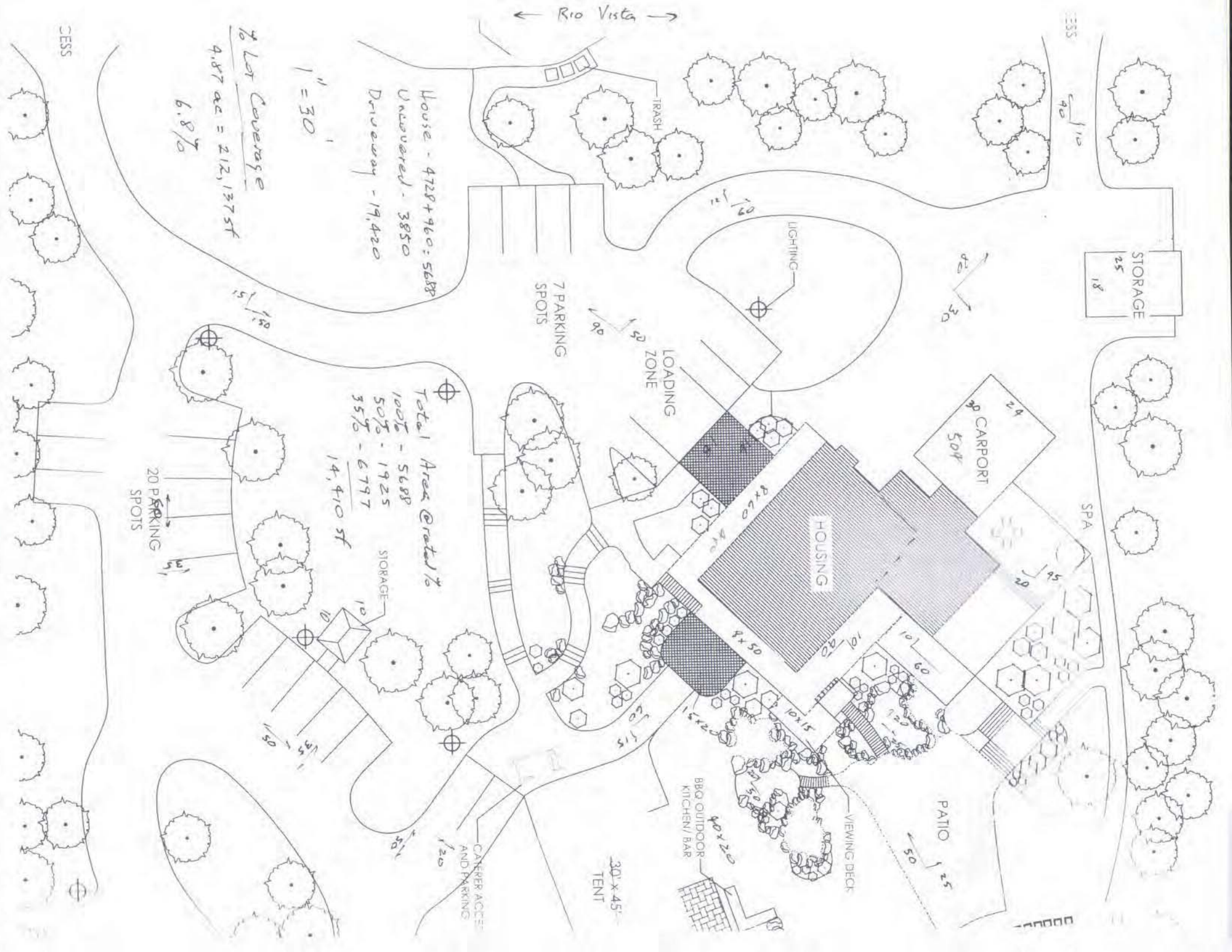
STORAGE

CARRIER ACCE
AND PARKING

30' x 45'
TENT

20 PARKING
SPOTS

CESS



STORMWATER APPLICATION
City of McCall

Fill in all information. Submit one copy of signed application and three copies of Stormwater Management Plan/Report to the City Engineer.

1. Project Name: Evergreen Gems Cloud 9
Location: 201 Rio Vista Boulevard, McCall, ID 83638

2. Owner's Name: James L. Evans
Street: 201 Rio Vista Blvd. City: McCall
State: ID Zip Code: 83638 Phone: 601-547-5877

3. Project Description: The proposed project includes a CUP application to hold events on the property. Improvements are generally landscape related with minimal additional impervious areas. The project will provide additional gravel parking areas to accommodate guests, paver patios to provide social areas and a dance floor, as well as the conversion of some existing gravel driveway areas into lawn for events. The project will also provide substantial additional vegetation and plantings across the site.
 - a. Total property area, in acres. 4.86 Acres
 - b. Proposed impervious surface (asphalt, rooftop, concrete, sidewalk, etc.) in square feet. The total approximate existing impervious area is 9,319.2 SF (Existing Conditions).
The project is proposing additional impervious areas as follows: Covered Parking - 929 SF
Additional Decking - 1,552 SF, Paver Patio Space - 2,288 SF. TOTAL IMPERVIOUS = 13,939 SF
 - c. Describe existing vegetation present on site. Existing site contains coniferous and deciduous trees, as well as sparse to light understory vegetation such as sagebrush
 - d. Start date of construction. Spring 2022
 - e. Estimated length of time to complete improvements. 6-12 months

4. Stormwater Management Plan/Report attached? Yes No

5. Circle the section of the Stormwater Management Plan/Report Checklist which are applicable to project.
A X B X C X D X E F X

6. Party responsible for operation and maintenance of project, including maintenance of temporary and permanent Best Management Practices:

James L. Evans

Owner

Name

Title

James L. Evans

Signature

10.24.21

Date

201 Rio Vista Blvd.

Address

610
601-547-5877

Daytime Phone

After Hours Phone

Do not write below this line.

This Stormwater Management Plan/Report is:

Approved: _____

Not Approved: _____

Approved, with conditions: _____

By The City of McCall

Representative

Title

Signature

Date

Austin Edwards
Evergreen Gems Property Management
201 Rio Vista Blvd
McCall, Idaho 83638
E. Austin@Evergreengems.com
P. (208) 871-7038

RECEIVED

By Meredith Todd at 9:55 am, Oct 26, 2021

October 23, 2021

City of McCall
216 E. Park Street
McCall, Idaho 83638

Re: Evergreen Gems Cloud 9 Stormwater Drainage Narrative

Megan,

The proposed CUP application for the Evergreen Gems Cloud 9 event center is located on a parcel that had a stormwater drainage report completed on it in January of 2019. The City of McCall has provided us with this report. The proposed site improvements with the Cloud 9 project will not substantially alter the site from what was described in this referenced report dated January 17, 2019, prepared for CUP-18-11. The intent of this narrative is to describe any revisions that need to be made to this initial report to relate it to the Cloud 9 project. Per the included Stormwater Application, we are required to address sections A, B, C, D, & F of the Stormwater Management Plan/Report Checklist. Many of these sections will not require modification with the Cloud 9 project and are described in more detail below.

Part A: Basin Characteristics

This project consists of the renovation of an existing bed and breakfast to an event center to be used for weddings, work events, etc. on the property located at 201 Rio Vista Blvd, McCall, ID 83638. The project does not propose to install any improvements that will alter the existing drainage patterns on the site. The project will provide additional vegetated areas, consisting of grass, trees, shrubs and other plantings, as well as some additional gravel parking areas for event guests. However, these improvements will be constructed in a way that will maintain historic drainage patterns (Included Grading, Drainage & Storm Water Management Plan). The basin characteristics of the site are described in the city provided Stormwater Drainage Report dated January 17, 2019 and generally remain unchanged. The items to be altered are detailed below:

1. Project Site

- d. Total area of existing asphalt parking/driveway is 0.124 acres (5,405.0 SF). The total area of existing gravel parking/driveway is 0.178 acres (7,771.4 SF). This project will remove approximately 0.09 acres (3,950 SF) of existing gravel driveway and will replace it with vegetated areas. The project will add approximately 0.46 acres (20,115 SF) of gravel driveway and parking areas, for a net increase of 0.37 acres of gravel driveway.
- e. The total roof area of the existing residence, shed, and proposed covered parking is 0.11 acres (4,843.2 SF).
- f. There is approximately 0.32 acres (13,939 SF) of impervious area existing & proposed within the property boundary.

Part B: Erosion and Sediment Control

1. Description of existing site prior to activity.

- a. Please reference section B.1.a of the city provided Stormwater Drainage Report dated January 17, 2019 associated with CUP-18-11 for existing site conditions prior to activity. The existing site has remained virtually unchanged since that time aside from the addition of approximately 330 SF of impervious roof area, for a total building roof area of 0.097 acres (4,244.2 SF).
- b. Please reference section B.1.b of the city provided Stormwater Drainage Report dated January 17, 2019 associated with CUP-18-11 for site run-off characteristics. This will not change with the proposed project.

2. Description of land improvement activity.

- a. Land improvements activities proposed with this project include additional vegetated areas, consisting of grass, trees, shrubs and other plantings, as well as additional patio areas, deck areas, a water feature, and gravel parking areas. These improvements can be seen on the landscape plan provided with the application. The areas of additional proposed cover are as follows:
 - i. Building Roofs – 0.013 acres (599 SF)
 - ii. Gravel Parking – 0.46 acres (20,115 SF)
 - iii. Patio areas – 0.052 acres (2,288 SF)
 - iv. Decking – 0.035 acres (1,552 SF)
 - v. Water Feature – 0.018 acres (864 SF)
- b. The proposed site improvements will not alter the current drainage patterns through the site. These improvements will generally match existing site topography and existing run-off characteristics will remain the same. Existing vegetation will act as erosion and sediment control. The existing low-lying area/detention basin on the southern portion of the property will detain runoff in accordance with the included Grading, Drainage & Stormwater Management plan. This existing detention basin will remain unchanged from that described in the city provided Stormwater Drainage Report dated January 17, 2019 and offers more than adequate capacity to handle a design storm on the proposed site (See part D).

3. A plan which demonstrates the methods for sediment and erosion control. The plan should indicate the size, location and method for installation or implementation of the BMP.

- a. A stormwater management plan can be found on the included Grading, Drainage & Stormwater management plan. Other BMPs may be necessary as described in section B.4 below.

4. Details and specification for the proposed BMPs which describe their installation and maintenance procedures.

- a. The provided Grading, Drainage & Stormwater management plan specifies that a 50' minimum vegetative buffer be maintained on the downgradient portion of the site to act as erosion and sediment control. Based on the proposed construction activities, the provided buffer should be well in excess of the required 50'. Should additional BMPs be required please see section B.4 of the city provided Stormwater Drainage Report dated January 17, 2019 from CUP-18-11. The site conditions and potential BMP installation

and maintenance procedures required for the proposed project remain identical to that application.

5. **A sequence and schedule of construction activities, including when erosion and sediment control devices and practices will be implemented. The sequence and schedule must include a timetable for project finish and a strategy for long term site stabilization and removal of temporary BMPs.**
 - a. Permanent BMPs are currently provided on site as shown on the provided Grading, Drainage & Stormwater management plan.

Part C: Conveyance System

Drainage calculations are not necessary for this project as there are no existing conveyance facilities on-site, or downstream of the development area. Runoff currently drains away from existing structures and to an existing low-lying, natural, detention basin on the south end of the site. This area can be seen on the provided Grading, Drainage & Stormwater Management Plan. This basin volume was calculated in the city provided Stormwater Drainage Report dated January 17, 2019, from CUP-18-11 and will remain unchanged with this project. Please see associated report for basin volume calculations.

Part D: First Flush Treatment

A Hydraulic analysis of the first flush for the proposed project was performed using the same criteria described in the city provided Stormwater Drainage Report dated January 17, 2019, from CUP-18-11. Please refer to the referenced report for analysis criteria. The Simplified SCS Runoff Curve Number Method (TR-55) was used.

Post-Development Drainage Area General Assumptions:

Calculation Method	SCS Curve Number Method, Hydraflow Hydrographs Extension for AutoCAD Civil 3d
Runoff Curve Number (CN):	81.4 (Composite)
Design Event	24-hour rainfall event
Rainfall, P (24-hour):	0.81 inches

Table 1

Per the analysis results shown in **Appendix A**, and the calculations shown in **Appendix B** of this report, the total runoff volume of the 24-hour, 95% rainfall event is estimated to be 830 CF. The detention of this volume can be detained by the existing basin located on the southeast corner of the site. The volume of this detention facility is 6,195 CF per the city provided Stormwater Drainage Report conducted on this site in 2019. This basin volume will not change with this project. Please refer to the referenced resource for basin volume calculations. The passive pretreatment for sediment through the existing natural vegetation should provide the necessary treatment required for runoff from hard surfaces located on the site.

Drainage Area	Pre-treatment	24-hour, 95% storm volume	Detention Design Volume
Total Property Boundary	Yes (Passive)	830 CF	6,195 CF

Table 2

As illustrated in Table 2 above, the existing detention basin can carry approximately seven times the estimated 24-hour rainfall event.

Part E: Permanent BMPs


Please reference section E of the city provided Stormwater Drainage Report dated January 17, 2019 associated with CUP-18-11 for permanent BMPs. This will not change with the proposed project.

Part F: Operation and Maintenance

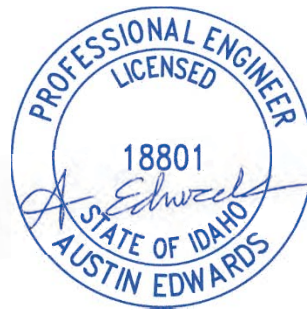
Please reference section F of the city provided Stormwater Drainage Report dated January 17, 2019 associated with CUP-18-11 for operation and maintenance activities. This will not change with the proposed project.

Please don't hesitate to contact me with any questions.

Sincerely,

A handwritten signature in blue ink that reads "A Edwards". The signature is fluid and cursive.

Austin Edwards, PE



10/23/2021

NORTHWEST PASSAGE BED AND BREAKFAST STORMWATER DRAINAGE REPORT

JANUARY 2019

CUP-18-11

Prepared for:

Colby Peters
201 Rio Vista Boulevard
McCall, Idaho 83638



Revised January 17, 2019



Crestline Engineers, Inc.
323 Deinhard Lane, Suite C
PO Box 2330
McCall, Idaho 83638
(208) 634-4140

NORTHWEST PASSAGE BED AND BREAKFAST STORMWATER DRAINAGE REPORT

JANUARY 2019

Crestline Engineers, Inc.
323 Deinhard Lane, Suite C
PO Box 2330
McCall, Idaho 83638
(208) 634-4140

NORTHWEST PASSAGE BED AND BREAKFAST
STORMWATER DRAINAGE REPORT
JANUARY 2019

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2. Stormwater Detention Basin Volume Calculations	
3. Stormwater Detention Storage/Peak Flow Attenuation Summary Tables	
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APPENDIX A – FIGURES/DRAWINGS	
FIGURE 1 OF 1	VICINITY MAP
DRAWING NO. C-1	EXISTING PHYSICAL CONDITIONS WITH SITE PLAN
DRAWING NO. C-2	GRADING, DRAINAGE, AND STORMWATER MANAGEMENT PLAN

SECTION I

STORMWATER APPLICATION, CITY OF MCCALL

SECTION II

STORMWATER DRAINAGE REPORT

NORTHWEST PASSAGE BED AND BREAKFAST
STORMWATER DRAINAGE REPORT
JANUARY 2019

PART A: BASIN CHARACTERISTICS

The project consists of the renovation of an existing residence to operate as a bed and breakfast on the property located at 210 Rio Vista Boulevard, McCall, Idaho (see Appendix A, Vicinity Map). As currently proposed, the project does not include any ground disturbing activities or improvements which will alter the existing drainage patterns on the property, including the existing asphalt and gravel driveway/parking areas. This report also includes necessary provisions should any future minor earth disturbing activities be required on the site that do not increase total areas of impervious surfaces. The total area associated with the property is 4.86 acres (211,739.0 S.F.) and for the purposes of this report, a development area 2.43 acres (105,869.5 S.F.) was also analyzed. Existing topography and property boundaries as shown within this report/plan are based upon City of McCall Topographic Data obtained from the City's GIS department.

The existing site contains coniferous and deciduous trees such as Aspen trees. The site also consists of sparse to light native understory vegetation such as sagebrush. There are existing asphalt and gravel parking spaces and driveways that are approximately 0.302 acres (13,176.4 S.F.) which provides access to the existing residence/B&B and surrounding out buildings. The topography of the site slopes to the southeast with an average slope of approximately 4.3%. Soils located within the property consist of McCall complex and Kangas. McCall complex is a very deep and somewhat excessively drained soil located on slopes ranging 5 to 50 percent. Kangas is a fine gravelly loamy coarse sand located on slopes ranging 0 to 2 percent. Please, refer to the *Soil Survey of Valley Area, Idaho, Parts of Adams and Valley Counties* for additional information on soils within the project area.

Although this project has already been developed, this report addresses the project's compliance with the City of McCall DMGs stormwater management and erosion/sediment control measures such that it shows pre-development of the property prior to the building of the residence and post-development, after the residence was built. Through the implementation of this plan, potential stormwater impacts to downstream water resources and adjoining properties will be mitigated.

The development area associated with this project consists of 2.43 acres (105,869.5 S.F.). The total impervious surface area, which includes the existing residence, shed, and existing asphalt parking/driveway surfaces, is approximately 0.21 acres (9,319.2 S.F.). There are no known existing stormwater drainage improvements or BMPs on the site.

1. Project Site

- a. The total site area is 4.86 acres (211,739.0 S.F.).
- b. The development area for this project is 2.43 acres (105,869.5 S.F.).

- c. The development area density is one (1) existing primary residence including an existing shed per 2.43 acres (105,869.5 S.F.).
- d. The total area of the existing asphalt parking/driveway is 0.124 acres (5,405.0 S.F.). The total area of the existing gravel parking/driveway is 0.178 acres (7,771.4 S.F.).
- e. The total roof area of the existing residence and shed are 0.090 acres (3,914.2 S.F.).
- f. There is approximately 9,319.2 S.F. of impervious area within the property boundary.

2. Summary of the physical conditions onsite as well as for the upstream contributing area.

Onsite Drainage Area

There are no proposed improvements within the total property and development area at this time besides the renovation of the existing residence. The post-development land use conditions of the renovation will not alter the property boundary site or development area, therefore, onsite drainage area will remain the same. The existing conditions within the property and upstream contributing areas are provided in Appendix A, Construction Drawings and include the following:

- a. The total property boundary area is approximately 4.40% impervious. The development area is approximately 8.80% impervious. Pre-development of the total property boundary area and development area was approximately 0% impervious.
- b. The total property boundary area flow path drainage length is approximately 629.0 feet. The development area flow path drainage length is approximately 355.0 feet. Pre-development of the total property boundary area flow path drainage length was 640.9 feet. Pre-development of the development area flow path drainage length was approximately 355.0 feet.
- c. The average slope of the drainage in the total property boundary area flow path is approximately 4.3%. The average slope of the drainage in the development area flow path is approximately 6.8%. Pre-development, the average slope of the drainage in the total property boundary area flow path was also approximately 4.3% and the average slope of the drainage in the development area flow path was approximately 6.8%.
- d. There are no known wetlands on the property.

Upstream Contributing Drainage Area

Runoff from the property enters the northeast portion of the site. A negligible amount of offsite runoff flows through the property as a majority of it is intercepted by Rio Vista Boulevard.

3. Existing drainage facilities impacted by the proposed development on the site and downstream of the proposed development.

There are no existing drainage facilities that will be impacted upon project completion.

PART B: EROSION AND SEDIMENT CONTROL

1. Description of existing site prior to activity.

- a. Total Property Boundary Area Surfaces = 4.86 acres (211,739.0 S.F.)
Total Impervious Area = 0 acres (0 S.F.)
Total Development Area Surfaces = 2.43 acres (105,869.5 S.F.)
Total Impervious Area = 0 acres (0 S.F.)

For the purposes of this report, historic pre-development conditions associated with an unimproved site/vacant lot were used for stormwater modeling purposes. Actual existing site conditions at the time of this report can be found below and are anticipated to remain unchanged throughout the implementation of the project.

Total Property Boundary Area Surfaces = 4.86 acres (211,739.0 S.F.)
Building Roofs = 3,914.2 acres (0.090 S.F.)
Asphalt Parking/Driveway = 0.124 acres (5,405.0 S.F.)
Gravel Parking/Driveway = 0.178 acres (7,771.4 S.F.)
Open Space = 4.468 acres (194,648.4 S.F.)
Total Impervious Area = 0.21 acres (9,319.2 S.F.)

Total Development Area Surfaces = 2.43 acres (105,869.5 S.F.)
Building Roofs = 3,914.2 acres (0.090 S.F.)
Asphalt Parking/Driveway = 0.124 acres (5,405.0 S.F.)
Gravel Parking/Driveway = 0.178 acres (7,771.4 S.F.)
Open Space = 2.038 acres (88,778.9 S.F.)
Total Impervious Area = 0.21 acres (9,319.2 S.F.)

- b. A negligible amount of offsite runoff runs through the property. Upslope runoff is currently intercepted by Rio Vista Boulevard and an existing/adjacent swale on the north side of the road. Upslope runoff enters in the northeastern section of the property, flowing downstream through the

property, and is filtered within property boundary area and is anticipated to not leave the site.

2. Description of existing and current land improvement activity.

- a. Because the project only includes the renovation to the existing residence at this time, there are no proposed changes to the existing site conditions as referenced in Part B.1.a above.
- b. Due to the scope of the project, the renovation of the residence, the project's overall drainage pattern will remain the same and will not modify the existing drainage patterns through the site. Existing vegetation and site features will act as necessary for erosion and sediment control. The existing low-lying area/detention basin located in the southern portion of the property will detain runoff and shall be in accordance with the direction of the drainage flow direction arrows as specified in Drawing No. C-2, Grading, Drainage, and Stormwater Management Plan (Appendix A) Plan.

3. A plan which demonstrates the methods for sediment and erosion control. The plan should indicate the size, location and method for installation or implementation of the BMPs.

A Stormwater Management Plan can be found in Appendix A. Other BMPS's not shown on the plan may be necessary during the associated project improvements and are listed below in Part B.4.

4. Details and specifications for the proposed BMPs which describe their installation and maintenance procedures.

Drawing No. C-2, Grading, Drainage, and Stormwater Management Plan in Appendix A, identifies the location of the existing low-lying area/detention basin for erosion and stormwater controls. There are no current improvements within the total property and development area upon project completion. There are also no other proposed changes to the property or existing asphalt or gravel parking/driveway. The project consists entirely of the renovation of the existing residence and there will be no other construction activity at this time. The existing low-lying area/detention basin and natural features of the site will continue to act as the method for erosion and sediment control. The following best management practices (BMPs) are presented, in accordance with the City of McCall's DMGs and as listed in the Idaho Department of Environmental Quality's Catalog of Stormwater Best Management Practices for Idaho Cities and Counties:

- Timing of construction is critical when working in McCall and the surrounding area. Schedule and sequence construction work and erosion control applications so that they occur under optimal conditions that is, during periods when the potential for erosion is lowest, such as dry weather (Erosion and Sediment Control BMP 1).

- Protection of existing vegetation is prescribed for all areas to the greatest extent possible. If determined necessary, existing weeds should be maintained to provide an improved vegetated buffer to filter runoff and areas may need to be protected from vehicular traffic by standard construction or other fencing (Erosion and Sediment Control BMP 3).
- Establishment of clearing limits using standard construction fencing/staking or other means to help identify areas where construction activities (equipment, storage of materials, etc.) will not be allowed and existing vegetation can be preserved (Erosion and Sediment Control BMP 4).
- Stabilization of Construction Entrance/Exit will not be required as at this time as improvements are associated with the renovation of the exiting residence only (Erosion and Sediment Control BMP 5).
- The use of good housekeeping practices should be used, where applicable, during all aspects of the construction project (Erosion and Sediment Control BMPs 7 - 14).
 - BMP 7 Dust Control
 - BMP 8 Cover for Materials and Equipment
 - BMP 9 Stockpile Management
 - BMP 10 Spill Prevention and Control
 - BMP 11 Vehicle/Equipment Washing and Maintenance
 - BMP 12 Waste Management
 - BMP 13 Concrete Waste Management
 - BMP 14 Sanitary/Septic Waste Management
- In the event of earth disturbing activities, revegetate all exposed areas to provide stabilization and prevent sediment transport after construction is completed (Erosion and Sediment Control BMPs 15 - 23).
 - BMP 15 Mulching
 - BMP 16 Hydromulching
 - BMP 17 Geotextile
 - BMP 18 Matting
 - BMP 19 Soil Binders
 - BMP 20 Topsoiling
 - BMP 21 Seeding
 - BMP 22 Sodding
 - BMP 23 Planting
- Inlet protection will not be required. Runoff shall be contained and treated onsite during construction. (Erosion and Sediment Control BMP 31).

- Installation of silt fence or fiber rolls along portions of the perimeter of the development as shown on the Grading, Drainage, and Stormwater Management Plans, may be used within/adjacent to the property boundary at the low-lying area should earth disturbing activities be necessary to prevent sediment associated with runoff from offsite discharge. (Erosion and Sediment Control BMPs 35-36).
 - BMP 35 Fiber Rolls
 - BMP 36 Silt Fence
- Landscaping improvements and existing vegetation are intended to provide a vegetative buffer to filter, intercept, and detain stormwater runoff. Vegetative buffers reduce the flow and velocity of surface runoff, promote infiltration, and reduce pollutant discharge by capturing and holding sediments and other pollutants carried in runoff water (Erosion and Sediment Control BMP 37).
- The existing low-lying area/detention basin on the property is intended to capture, treat, retain, and infiltrate runoff generated from the proposed/existing development area. (Permanent Stormwater Control BMP 2).

5. A sequence and schedule of construction activities, including when erosion and sediment control devices and practices will be implemented. The sequence and schedule must include a timetable for project finish and a strategy for long term site stabilization and removal of temporary BMP's.

Permanent BMPs are currently existing on the site and shown within Appendix A, Drawing No. C-2, Grading, Drainage, and Stormwater Management Plan.

Housekeeping BMPs should be implemented when improvement activities begin and preservation of existing vegetation should be observed before, during and after any construction activities.

PART C: CONVEYANCE SYSTEM

It was determined that conveyance calculations for the project were not necessary because there are no existing conveyance facilities on the site or downstream of the area that will be affected by the development. Runoff from the existing residence currently drains away from the building to a vegetated existing low-lying area/detention basin as indicated within Appendix A, Drawing No. C-2, Grading, Drainage, and Stormwater Management Plan. The existing low-lying area/detention basin is estimated to have areas of $\pm 6"$ and is approximately at 6,175 C.F. to detain the first flush storm event as well as the difference in volume of the 10-year storm event. Because of this, no downstream conveyance is required.

PART D: FIRST FLUSH TREATMENT

A hydraulic analysis of the first flush for the proposed project was performed using the Simplified SCS Runoff Curve Number Method (TR-55) and rainfall intensity data presented in the conclusion of Memorandum "Recommended Modifications to the McCall Drainage Management Guidelines: First Flush Treatment and Storm Size Criteria", from Nathan Stewart, City Engineer, dated February 17, 2016. The Memorandum presents a recommendation for using a storm size of 0.81 inches for the 24-hour, 95% rainfall event first flush stormwater treatment.

Post-Development Drainage Area General Assumptions:

Table 1

Calculation Method:	SCS Curve Number Method, Hydraflow Hydrographs Extension for AutoCAD Civil 3D
Runoff Curve Number (CN):	80 (Composite)
Design Event:	24-hour rainfall event
Rainfall, P (24 Hour):	0.81 inches

The surface existing low-lying area/detention basin shown on Drawing No. C-2, Grading, Drainage and Stormwater Management Plan (Appendix A) is sized to capture and detain the first flush volume generated over its respective drainage area by the design storm.

The total post-development runoff volume of the 24-hour, 95% rainfall event for the total property boundary site is estimated to be 603 C.F. The total post-development runoff volume of the 24-hour, 95% rainfall event for the development area is estimated to be 379 C.F. The detention of the largest determined volume will be detained within the vegetated existing low-lying area/detention basin which is approximately 6" deep and located near the southeastern portion of the property. This existing low-lying area/detention basin is assumed to be almost ten (10) times the required amount as indicated by the City of McCall DMGs. Excess flows from storm events larger than the 10-year storm event will overflow from the low-lying and infiltrate across adjacent vegetated areas within the property boundary area.

The existing residence is located on the northern end of the property. In addition to the existing low-lying area/detention basin, the sites natural/native landscape areas of native grass and sagebrush will provide a sufficient vegetative buffer strip that will filter, as well as infiltrate stormwater runoff within the property boundary area. Refer to Drawing No. C-2, Grading, Drainage, and Stormwater Management Plan (Appendix A) for the existing low-lying area/detention basin location. Storm and design volumes are presented in Table 2 below. Runoff from the drainage area consists primarily of the asphalt and gravel

parking spaces and driveways, building roofs, and the existing residence, therefore, the passive pretreatment for sediment through the natural vegetation and the existing low-lying area/detention basin should provide the necessary treatment. Storm and design volumes are presented in Table 2.

Table 2

Drainage Area	Pretreatment	24-hour, 95% Storm Volume	Detention Design Volume
Total Property Boundary	Yes (passive)	603 C.F.	6,195 C.F.
Development Area	Yes (passive)	379 C.F.	6,195 C.F.

For the purpose of this analysis, the site was evaluated using pre-development conditions as a vacant vs. existing/post-development conditions to ensure that adequate first flush treatment was provided, and historic conditions were accounted for during the initial development of the project.

PART E: PERMANENT BMP'S

Permanent BMP's for the project includes the following:

- Existing native, re-planted vegetation and vegetative swales provide filtration of stormwater runoff between the project boundary area and adjacent properties.
- Onsite existing low-lying area/detention basin, landscaping, and re-vegetation provides nutrient uptake and natural filtration.

PART F: OPERATION AND MAINTENANCE

During construction, operation and maintenance of the Stormwater Management Plan will be the responsibility of the associated Contractor(s). This plan should be implemented in accordance with the Idaho Department of Environmental Quality's Catalog of Stormwater Best Management Practices for Idaho Cities and Counties and the City of McCall DMGs. All erosion and sediment controls including stormwater treatment facilities shall be inspected weekly during construction. Additional inspections should be completed in anticipation of, and immediately following event-based runoff events (spring snow melt/significant precipitation events). If implemented, construction areas with excess sediment build-up around fiber rolls and/or silt fence should be cleaned at the time of inspections.

Revegetated areas should be monitored for successful vegetation generation. Areas that remain exposed and/or may become eroded shall be stabilized immediately with mulch and/or straw blankets.

Adjustments to the stormwater management plan should be made by the Contractor in the event that excessive erosion continues to occur at the site during construction. After construction of the site improvements, upon final stabilization of the site and acceptance by the Owner, the Owner will assume responsibility for the operation and maintenance of the stormwater BMPs.

SECTION III
STORMWATER CALCULATIONS



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 McCALL, IDAHO 83638
 208.634.4140 · 208.634-4146 FAX

PROJECT: Northwest Passage Bed and Breakfast

CLIENT: Colby Peters

JOB NO.: 18037 **DATE:** January 10, 2019

BY: AMD

REVISION DATE: January 17, 2019

RE: Northwest Passage Bed and Breakfast - Stormwater Calculations

Drainage Area Calculations

Drainage Areas	(ft²)	(Acres)
Total Property Boundary	211,739.0	4.86
Development Area	105,869.5	2.43

Pre-Development: Total Property Boundary Area Surfaces	(ft²)	(Acres)	(%)
Woods (CN=73)	211,739.0	4.86	100.00%
	211,739.0	4.86	100.00%

Total Impervious Surface Area = 0.0 0.00 0.00%

Post-Development: Total Property Boundary Area Surfaces	(ft²)	(Acres)	(%)
Building Roofs (CN=98)	3,914.2	0.090	1.85%
Asphalt Parking/Driveway (CN=98)	5,405.0	0.124	2.55%
Gravel Parking/Driveway (CN=89)	7,771.4	0.178	3.67%
Open Space (CN=79)	194,648.4	4.468	91.93%
	211,739.0	4.86	100.00%

Total Impervious Surface Area = 9,319.2 0.21 4.40%

Pre-Development: Development Area Surfaces	(ft²)	(Acres)	(%)
Woods (CN=73)	105,869.5	2.430	100.00%
	105,869.5	2.43	100.00%

Total Impervious Surface Area = 0.0 0.00 0.00%

Post-Development: Development Area Surfaces	(ft²)	(Acres)	(%)
Building Roofs (CN=98)	3,914.2	0.090	3.70%
Asphalt Parking/Driveway (CN=98)	5,405.0	0.124	5.11%
Gravel Parking/Driveway (CN=89)	7,771.4	0.178	7.34%
Open Space (CN=79)	88,778.9	2.038	83.86%
	105,869.5	2.43	100.00%

Total Impervious Surface Area = 9,319.2 0.21 8.80%

Drainage Area Flow Paths

	Length	Elevation Change	Slope
	(ft)	(ft)	(%)
Pre-Development Flow Path (Total Property Boundary):			
Sheet Flow (n=0.40, Light Underbrush Woods)	231.0	21.9	9.50%
Sheet Flow (n=0.40, Light Underbrush Woods)	69.0	1.5	2.20%
Shallow Concentrated Flow (Unpaved)	329.0	3.6	1.10%

Total Length/Average Slope = 629.0 27.1 4.31%

	Length	Elevation Change	Slope
	(ft)	(ft)	(%)
Post-Development Flow Path (Total Property Boundary):			
Sheet Flow (n=0.24, Grass, Dense Grasses)	231.0	21.9	9.50%
Sheet Flow (n=0.24, Grass, Dense Grasses)	69.0	1.5	2.20%
Shallow Concentrated Flow (Unpaved)	329.0	3.6	1.10%

Total Length/Average Slope = 629.0 27.1 4.31%

	Length	Elevation Change	Slope
	(ft)	(ft)	(%)
Pre-Development Flow Path (Development Area):			
Sheet Flow (n=0.40, Light Underbrush Woods)	231.0	21.9	9.50%
Sheet Flow (n=0.40, Light Underbrush Woods)	69.0	1.5	2.20%
Shallow Concentrated Flow (Unpaved)	55.0	0.6	1.10%

Total Length/Average Slope = 355.0 24.1 6.78%

	Length	Elevation Change	Slope
	(ft)	(ft)	(%)
Post-Development Flow Path (Development Area):			
Sheet Flow (n=0.24, Grass, Dense Grasses)	231.0	21.9	9.50%
Sheet Flow (n=0.24, Grass, Dense Grasses)	69.0	1.5	2.20%
Shallow Concentrated Flow (Unpaved)	55.0	0.6	1.10%

Total Length/Average Slope = 355.0 24.1 6.78%



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CLIENT: Colby Peters

JOB NO.: 18037 **DATE:** January 10, 2019

BY: AMD

REVISION DATE: _____

RE: Northwest Passage Bed and Breakfast - Stormwater Calculations

Stormwater Detention Basin Volume Calculations

Stormwater Detention Basin No. 1	Elev. (ft)	Height (ft)	Area (ft ²)	Volume (ft ³)
Top	5040.00		24,778.3	
Bottom	5039.50	0.50	0.0	6,195
				6,195

Total Proposed Detention Volume = 6,195 (ft³)

Required Water Quality Detention Volume = 603 (ft³)

Proposed stormwater detention is greater than the required water quality detention volume and therefore, storage is adequate.

Notes:

1. The total proposed detention volume shown represents the minimum storage volume of each detention basin. As peak flows are routed through each basin additional storage will be attained as stormwater flows are restricted by individual overflows should it release/drain to downslope areas.



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REVISION DATE: January 17, 2019

RE: Northwest Passage Bed and Breakfast - Stormwater Calculations

Stormwater Detention Storage/Peak Flow Attenuation Summary Tables

Drainage Area	Area (Acres)	Area (ft ²)	Post-Dev. 1 st Flush Runoff Volume (V _{1st-Post}) (ft ³)	Pre-Dev. 10 Year Runoff Volume (V _{10-Pre}) (ft ³)	Post-Dev. 10 Year Runoff Volume (V _{10-Post}) (ft ³)	V _{10-Post} - V _{10-Pre} (ft ³)	Required Storage Volume (ft ³)	Proposed Storage (ft ³)
Total Property Boundary	4.86	211,739	603	4,388	8,148	3,760	603	6,195
Development Area	2.43	105,869	379	2,182	4,386	2,204	379	6,195

Notes:

- See Hydrograph Report pages 1 - 10 for runoff calculations.

Drainage Area	Area (Acres)	Area (ft ²)	Pre-Dev. Peak Discharge (Q _{10-Pre}) (ft ³ /s)	Post-Dev. Peak Discharge (Q _{10-Post}) (ft ³ /s)	Post-Dev. Peak Discharge w/Detention (Q _{10Post}) (ft ³ /s)
Total Property Boundary	4.86	211,739	0.31	1.32	
Development Area	2.43	105,869	0.20	0.76	

Notes:

- See Hydrograph Report pages 1 - 10 for runoff calculations.

Hydrograph Report

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

Thursday, 01 / 17 / 2019

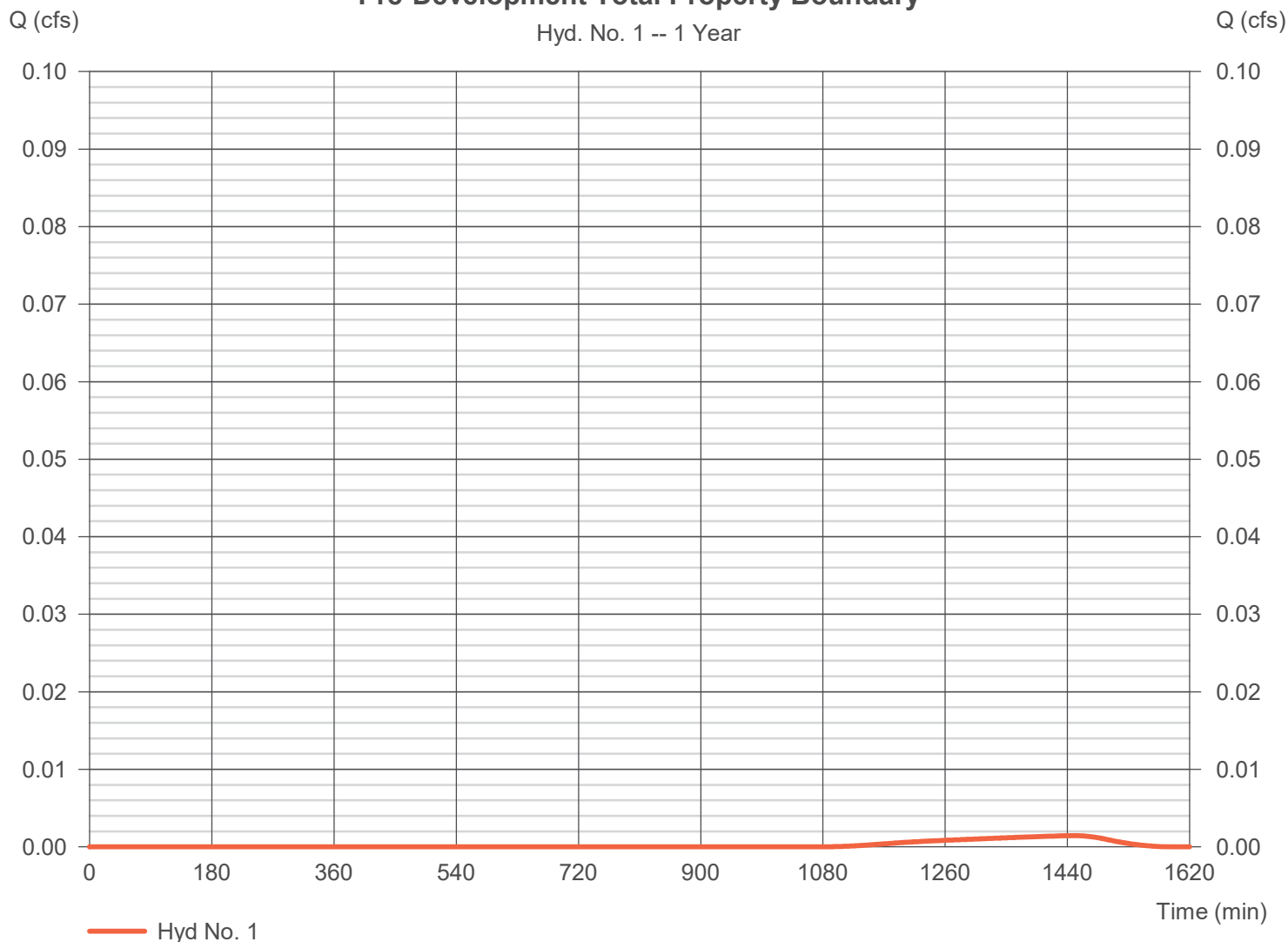
Hyd. No. 1

Pre-Development Total Property Boundary

Hydrograph type	= SCS Runoff	Peak discharge	= 0.001 cfs
Storm frequency	= 1 yrs	Time to peak	= 1447 min
Time interval	= 1 min	Hyd. volume	= 23 cuft
Drainage area	= 4.860 ac	Curve number	= 73*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 97.00 min
Total precip.	= 0.81 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(4.860 x 73)] / 4.860

Pre-Development Total Property Boundary



Hydrograph Report

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Thursday, 01 / 17 / 2019

Hyd. No. 1

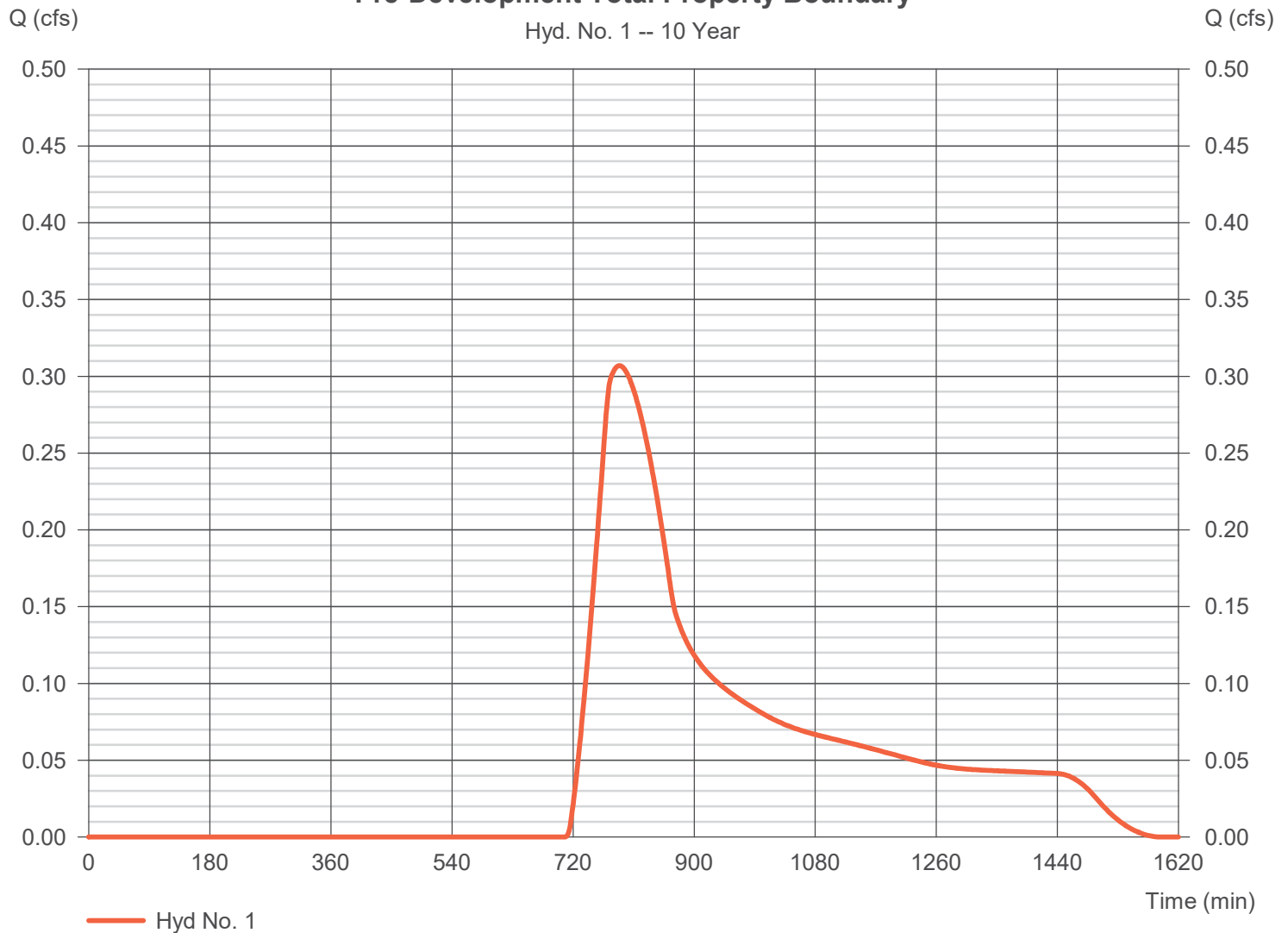
Pre-Development Total Property Boundary

Hydrograph type	= SCS Runoff	Peak discharge	= 0.307 cfs
Storm frequency	= 10 yrs	Time to peak	= 789 min
Time interval	= 1 min	Hyd. volume	= 4,388 cuft
Drainage area	= 4.860 ac	Curve number	= 73*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 97.00 min
Total precip.	= 1.83 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(4.860 x 73)] / 4.860

Pre-Development Total Property Boundary

Hyd. No. 1 -- 10 Year



Hydrograph Report

Hyd. No. 2

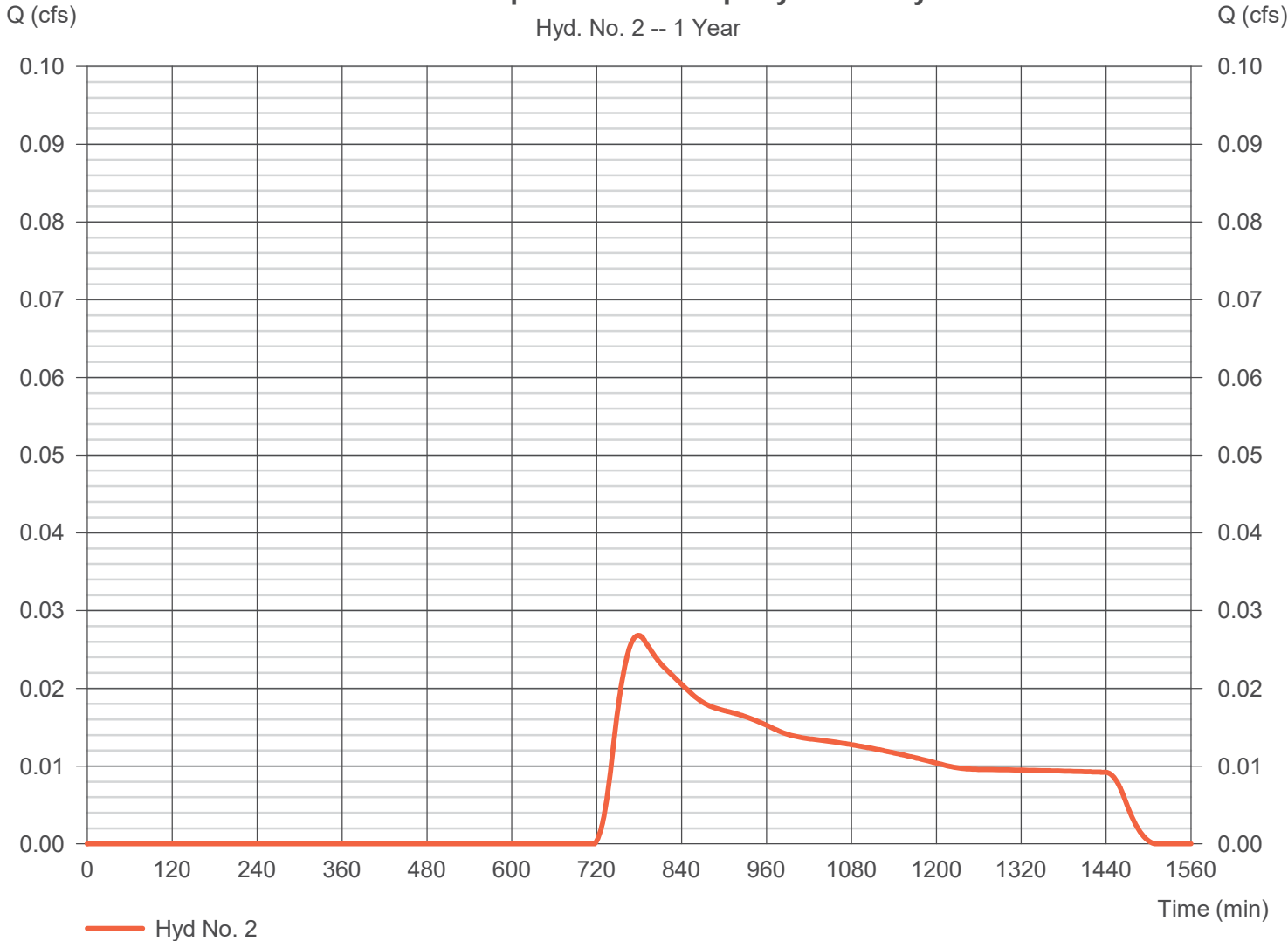
Post-Development Total Property Boundary

Hydrograph type	= SCS Runoff	Peak discharge	= 0.027 cfs
Storm frequency	= 1 yrs	Time to peak	= 779 min
Time interval	= 1 min	Hyd. volume	= 603 cuft
Drainage area	= 4.860 ac	Curve number	= 80*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 45.60 min
Total precip.	= 0.81 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(0.090 x 98) + (0.124 x 98) + (0.178 x 89) + (4.468 x 79)] / 4.860

Post-Development Total Property Boundary

Hyd. No. 2 -- 1 Year



TR55 Tc Worksheet

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

Hyd. No. 2

Post-Development Total Property Boundary

<u>Description</u>	<u>A</u>	<u>B</u>	<u>C</u>	<u>Totals</u>
Sheet Flow				
Manning's n-value	= 0.240	0.240	0.011	
Flow length (ft)	= 231.0	69.0	0.0	
Two-year 24-hr precip. (in)	= 1.13	1.13	0.00	
Land slope (%)	= 9.50	2.20	0.00	
Travel Time (min)	= 25.16	+ 17.18	+ 0.00	= 42.34
Shallow Concentrated Flow				
Flow length (ft)	= 329.00	0.00	0.00	
Watercourse slope (%)	= 1.10	0.00	0.00	
Surface description	= Unpaved	Paved	Paved	
Average velocity (ft/s)	=1.69	0.00	0.00	
Travel Time (min)	= 3.24	+ 0.00	+ 0.00	= 3.24
Channel Flow				
X sectional flow area (sqft)	= 0.00	0.00	0.00	
Wetted perimeter (ft)	= 0.00	0.00	0.00	
Channel slope (%)	= 0.00	0.00	0.00	
Manning's n-value	= 0.015	0.015	0.015	
Velocity (ft/s)	=0.00	0.00	0.00	
Flow length (ft)	({0})0.0	0.0	0.0	
Travel Time (min)	= 0.00	+ 0.00	+ 0.00	= 0.00
Total Travel Time, Tc				45.60 min

Hydrograph Report

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

Thursday, 01 / 17 / 2019

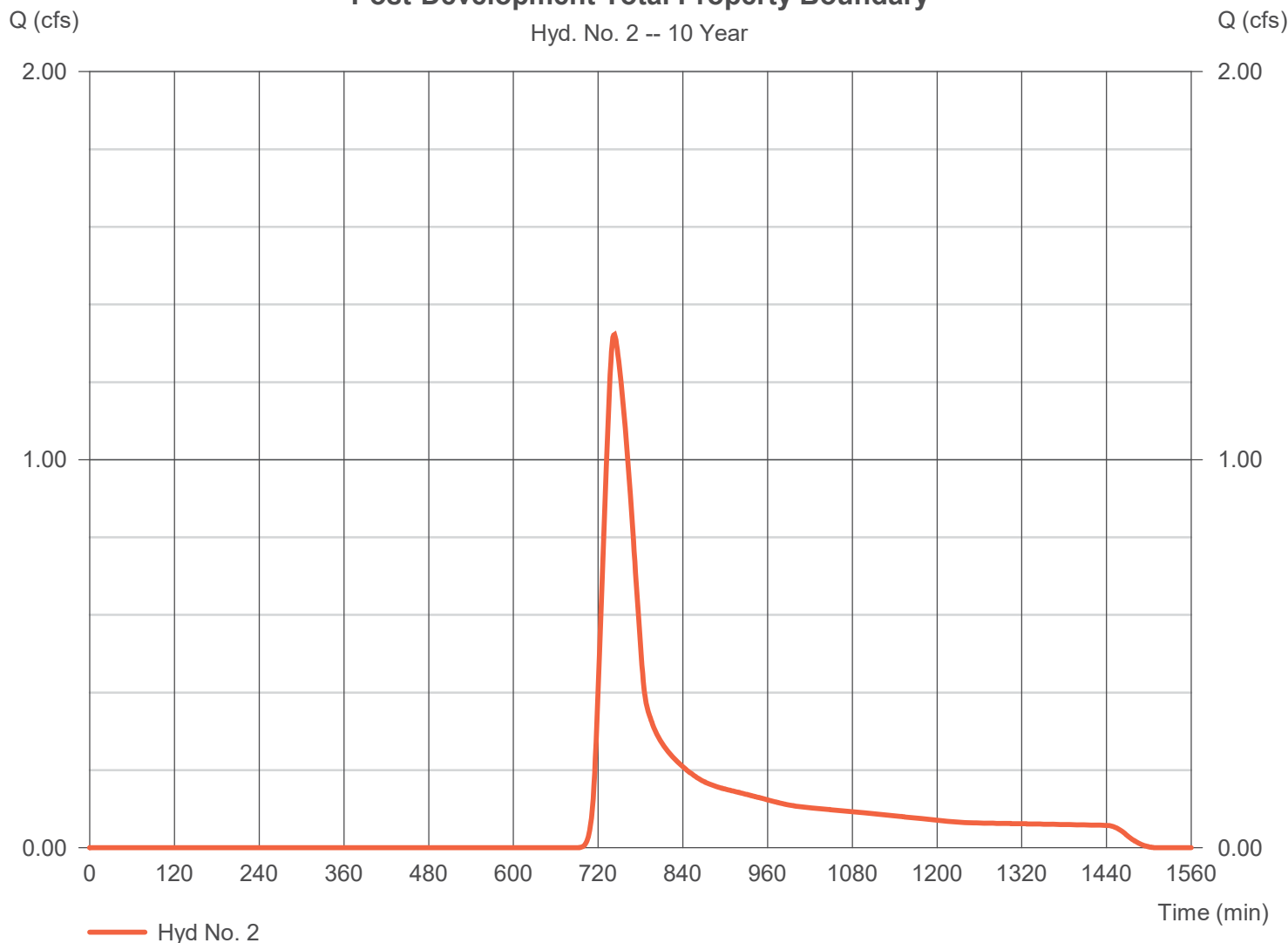
Hyd. No. 2

Post-Development Total Property Boundary

Hydrograph type	= SCS Runoff	Peak discharge	= 1.322 cfs
Storm frequency	= 10 yrs	Time to peak	= 743 min
Time interval	= 1 min	Hyd. volume	= 8,148 cuft
Drainage area	= 4.860 ac	Curve number	= 80*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 45.60 min
Total precip.	= 1.83 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(0.090 x 98) + (0.124 x 98) + (0.178 x 89) + (4.468 x 79)] / 4.860

Post-Development Total Property Boundary



Hydrograph Report

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

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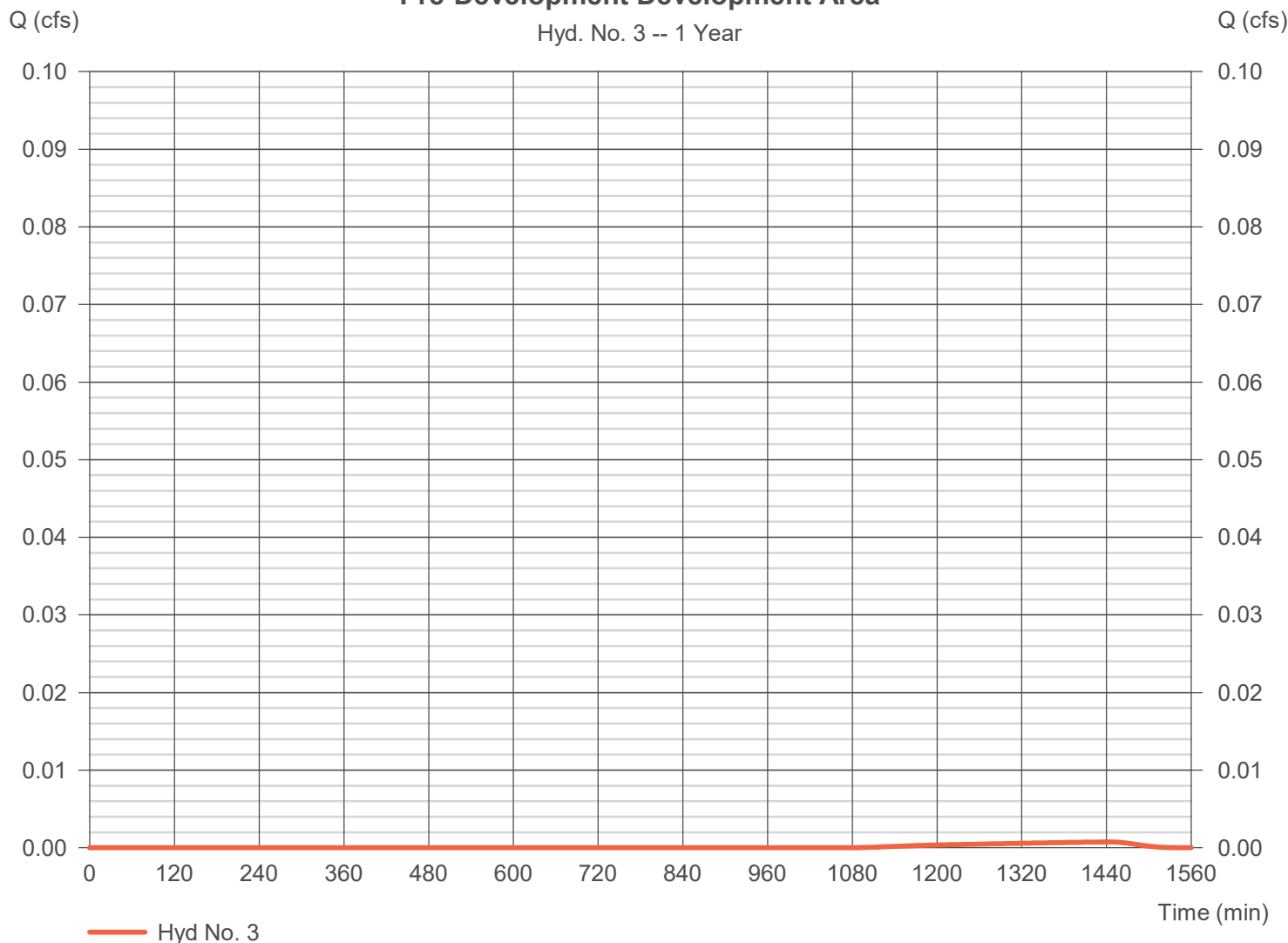
Hyd. No. 3

Pre-Development Development Area

Hydrograph type	= SCS Runoff	Peak discharge	= 0.001 cfs
Storm frequency	= 1 yrs	Time to peak	= 1442 min
Time interval	= 1 min	Hyd. volume	= 12 cuft
Drainage area	= 2.430 ac	Curve number	= 73*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 64.30 min
Total precip.	= 0.81 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(2.430 x 73)] / 2.430

Pre-Development Development Area



Hydrograph Report

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

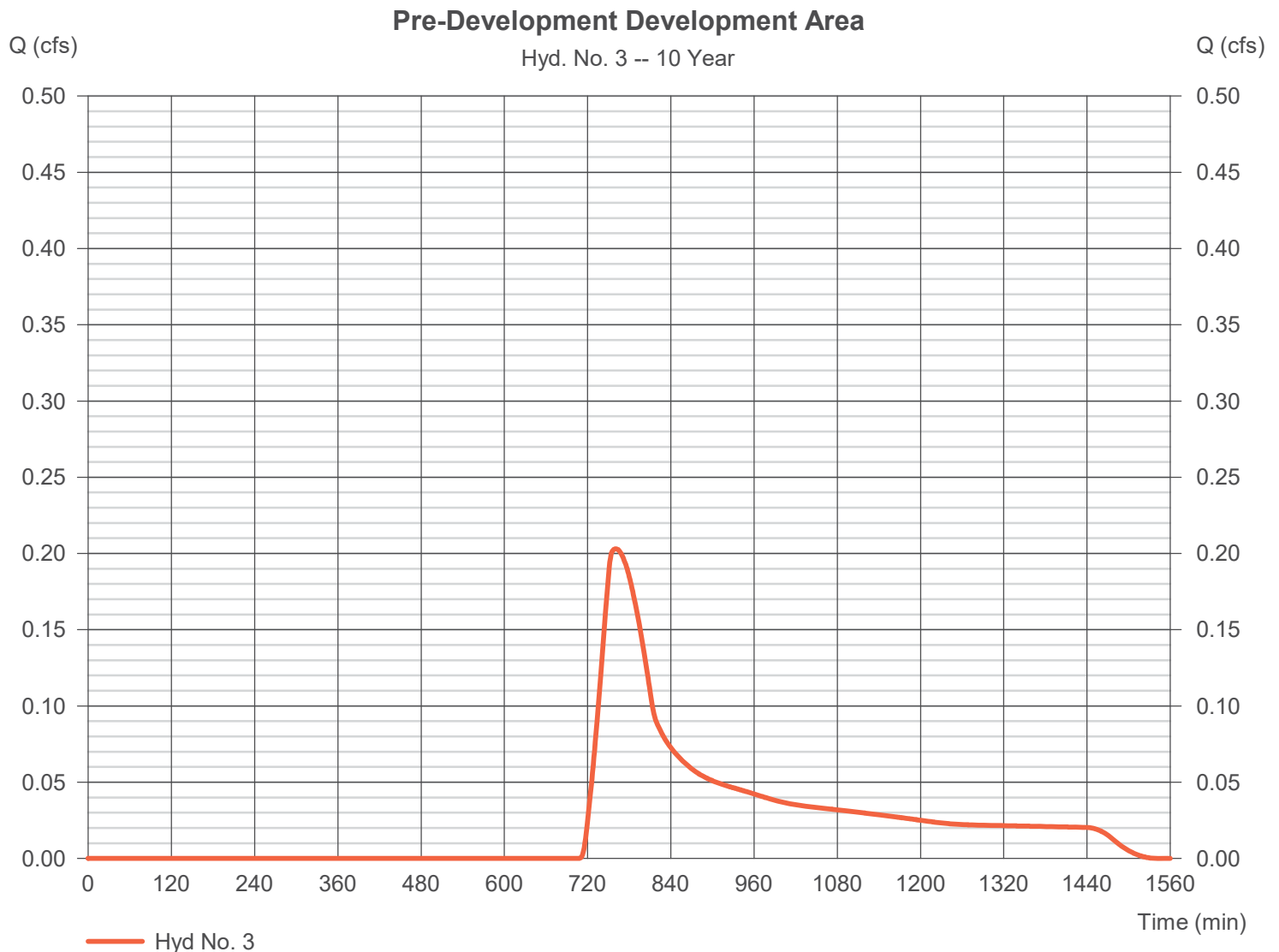
Thursday, 01 / 17 / 2019

Hyd. No. 3

Pre-Development Development Area

Hydrograph type	= SCS Runoff	Peak discharge	= 0.203 cfs
Storm frequency	= 10 yrs	Time to peak	= 761 min
Time interval	= 1 min	Hyd. volume	= 2,182 cuft
Drainage area	= 2.430 ac	Curve number	= 73*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 64.30 min
Total precip.	= 1.83 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = $[(2.430 \times 73)] / 2.430$



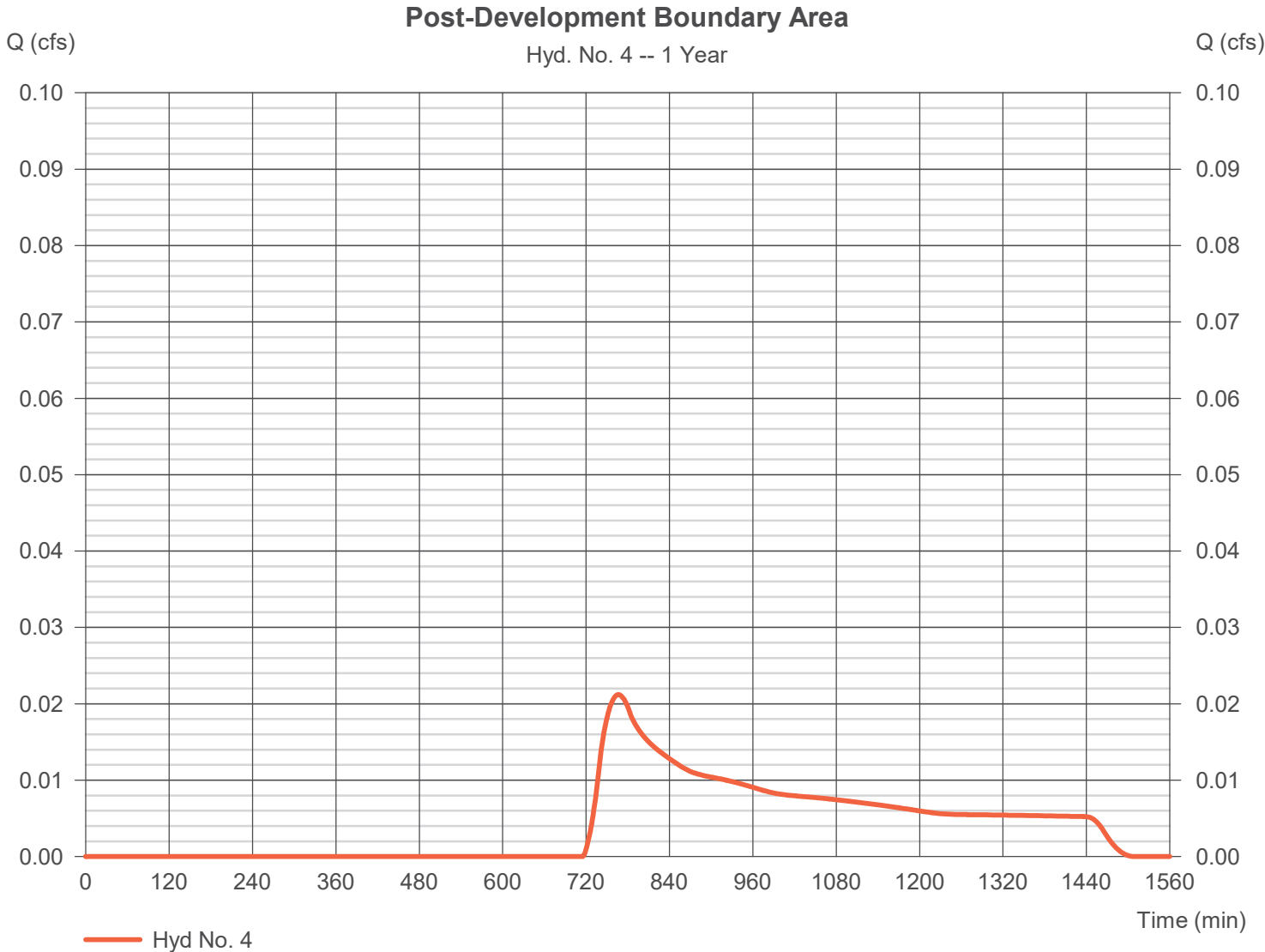
Hydrograph Report

Hyd. No. 4

Post-Development Boundary Area

Hydrograph type	= SCS Runoff	Peak discharge	= 0.021 cfs
Storm frequency	= 1 yrs	Time to peak	= 766 min
Time interval	= 1 min	Hyd. volume	= 379 cuft
Drainage area	= 2.430 ac	Curve number	= 81*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 42.90 min
Total precip.	= 0.81 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

* Composite (Area/CN) = [(0.090 x 98) + (0.124 x 98) + (0.178 x 89) + (2.038 x 79)] / 2.430



TR55 Tc Worksheet

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

Hyd. No. 4

Post-Development Boundary Area

<u>Description</u>	<u>A</u>	<u>B</u>	<u>C</u>	<u>Totals</u>
Sheet Flow				
Manning's n-value	= 0.240	0.240	0.011	
Flow length (ft)	= 231.0	69.0	0.0	
Two-year 24-hr precip. (in)	= 1.13	1.13	0.00	
Land slope (%)	= 9.50	2.20	0.00	
Travel Time (min)	= 25.16	+ 17.18	+ 0.00	= 42.34
Shallow Concentrated Flow				
Flow length (ft)	= 55.00	0.00	0.00	
Watercourse slope (%)	= 1.10	0.00	0.00	
Surface description	= Unpaved	Paved	Paved	
Average velocity (ft/s)	=1.69	0.00	0.00	
Travel Time (min)	= 0.54	+ 0.00	+ 0.00	= 0.54
Channel Flow				
X sectional flow area (sqft)	= 0.00	0.00	0.00	
Wetted perimeter (ft)	= 0.00	0.00	0.00	
Channel slope (%)	= 0.00	0.00	0.00	
Manning's n-value	= 0.015	0.015	0.015	
Velocity (ft/s)	=0.00	0.00	0.00	
Flow length (ft)	({0})0.0	0.0	0.0	
Travel Time (min)	= 0.00	+ 0.00	+ 0.00	= 0.00
Total Travel Time, Tc				42.90 min

Hydrograph Report

Hydraflow Hydrographs Extension for AutoCAD® Civil 3D® 2018 by Autodesk, Inc. v12

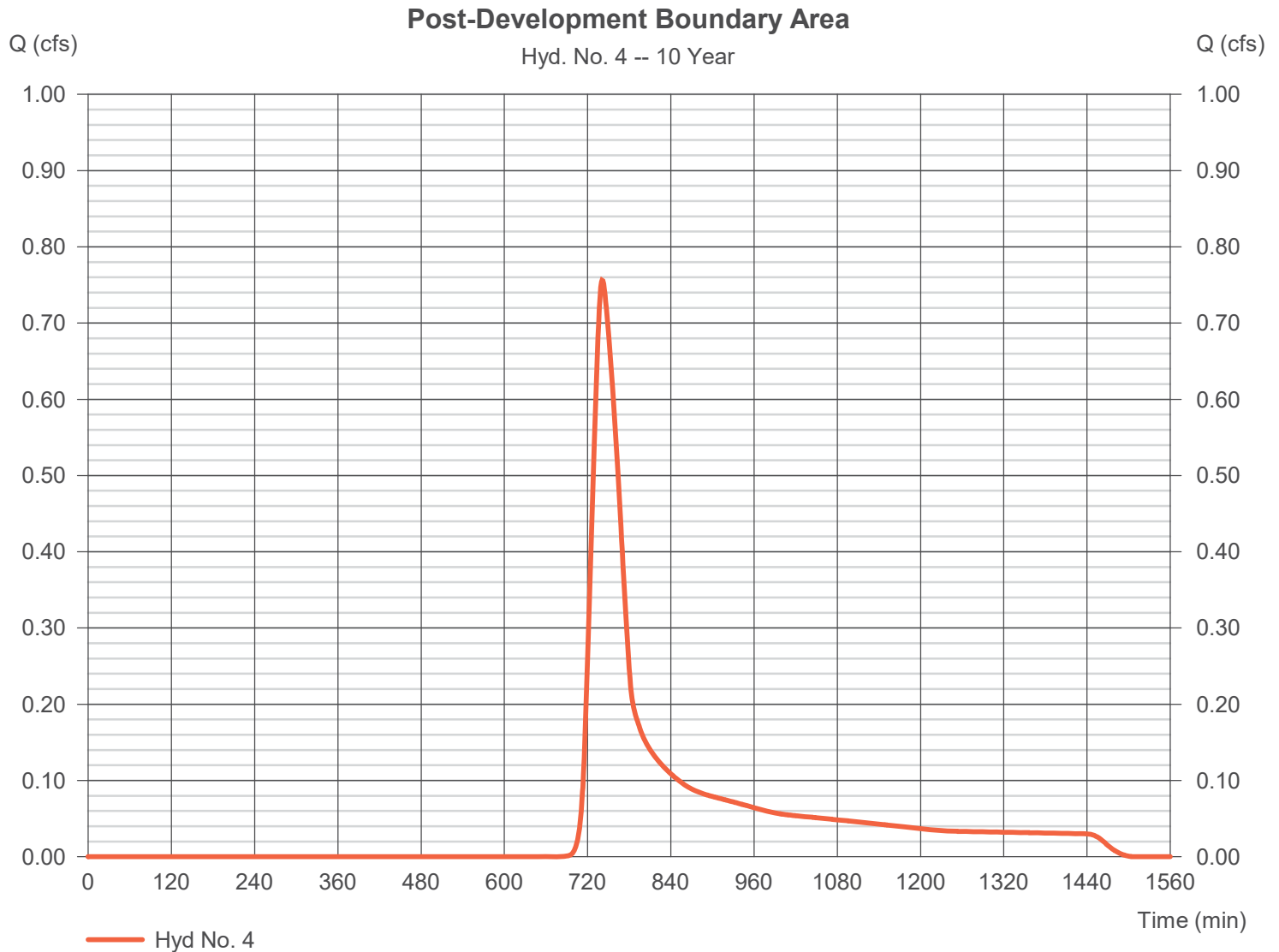
Thursday, 01 / 17 / 2019

Hyd. No. 4

Post-Development Boundary Area

Hydrograph type	= SCS Runoff	Peak discharge	= 0.756 cfs
Storm frequency	= 10 yrs	Time to peak	= 741 min
Time interval	= 1 min	Hyd. volume	= 4,386 cuft
Drainage area	= 2.430 ac	Curve number	= 81*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 42.90 min
Total precip.	= 1.83 in	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

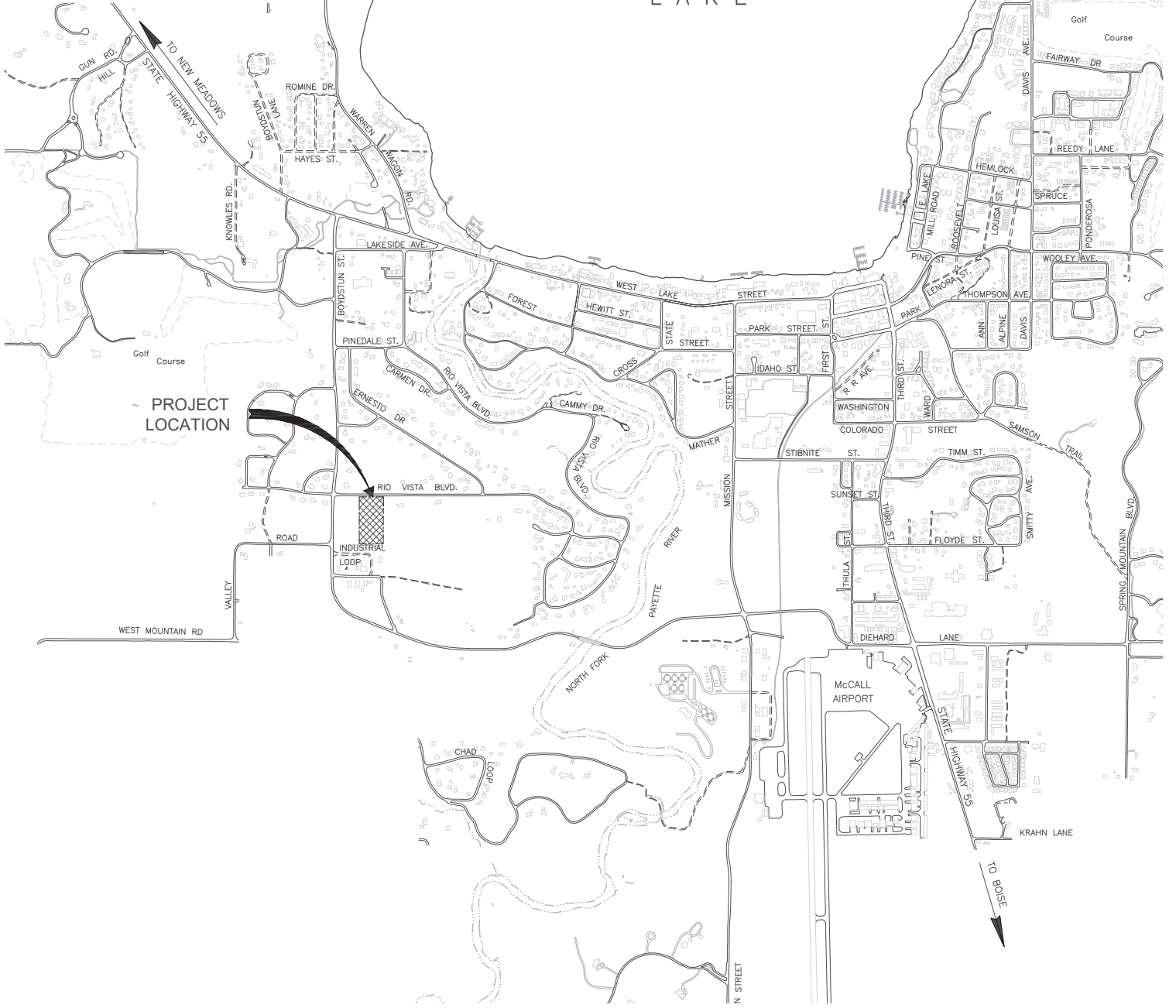
* Composite (Area/CN) = [(0.090 x 98) + (0.124 x 98) + (0.178 x 89) + (2.038 x 79)] / 2.430



APPENDIX A
FIGURES/DRAWINGS



PAYETTE LAKE



NORTH
SCALE: 1" = 2,000'

CRESTLINE
ENGINEERS

323 DEINHARD LANE, SUITE C · PO BOX 2330
McCALL, IDAHO 83638
208.634.4140 · 208.634.4146 FAX




NORTHWEST PASSAGE BED AND BREAKFAST
McCALL, IDAHO
VICINITY MAP

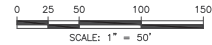
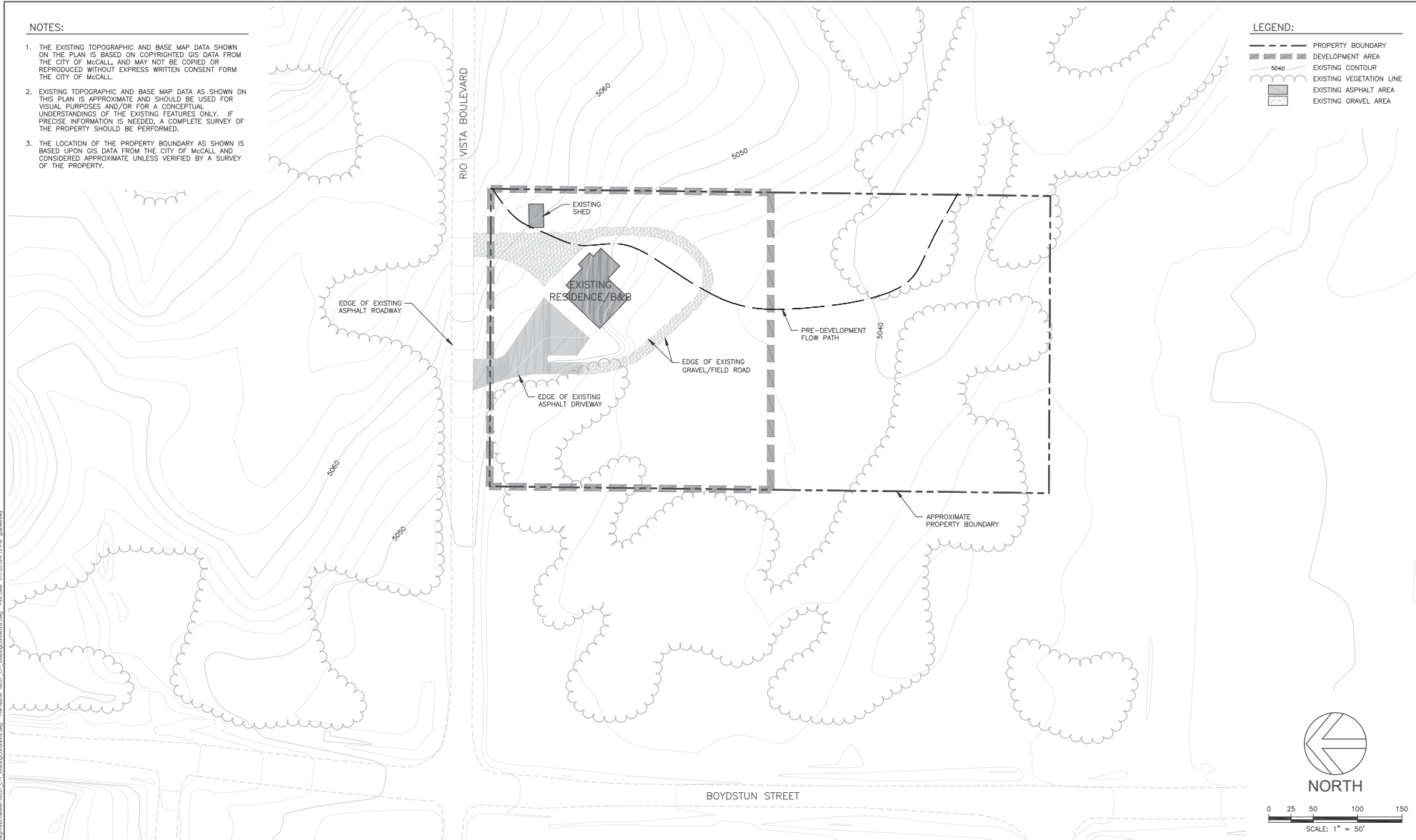
PROJECT	18037	DRAWN	FIGURE NO.
DATE	1/10/2019	AMD	1 OF 1

NOTES:

1. THE EXISTING TOPOGRAPHIC AND BASE MAP DATA SHOWN ON THE PLAN IS BASED ON COPYRIGHTED GIS DATA FROM THE CITY OF McCALL, AND MAY NOT BE COPIED OR REPRODUCED WITHOUT EXPRESS WRITTEN CONSENT FROM THE CITY OF McCALL.
2. EXISTING TOPOGRAPHIC AND BASE MAP DATA AS SHOWN ON THIS PLAN IS APPROXIMATE AND SHOULD BE USED FOR VISUAL PURPOSES AND/OR FOR A CONCEPTUAL UNDERSTANDING OF THE EXISTING FEATURES ONLY. IF PRECISE INFORMATION IS NEEDED, A COMPLETE SURVEY OF THE PROPERTY SHOULD BE PERFORMED.
3. THE LOCATION OF THE PROPERTY BOUNDARY AS SHOWN IS BASED UPON GIS DATA FROM THE CITY OF McCALL AND CONSIDERED APPROXIMATE UNLESS VERIFIED BY A SURVEY OF THE PROPERTY.

LEGEND:

-  PROPERTY BOUNDARY
-  DEVELOPMENT AREA
-  EXISTING CONTOUR
-  EXISTING VEGETATION LINE
-  EXISTING ASPHALT AREA
-  EXISTING GRAVEL AREA



NO.	REVISION	BY	DATE	DESIGN
1	UPDATE PER ENGINEERING REVIEW COMMENTS	GTT	1/17/2019	AMD
				DRAWN
				AMD
				CHECKED
				GTT
				APPROVED
				GTT

CRESTLINE
ENGINEERS
 323 DEINHARD LANE, SUITE C · PO BOX 2330
 McCALL, IDAHO 83638
 208.634.4140 · 208.634.4146 FAX

NORTHWEST PASSAGE BED AND BREAKFAST
 McCALL, IDAHO
 EXISTING CONDITIONS WITH SITE PLAN

VERIFY SCALE	
BAR IS ONE INCH ON FULL SIZE DRAWING 1"	
PROJECT	18037
DATE	1/10/2019
DRAWING NO.	SHEET NO.
C-1	1 OF 2

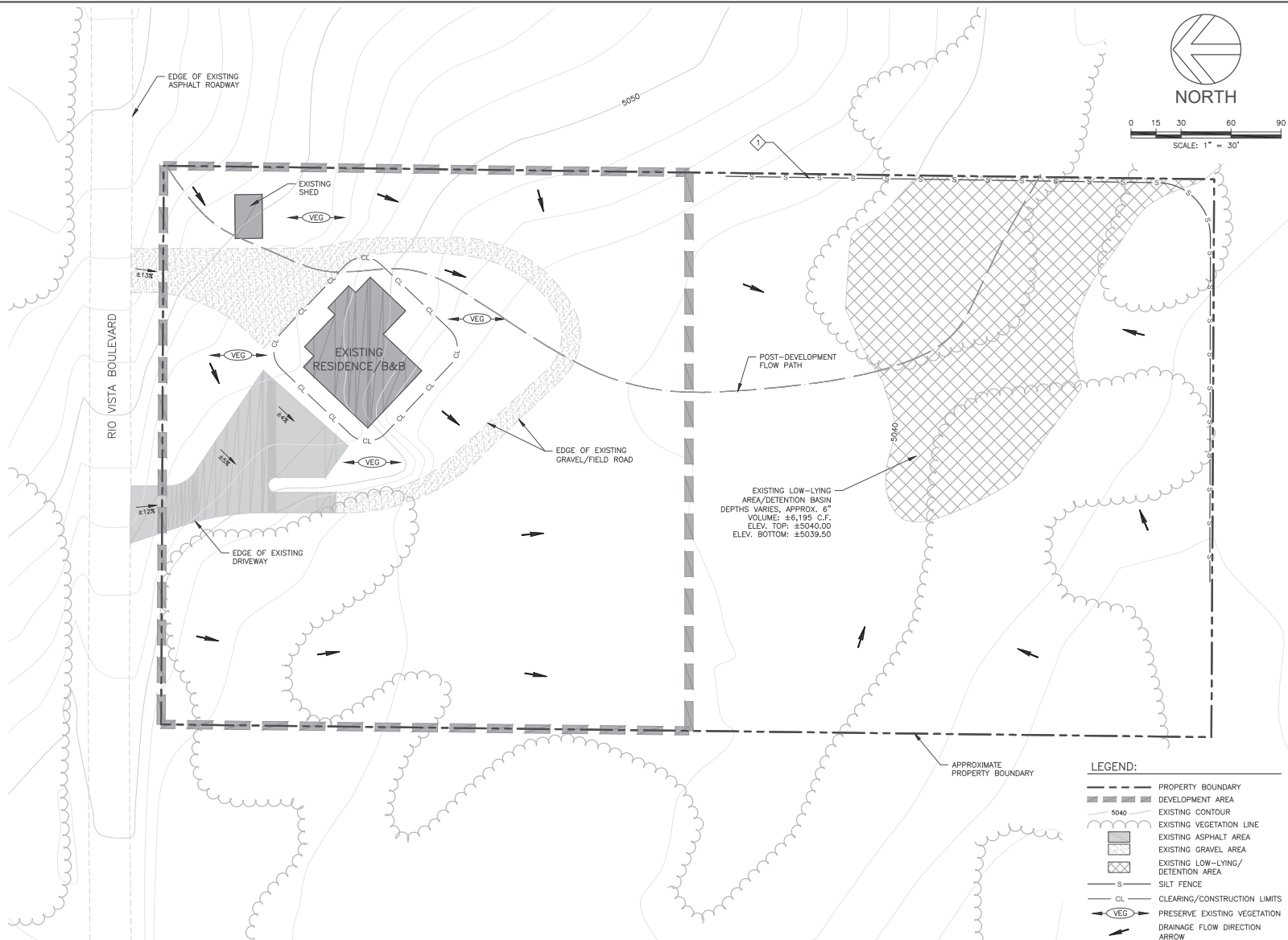
Path: J:\Projects\18037\CD\Design-Schematics\18037_C-1_ExistingConditions.dwg File Name: 18037_C-1_ExistingConditions.dwg Plt Date: 1/10/2019 4:12 PM gdmw/ky

NOTES:

1. THE EXISTING TOPOGRAPHIC AND BASE MAP DATA SHOWN ON THE PLAN IS BASED ON COPYRIGHTED GIS DATA FROM THE CITY OF McCALL AND MAY NOT BE COPIED OR REPRODUCED WITHOUT EXPRESS WRITTEN CONSENT FROM THE CITY OF McCALL.
2. EXISTING TOPOGRAPHIC AND BASE MAP DATA AS SHOWN ON THIS PLAN IS APPROXIMATE AND SHOULD BE USED FOR VISUAL PURPOSES AND/OR FOR A CONCEPTUAL UNDERSTANDING OF THE EXISTING FEATURES ONLY. IF PRECISE INFORMATION IS NEEDED, A COMPLETE SURVEY OF THE PROPERTY SHOULD BE PERFORMED.
3. THE LOCATION OF THE PROPERTY BOUNDARY AS SHOWN IS BASED UPON GIS DATA FROM THE CITY OF McCALL AND CONSIDERED APPROXIMATE UNLESS VERIFIED BY A SURVEY OF THE PROPERTY.
4. THE EXISTING SITE INFORMATION IS PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR'S CONSTRUCTION SURVEY PRIOR TO THE START OF ANY PROJECT CONSTRUCTION. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF THE EXISTENCE OF OBJECTS OR UTILITIES ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THESE DRAWINGS.
5. REFER TO THE "STATE OF IDAHO, CATALOG OF STORMWATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES" FOR FURTHER DETAILS ON BMP IMPLEMENTATION AND INSTALLATION.
6. ALL EROSION AND SEDIMENT CONTROL BMP'S SHALL BE INSTALLED PRIOR TO THE START OF ANY PROJECT CONSTRUCTION OR EARTH DISTURBING ACTIVITIES AND SHOULD REMAIN IN PLACE UNTIL ALL DISTURBED/EXPOSED AREAS HAVE BEEN STABILIZED AND/OR REVEGETATED.
7. THE OWNER AND/OR THE SELECTED CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL BMP'S IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REQUIREMENTS.
8. THE IMPLEMENTATION OF THESE EROSION AND SEDIMENT CONTROL MEASURES INCLUDING INSTALLATION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THIS PLAN IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL PROJECT CONSTRUCTION IS COMPLETED AND APPROVED BY THE OWNER. THE OWNER SHALL BE RESPONSIBLE FOR ALL MAINTENANCE AFTER THE PROJECT IS APPROVED.
9. WATTLES MAY BE USED IN PLACE OF SILT FENCE WHERE DETERMINED APPROPRIATE. SILT FENCE HAS BEEN SHOWN ON THE PROPERTY LINES IN SOME AREAS TO PREVENT ENCROACHMENT ONTO NEIGHBORING PROPERTIES.
10. WORK ACTIVITIES SHALL TAKE PLACE WITHIN THE CLEARING LIMITS AS SHOWN ON THIS PLAN. CONTRACTOR SHALL PRESERVE NATURAL VEGETATION OUTSIDE OF CLEARING LIMITS AT ALL TIMES.
11. STAGING AREA(S) TO BE LOCATED BY CONTRACTOR INCLUDING PORTABLE TOILETS, GARBAGE RECEPTACLES, CONCRETE WASHOUT, AND ALL OTHER CONTRACTOR FACILITIES.
12. ALL SITE GRADING ADJACENT TO BUILDINGS SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING AT A MINIMUM OF 1:2% IN HARDSCAPE AREAS AND 4% IN LANDSCAPE AREAS.
13. DRIVEWAY GRADES SHALL BE SLOPED AWAY FROM THE BUILDINGS AT A MINIMUM SLOPE OF 2% AND A MAXIMUM SLOPE OF 8% FOR A DISTANCE OF NO LESS THAN TEN (10) FEET. GRADING OF THE DRIVEWAY SHALL BE IN ACCORDANCE WITH THE DIRECTION OF THE DRAINAGE FLOW DIRECTION ARROWS AS SPECIFIED IN THE STORMWATER MANAGEMENT PLAN.
14. AREAS BETWEEN BUILDINGS AND PROPERTY BOUNDARIES SHALL BE SLOPED TO INSURE RUNOFF IS KEPT ON-SITE. SWALES SHALL BE CONSTRUCTED ADJACENT TO NEAR-SIDE PROPERTY LINES TO PREVENT RUNOFF FROM FLOWING ONTO ADJOINING PROPERTIES. THESE SWALES ARE INTENDED TO BE FIELD FIT AND MEANDERED AROUND EXISTING VEGETATION AND SITE FEATURES AS NECESSARY.
15. REVEGETATION AND STABILIZATION OF ALL DISTURBED PROJECT AREAS SHALL BE IN ACCORDANCE WITH THE PROJECT'S LANDSCAPE DESIGN. IF A LANDSCAPE DESIGN/PLAN IS NOT AVAILABLE, DISTURBED AREAS SHALL BE REVEGETATED WITH A GRASS MIXTURE NATIVE TO THAT AREA.

KEY NOTES:

- ◇ SILT FENCE OR WATTLES TO BE USED WITHIN/ADJACENT TO THE PROPERTY BOUNDARY AT LOW-LYING AREA SHOULD EARTH DISTURBING ACTIVITIES BE NECESSARY.



LEGEND:

- PROPERTY BOUNDARY
- DEVELOPMENT AREA
- 5040 EXISTING CONTOUR
- EXISTING VEGETATION LINE
- EXISTING ASPHALT AREA
- EXISTING GRAVEL AREA
- EXISTING LOW-LYING/ DETENTION AREA
- S SILT FENCE
- CL CLEARING/CONSTRUCTION LIMITS
- VEG PRESERVE EXISTING VEGETATION
- DRAINAGE FLOW DIRECTION ARROW

NO.	REVISION	BY	DATE	DESIGN
1	UPDATE PER ENGINEERING REVIEW COMMENTS	GTT	1/17/2019	AMD
				DRAWN
				AMD
				CHECKED
				GTT
				APPROVED
				GTT

CRESTLINE ENGINEERS
 323 DEINHARD LANE, SUITE C · PO BOX 2330
 McCALL, IDAHO 83638
 208.634.4140 · 208.634.4146 FAX

NORTHWEST PASSAGE BED AND BREAKFAST
 McCALL, IDAHO
 GRADING, DRAINAGE, AND
 STORMWATER MANAGEMENT PLAN

VERIFY SCALE	
PROJECT	18037
DATE	1/10/2019
DRAWING NO.	SHEET NO.
C-2	2 OF 2

Path: J:\Projects\18037\CD\Drawings\Stormwater\18037 Stormwater.dwg File Name: 18037 Stormwater.dwg PLOTDATE: 1/10/2019 4:18 PM g:\admin



Civil Engineering Consultants

323 Deinhard Lane, Suite C

PO Box 2330

McCall, Idaho 83638

(208) 634-4140

From: [Garrett de Jong](#)
To: [Brian Parker](#)
Cc: [John Powell](#)
Subject: RE: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard
Date: Friday, November 5, 2021 10:53:01 AM
Attachments: [image004.png](#)
[image005.png](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

RE: CUP-21-04 & DR-21-38: 201 Rio Vista Blvd – Susan Evans of Evergreen Gems and Cloud 9 Lodge

Hi Brian,

The building at 201 Rio Vista Blvd was likely built under the residential code. Idaho's adopted fire code correlates with the Building Code, in which commercial buildings are constructed. The fire code and building code do not apply to one- and two-family dwellings built under the residential code, which this home originally was. The city building official determines occupancy classification at the time of construction and can change it if the use of the building changes.

The fire code defines occupancies containing sleeping units where the occupants are primarily transient in nature as being classified as R-1, which includes boarding houses with more than ten occupants, congregate living facilities with more than ten occupants, hotels, and motels. A traditional home, where the occupants are primarily permanent in nature, is classified as R-3. The main difference between the two classifications regarding fire code is that R-1 occupancies require automatic fire sprinkler systems, but R-3 does not. In my opinion, based on the 2018 fire code, any building that sleeps over ten occupants, transient in nature, should have an automatic fire sprinkler system installed.

Because of the use classification, my comments are recommendations to the City of McCall Planning and Zoning and the City of McCall Building Official based on applying the 2018 International Fire Code to the intended use of the building, R-1. They are recommendations as the fire district does not have the authority to inspect or enforce fire code on one-and-two family dwellings.

Recommendations:

- An automatic sprinkler system installed in accordance with Section 903.3 shall be provided throughout the building. (IFC2018 903.2.8 Group R). The building fire alarm system shall monitor the automatic fire extinguishing system in accordance with NFPA 72 (IFC2018 904.3.5).
- Per (IFC2018 [A] 102.3 Change of use or occupancy), a building code analysis should be done to bring the structure up to building code standards, as determined by the city building official, including smoke detectors and carbon monoxide detectors installed following current building code and for the intended use.
- A parking plan for guest vehicles should be developed to make the driveway accessible for emergency vehicles (IFC2018 403.12.2).

Thank you,

Garrett

Garrett de Jong
Fire Chief
McCall Fire & EMS

201 Deinhard Lane
McCall, ID 83638
208-634-7070



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This message has been sent to you as official business of the McCall Fire Protection District. If you have a concern about the authenticity of this communication, including any attachments, please contact the sender directly for confirmation, either by telephone or separate e-mail. Unencrypted e-mail is inherently insecure and should be treated with caution.

Electronic Privacy Notice. This e-mail, and any attachments, contains information that is, or may be, covered by the Electronic Communications Privacy Act, 18 U.S.C. 2510-2521, and is also confidential and proprietary in nature. If you are not the intended recipient, please be advised that you are legally prohibited from retaining, using, copying, distributing, or otherwise disclosing this information in any manner. Instead, please reply to the sender that you have received this communication in error, and then immediately delete it. Thank you in advance for your cooperation.

From: Brian Parker <barker@mccall.id.us>

Sent: Thursday, November 4, 2021 2:21 PM

To: Bob Fodrea <bob@fodrealandgroup.com>; Chip Bowers <chip@bowerslandsurveys.com>; Chris Curtin <ccurtin@mccall.id.us>; Cynda Herrick <cherrick@co.valley.id.us>; Dale Caza <dcaza@plrwsd.org>; David Simmonds <dsimmonds50@gmail.com>; Delta James <djames@mccall.id.us>; Garrett de Jong <garrett@mccallfire.com>; Gene Trantham <gtrantham@mccall.id.us>; IDL Jurisdictional Inbox <IDL_jurisdictional@idl.idaho.gov>; ITD Development Services <D3Development.Services@itd.idaho.gov>; ITD District 3 Permits <ITDD3Permits@itd.idaho.gov>; Jasen King, IDL <jking@idl.idaho.gov>; Jeff Bateman <jbateman@plrwsd.org>; jennifer.schildgen@itd.idaho.gov; John Powell <jpowell@mccall.id.us>; Jordan Messner <jordan.messner@idfg.idaho.gov>; jthrower@gmail.com; Kurt Wolf <kwolf@mccall.id.us>; Lance Holloway, DEQ <lance.holloway@deq.idaho.gov>; Laura Shealy BPLWQAC <idchik5@gmail.com>; Laurie Frederick, Valley Co Cartographer <lfrick@co.valley.id.us>; Levi Brinkley <lbrinkley@mccall.id.us>; Linda Stokes <lstokes@mccall.id.us>; Lori Hunter (lhunter@co.valley.id.us) <lhunter@co.valley.id.us>; Lorraine Brush <lbrush@plrwsd.org>; Mark Wasdahl, ITD <mark.wasdahl@itd.idaho.gov>; Meredith Todd <mtodd@mccall.id.us>; Michelle Groenevelt <mgroenevelt@mccall.id.us>; Mike Reno

<mreno@cdh.idaho.gov>; Morgan Stroud <mstroud@mccall.id.us>; Nathan Stewart <nstewart@mccall.id.us>; Regan Berkley <regan.berkley@idfg.idaho.gov>; Richard Stein <rstein@mccall.id.us>; Sabrina Sims <:ssims@mccall.id.us>; Sarah Arjona <Sarah.Arjona@itd.idaho.gov>; Scott Corkill, IDL <scorkill@idl.idaho.gov>; Sheri Staley - Idaho Power <sstaley@idahopower.com>; Steve Moser, Idaho Power <smoser@idahopower.com>; Valley County Road Dept <roaddept@co.valley.id.us>

Subject: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

All,

Please provide comment on the attached application by December 7th, 2021.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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PAYETTE LAKES RECREATIONAL WATER AND SEWER DISTRICT

201 Jacob Street • McCall, Idaho 83638 • office 208-634-4111 • fax 208-634-7613

November 16, 2021

Brian Parker
City of McCall
216 East Park Street
McCall, Idaho 83638

RE: CUP-21-04, 201 Rio Vista Boulevard

Brian:

Payette Lakes Recreational Water and Sewer District (District) has reviewed the proposed project at the above referenced address, and has the following comments.

1. There is a sewer main line and manholes in the area of construction/landscaping, the sewer main line and manholes shall be protected from damage during landscaping and construction of the proposed improvements to prevent excessive water, or debris from entering the sewer system. Failure to do so will result in the owner/contractor being responsible for any cost incurred by the District for cleaning or removing debris that enters the sewer system.
2. No trees or permanent structures are allowed within the existing sewer easement.
3. We understand that "porta potties" will be utilized for events.

Please contact me should you have any questions.

Best Regards,

A handwritten signature in blue ink that reads "Dale Caza".

Dale Caza
Manager, Payette Lakes Recreational Water and Sewer District
Email: dcaza@plrwsd.org

From: [John Powell](#)
To: [Brian Parker](#)
Cc: [Fire District, Garrett DeJong](#)
Subject: RE: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard
Date: Thursday, November 18, 2021 3:06:01 PM
Attachments: [image001.png](#)
[image002.png](#)

Hi Brian,

As a follow-up to Garrett's email, I agree with him. Here is a summary of how the project relates to the building code requirements:

1. Bed & Breakfast – Currently approved for 19 occupants. If the applicant wishes to increase the number of occupants the occupancy type will change to R-1 and fire sprinklers will be required, along with other potential upgrades.
2. Event Center – An event center is a different occupancy type. It would be classified as an Assembly Occupancy. This would also activate the requirement for fire sprinklers and fire walls, etc. If the applicant wishes to use the grounds as an event center, the Bed & Breakfast structure may not be used as part of the Event. (Unless a code study and code upgrades are made to comply with an A-3 Occupancy.) Additionally, should the kitchen be used for Events, the kitchen would need to be upgraded to a commercial kitchen.

I think this covers it from my end. Garrett, let us know if I missed something.

Thanks,

John Powell | Building Official

216 E. Park St. | McCall, Idaho 83638

Phone: 208.634.8648 | www.mccall.id.us

From: Brian Parker <barker@mccall.id.us>
Sent: Wednesday, November 10, 2021 1:02 PM
To: James Evans <jim@evergreengems.com>; Susan Evans <susan@evergreengems.com>
Cc: Garrett de Jong <garrett@mccallfire.com>; John Powell <jpowell@mccall.id.us>
Subject: FW: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

Jim and Susan,

Please see the comments below from the McCall Fire Chief.

Thank you,

Brian Parker, AICP | City Planner

216 E. Park Street | McCall | Idaho 83638

Direct: 208.634.4256 | Fax: 208.634.3038

From: [Kurt Wolf](#)
To: [Brian Parker](#)
Cc: [Edward Heider](#)
Subject: RE: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard
Date: Friday, November 26, 2021 1:23:34 PM
Attachments: [image001.png](#)
[image002.png](#)

Brian,

I would like to express our concern regarding the noxious weed problems this property has and encourage planning and zoning to make it a condition of approval that these issues be addressed. It would be our recommendation that they have a mitigation plan to control and manage this issue both during construction and into the future. I have copied Eddie on this as well and we would be happy to consult with the property owner regarding this concern, as would the County Weeds Department.

Thank You,

Kurt K Wolf
Parks & Recreation Director, City Arborist
City of McCall – Parks & Recreation Dept.
ISA Certified Arborist (PN-7353A) – ISA Tree Risk Assessment Qualification
American Society of Landscape Architects (ASLA)
www.mccallparksandrec.com
216 E. Park Street
McCall, Idaho 83638
Office: 208-634-8967
Cell: 208-315-0063
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)

From: Brian Parker <bparker@mccall.id.us>
Sent: Thursday, November 4, 2021 2:21 PM
To: Bob Fodrea <bob@fodrealandgroup.com>; Chip Bowers <chip@bowerslandsurveys.com>; Chris Curtin <ccurtin@mccall.id.us>; Cynda Herrick <cherrick@co.valley.id.us>; Dale Caza <dcaza@plrwsd.org>; David Simmonds <dsimmonds50@gmail.com>; Delta James <djames@mccall.id.us>; Garrett de Jong (garrett@mccallfire.com) <garrett@mccallfire.com>; Gene Trantham <gtrantham@mccall.id.us>; IDL Jurisdictional Inbox <IDL_jurisdictional@idl.idaho.gov>; ITD Development Services <D3Development.Services@itd.idaho.gov>; ITD District 3 Permits <ITDD3Permits@itd.idaho.gov>; Jasen King, IDL <jking@idl.idaho.gov>; Jeff Bateman <jbateman@plrwsd.org>; jennifer.schildgen@itd.idaho.gov; John Powell <jpowell@mccall.id.us>; Jordan Messner <jordan.messner@idfg.idaho.gov>; jthrower@gmail.com; Kurt Wolf <kwolf@mccall.id.us>; Lance Holloway, DEQ <lance.holloway@deq.idaho.gov>; Laura Shealy BPLWQAC <idchik5@gmail.com>; Laurie Frederick, Valley Co Cartographer <lfriderick@co.valley.id.us>; Levi Brinkley <lbrinkley@mccall.id.us>; Linda Stokes <lstokes@mccall.id.us>; Lori Hunter (lhunter@co.valley.id.us) <lhunter@co.valley.id.us>; Lorraine Brush <lbrush@plrwsd.org>; Mark Wasdahl, ITD <mark.wasdahl@itd.idaho.gov>; Meredith Todd <mtodd@mccall.id.us>; Michelle Groenevelt <mgroenevelt@mccall.id.us>; Mike Reno <mreno@cdh.idaho.gov>; Morgan Stroud <mstroud@mccall.id.us>; Nathan Stewart

<nstewart@mccall.id.us>; Regan Berkley <regan.berkley@idfg.idaho.gov>; Richard Stein <rstein@mccall.id.us>; Sabrina Sims <ssims@mccall.id.us>; Sarah Arjona <Sarah.Arjona@itd.idaho.gov>; Scott Corkill, IDL <scorkill@idl.idaho.gov>; Sheri Staley - Idaho Power <sstaley@idahopower.com>; Steve Moser, Idaho Power <smoser@idahopower.com>; Valley County Road Dept <roaddept@co.valley.id.us>

Subject: City of McCall Request for Comment - CUP-21-04 - 201 Rio Vista Boulevard

All,

Please provide comment on the attached application by December 7th, 2021.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



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Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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Meredith Todd

From: Morgan Stroud
Sent: Thursday, November 18, 2021 3:52 PM
To: susan@evergreengems.com
Cc: Brian Parker; Meredith Todd
Subject: CUP21-04 - 201 Rio Vista Blvd - Engineering Review #1

The Public Works Department has reviewed the documents submitted for review for **CUP21-04** and have the following comments and concerns.

1. Thank you for the detailed stormwater report update. It appears that the project will likely be able to comply with the Drainage Management Guideline's, subject to the following update to the "Grading, Drainage & Storm Water Management Plan". I did not see if there were any proposed BMP's for the construction that is happening with this project. There is an area shown that identifies that a 50 foot vegetative buffer needs to be maintained. Please update this plan sheet to show clearing limits and other BMP's that should be implemented during construction.
2. Obstructions proposed within the Rio Vista Blvd right-of-way will need to be removed, including the private bus stop and trash cans. If the bus stop is considered public, we will need to discuss this is more with the Public Works Staff in detail to determine if it can be located within the public rights-of-way.
3. Please identify if the existing water service will suffice for the fire requirements needed by the McCall Fire & EMS. If the existing water service needs to be altered or adjusted, please identify the changes on your utility plan and resubmit it for review and comments.
4. This property already has an active Public Works permit. This project will need to follow and complete improvements that are detailed in that permit. If changes are being proposed to the work identified in that permit. Please contact Cris Malvich at 208.634.8946 to discuss the changes and next steps.

The comments above shall be addressed prior to Final Engineering Approval. Please continue to work with me to address the comments above. Final Engineering Approval is required prior to building permit issuance. Please let me know if you have any questions or concerns.

Thank you,
Morgan Stroud, E.I.T. | Staff Engineer
216 E Park Street | McCall | ID 83638
Direct: 208.634.3458 | Cell: 208.315.2299



November 29, 2021

RECEIVED

By Meredith Todd at 4:18 pm, Nov 30, 2021

To: Brian Parker City Planner
216 E. Park St.
McCall, ID. 83638

From: Kent W. Graefe/Tammy L. Buck
221 Rio Vista Blvd.
McCall, Id. 83638

Subject: Public Hearing on 201 Ernesto Event Venue.

Dear Brian and Planning and Zoning Commission Members,

Thank you for allowing us to comment on the propose expansion of the CUP by Susan Evans/Cloud 9 Lodge. We reside **full-time** on a 5 acre parcel 2 lots to the East of the proposed Event Venue (Approximately 450 feet). It is a tranquil existence.

We as property owners have major concerns regarding this expansion. Our primary concern is **NOISE**. By creating exterior “party zones” you are defacto creating a place where outdoor voices and loud music are encouraged. All the landscaping and water features will do nothing to curtail ambient noise which will impact our quality of life directly. Loud wedding parties with 80’s music are not a good mix with a quiet neighborhood.

We are not opposed to the increase from 19 to 26 guests as long as they are **housed inside** the Cloud 9 Lodge. Any form of “Dry Camping” is absurd. It will generate noise that impacts the entire neighborhood. Therefore, we are emphatically opposed to any expansion of the property for any additional exterior use including “Dry Camping”, or RV Parking. We all know that this will create a PARTY ZONE that will create a substantial volume of Noise. The summer and Indian Summer are special times when locals can sit outside on their deck, or open their windows to let in fresh air. **Any plans for outdoor expansion should be opposed by all P&Z Commissioners to protect the adjacent land owners.**

The original CUP was written to allow a Bed and Breakfast to exist, and prosper. We respect that. They have the right to operate a B and B for a profit. We do not feel they have the right to expand the operation into something that resembles a KOA Campground with outdoor music/concerts. It is a quiet neighborhood.

The concerns of parking and septic use are another issue if gatherings are allowed they may draw in 50 to 75 seasonal participants. We will let others elaborate on that impact in effect to their lives, and property.

Once again, thanks for allowing us to comment publicly! We hope you will do your best to oppose any outdoor expansion plans by the Cloud 9 Lodge. McCall is a very special place for us, and solitude can easily be disrupted.

Thanks again,

Kent W. Graefe
Tammy L. Buck
Property Owners at 221 Rio Vista

From: [Brian Parker](#)
To: [Christina Nemecek](#); [Liz Rock](#); [Ryan Kinzer](#); [jstunnell@gmail.com](#); [Tony Moss](#)
Cc: [Meredith Todd](#)
Subject: FW: Northwest Passage Event Center Proposal
Date: Wednesday, December 1, 2021 9:52:25 AM
Attachments: [image001.png](#)
[image002.png](#)

RECEIVED
By Meredith Todd at 12:28 pm, Dec 01, 2021

Commissioners,

Please see the comment below regarding CUP-21-04 located at 201 Rio Vista Boulevard.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038



Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)



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From: J S <jpsanchez27@gmail.com>
Sent: Wednesday, December 1, 2021 9:46 AM
To: Brian Parker <bparker@mccall.id.us>
Subject: Northwest Passage Event Center Proposal

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Brian,

My name is Jon Sanchez. I live on Rio Vista Blvd. I understand comments have been closed so I thought I would send an email. It's very concerning that this place will be used for an event center. It is right next to many homes and on a road that is not typically made for heavy traffic.

It's my understanding that they will be having weddings, and other events during the Summer and perhaps Spring and Winter. These will likely include late nights, and DJ's and other very busy and loud events. What is the plan to mitigate DJ noise during the day or evening and what times are

being proposed for curfew?

The many houses that surround this area will likely not be very happy with the noise and crowds and not something they bought into. For my part, I get a large amount of traffic already. With events going on, I imagine this will increase by several factors. What plans are in place to handle the increased traffic?

My opinion is that this is not the right area to put something like this but I would like an answer to my questions if there are answers available.

Thank you,
Jon

From: [Laura Wennerlund](#)
To: [Brian Parker](#)
Cc: [Meredith Todd](#)
Subject: Comments >> CUP-21-04 & DR-21-38: 201 Rio Vista Blvd
Date: Thursday, December 2, 2021 1:34:28 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Brian:

As residents of McCall, located at 214 Rio Vista Blvd, my husband and I are extremely concerned for us and our entire residential neighborhood if the city approves the conditional use permit allowing 201 Rio Vista, a B&B/ short term rental property, to expand it's use to be an Event Party Venue.

As part of the application process, the owners of 201 Rio Vista are required to hold Public Hearings for surrounding home owners. In this case, the owners neglected to notify us by mail, email, text, or phone call. Because of this, we apologies our feedback/comments are tardy yet hope our concerns will be heavily considered.

My husband and I purchased our McCall home located at 214 Rio Vista Blvd in 2018 for a variety of reasons; for the love of McCall, the peace and serenity of living on a quiet residential street, and to partake in all the wonderful recreational activities in McCall and surrounding areas. Rio Vista is residential street, lined with single family homes landscaped with the natural beauty of the mountains and area. Daily you see children out at play, families out walking, biking, etc., all the while feeling safe and secure in the Rio Vista neighborhood.

While the property located at 201 Rio Vista Blvd might be zoned slightly different than those of us who own properties across the street, the location of 201 Rio Vista sits next to and well within single family residences. And as such, a Commercial business for the sole purpose to host large party events, with guest range of 50-200 people, truly has the potential to destroy not only the enjoyable family atmosphere but could potentially cause safety and environment problems for all us home owners.

Our list of concerns include: (not listed in any particular order of priority)

- Devalue residential property who are within a stones throw from a commercial event venue
- Can McCall water facility accommodate the addition strain on public drinking water and fire protection ?

- Heavy street traffic and traffic safety on Rio Vista all day and night
- Adequate parking stalls located at 201 Rio Vista to accommodate all staff and guest vehicles. OR will vehicles have to park on Rio Vista?
- Heavy foot traffic on Rio Vista street for staff and guests who park on street
- Neighborhood and public safety with hundreds and thousands over time, of strangers in the neighborhood
- Noise pollution stemming from cars, people, event PA system, catering staff and trucks, port-a potties drop-off and pick-up, event tent construction and take down, Bands- DJ's playing during day and night
- Guests who consume too much alcohol during event party
- Trash will cause an increase in bear traffic in the neighborhood
- Trespassing, property damage by guests attending events
- Property and Street Lighting for public safety
- Public sewage capacity at B&B with the increase in usage
- Cellular and Cable broadband / bandwidth degradation in the neighborhood when hundreds of guest devices try to connect
- What are residents of Rio Vista recourse should to many calls of service occur ?

Questions:

- Will the property owner hire staff to run, manage, crowd control every event ?
- How many Party Events per week, month and year will be authorized and legal?
- What is the maximum people capacity allowable at events? Maximum should be inclusive of event staff.
- What is the maximum vehicle capacity that can legally park onsite at 201 Rio Vista property ?
- Will the City allow guest vehicles, catering trucks, port-a potties to park directly on Rio Vista Blvd ?
- What are Event party hours of operations?
- Will multi-day events occur ?
- Will RV's, Campers, Tents, etc be allowed to park on the property to accommodate overnight event guests ?
- What are the allowable/legal sound level, noise limits during the day and night?
- What are noise mitigation strategies?
- Was a traffic, safety study done for Rio Vista to accommodate an event venue commercial business ? What are the findings?
- Does the city have personnel who can handle increase calls for service? e.g... mischief and damage done by event guests of the surrounding area/properties
- Is alcohol service to Event party guests permissible? Either paid by guests or full host bar
- Does McCall have plans to improve cellular and broadband connection services for residents, so no impact is felt when Events are in full swing?

To quickly summarize, A residential neighborhood is no place for a major commercial venture envisioned by this proposal. Rio Vista should remain residentially zoned not overrun by hundreds of cars, constant traffic, late night parties, hundreds of guests located feet from families with children and bedroom windows. In addition, having our property value be completely decimated, as no one would want to purchase a family and-or a 2nd home within hundreds feet from a commercial business event venue.

We just want to live our lives in a peaceful, quiet neighborhood that we chose to invest in and to build a life in a neighborhood that is 99% zoned as residential, not zoned business.

We hope the City, County, P&Z, Committee member et al will **NOT** approve this application. Thank you for your time.

Thanks,

Eric & Laura Wennerlund

On Nov 30, 2021, at 10:25 AM, Brian Parker <bparker@mccall.id.us> wrote:

The application materials can be found at the following link: [<image001.png>](#)
[Application Materials](#). This will expire on 12/7, so please download any files you may need to keep after that. The staff report and draft findings of fact should be published by the end of business today at <https://www.mccall.id.us/planning-zoning-agendas-minutes>.

Please let me know if you have any questions.

Thank you,

Brian Parker, AICP | City Planner
216 E. Park Street | McCall | Idaho 83638
Direct: 208.634.4256 | Fax: 208.634.3038
<[image002.png](#)>
Web: mccall.id.us
Blog: mccallcitysource.com
Social: [Facebook.com/cityofmccall](https://www.facebook.com/cityofmccall)

<image003.png>

[Please click to sign up for CodeRED!](#)

From: Laura Wennerlund <lwennerlund@apple.com>

Sent: Tuesday, November 30, 2021 10:19 AM

To: Brian Parker <bparker@mccall.id.us>

Subject: CUP-21-04 & DR-21-38: 201 Rio Vista Blvd

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Brian,

Per the public hearing notice, could you please email me the application and design review plans that will be under review this coming December 7, 2021.

My husband and I live at 214 Rio Vista Blvd and have concerns about an event center in a residential neighborhood. We would like all the information to fully assess the impacts to the neighborhood including real estate values.

Thank you in advance,

Laura

Laura Wennerlund • Education Account Executive | 208-830-8774

From: [Scott Cyr](#)
To: [Brian Parker](#)
Subject: CUP-21-04 & DR-21-38: 201 Rio Vista Blvd
Date: Sunday, December 5, 2021 7:17:31 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Brian:

As residents of McCall, located at 218 Rio Vista Blvd, my husband and I are extremely concerned for us and our entire residential neighborhood if the city approves the conditional use permit allowing 201 Rio Vista, a B&B/ short term rental property, to expand its use to be an Event Party Venue.

As part of the application process, the owners of 201 Rio Vista are required to hold Public Hearings for surrounding home owners. We are 1 property away, approximately 45' beyond the 300' notification and just noticed the two 8.5" x 11" notices, apologies that our feedback/comments are tardy yet hope our concerns will be heavily considered.

My husband and I purchased our McCall home located at 218 Rio Vista Blvd in 2018 for a variety of reasons; for the love of McCall, our retirement home, the peace and serenity of living on a quiet residential street within city limits, and to partake in all the wonderful recreational activities in McCall and surrounding areas. Rio Vista is residential street lined with single family homes landscaped with the natural beauty of the mountains and area. Daily you see children out at play, families out walking, biking, etc., all the while feeling safe and secure in the Rio Vista neighborhood.

The property located at 201 Rio Vista Blvd is zoned R3 not commercial and sits next to and well within single family residences. And as such, a Commercial business for the sole purpose to host large party events, with guest range of 50-200 people, truly has the potential to destroy not only the enjoyable family atmosphere but could potentially cause safety and environment problems for all us home owners.

Our list of concerns include: (not listed in any particular order of priority)

- Devalue residential property who are within a stone's throw from a commercial event venue
- Can McCall water facility accommodate the addition strain on public drinking water and fire protection?
- Heavy street traffic and traffic safety on Rio Vista all day and night
- Adequate parking stalls located at 201 Rio Vista to accommodate all staff and guest vehicles. OR will vehicles have to park on Rio Vista? (Winter snow banks)

- Heavy foot traffic on Rio Vista street for staff and guests who park on street
- Neighborhood and public safety with hundreds and thousands over time, of strangers in the neighborhood
- Noise pollution stemming from cars, people, event PA system, catering staff and trucks, port-a potties drop-off and pick-up, event tent construction and take down, Bands - DJ's playing during day and night
- Guests who consume too much alcohol during event party
- Trash will cause an increase in bear traffic in the neighborhood
- Trespassing, property damage by guests attending events
- Property and Street Lighting for public safety
- Public sewage capacity at B&B with the increase in usage
- Cellular and Cable broadband / bandwidth degradation in the neighborhood when hundreds of guest devices connect

Questions:

- Does the current zoning allow for this type of use?
- Will the property owner hire staff to run, manage, crowd control every event?
- How many Party Events per week, month and year will be authorized and legal?
- What is the maximum people capacity allowable at events? Maximum should be inclusive of event staff.
- What is the maximum vehicle capacity that can legally park onsite at 201 Rio Vista property?
- Will the City allow guest vehicles, catering trucks, port-a potties to park directly on Rio Vista Blvd?
- What are Event party hours of operations?
- Will multi-day events occur?
- What are residents of Rio Vista recourse should to many calls of service occur?
- Will RV's, Campers, Tents, etc. be allowed to park on the property to accommodate overnight event guests?
- What are the allowable/legal sound level, noise limits during the day and night?
- What are noise mitigation strategies?
- Was a traffic, safety study done for Rio Vista to accommodate an event venue commercial business? What are the findings?
- Does the city have personnel who can handle increase calls for service? e.g... mischief and damage done by event guests of the surrounding area/properties
- Is alcohol service to Event party guests permissible? Either paid by guests or full host bar

- Does McCall have plans to improve cellular and broadband connection services for residents, so no impact is felt when Events are in full swing?

To quickly summarize, a residential neighborhood is no place for a major commercial venture envisioned by this proposal. Rio Vista should remain residentially zoned not overrun by hundreds of cars, constant traffic, late night parties, hundreds of guests located feet from families with children and bedroom windows. In addition, having our property value be completely decimated, as no one would want to purchase a family and-or a 2nd home within hundreds feet from a commercial business event venue.

We just want to live our lives in a peaceful, quiet neighborhood that we chose to invest in and to build a life in a neighborhood that is 99% zoned as residential, not zoned business.

We hope the City, County, P&Z, Committee member et al will **NOT** approve this application. Thank you for your time.

Thanks,

Scott and Nancy Cyr
218 Rio Vista
Mc Call, ID
(208) 921-3374

November 19, 2021

Suzie Rohnert

Toby's Place

506 Pine St

McCall, ID 83638

Delivered via e-mail to suzie@tobysplace.org and jim@rm-signs.com

RE: Toby's Place Sign Application (SG-21-11)

This letter hereby provides City of McCall administrative approval of the signs represented in the sign design review application (received November 8th, 2021) which is described as follows:

Design:

- One (1) wall-mounted sign for the property at 506 Pine St.
- The sign will display the business name, 'Toby's Place; Thrift Shop,' in charcoal print against a pale green background with the business logo of a log cabin in the center.
- The hanging sign will measure 6' X 8' or approximately 48 sq. feet.
- The signage will be made of flat vinyl covering aluminum backboard, and be mounted flat against the North facing wall above the snowlevel.

Location:

- The replacement signs will be hung on the north side of the building, facing Railroad Ave./Pine St., under the eaves of the roof and out of the alleyway right of way.

Lighting:

- No new lighting is proposed at this time.

The linear dimension of the building frontage for 506 Pine Street is approximately 114 feet, thereby allowing a maximum of 100 square feet for the whole property. The existing signage between Toby's Place and the church on the property totals 46 sq. feet. Therefore, the proposed 48 sq. ft. of signage is within the allowable square footage for the facility, leaving 4 square feet remaining in the allowable limit. Should additional new signage or a change to signage be desired, the applicant must submit a new sign application with a copy of this application indicating remain square footage permissible. The colors proposed meet those suggested within the McCall Design Guidelines, and the proposed method of mounting is appropriate for the site and meets code.

Pursuant to McCall City Code (Title 3, Chapter 9; Chapter 14), the sign design meets the intent of the McCall Design Guidelines and **is hereby approved by the City of McCall**. If you have any questions about the contents of this letter, please feel free to contact me at mtodd@mccall.id.us.

Sincerely,

Meredith Todd

Assistant City Planner



1 ea single sided 6 x 8' sign. 48 sq ft.
Flat sign with no lighting.
Vinyl print on aluminum, colors as shown.

2022 McCall Area Planning and Zoning Commission Meeting Schedule and Application Deadlines



The McCall Area Planning & Zoning Commission reviews all development applications within the City of McCall and McCall Impact Area. The following are the scheduled **2022 McCall Area Planning and Zoning Meeting Dates and associated deadlines:**

Record of Survey, Shoreline or River Environs, or Scenic Route Applications

Full Application Deadline	Meeting Date
November 23	January 4
December 21	February 1
January 18	March 1
February 22	April 5
March 22	May 3
April 26	June 7
May 24	July 5
June 21	August 2
July 26	September 6
August 23	October 4
September 20	November 1
October 25	December 6

For more information about the McCall Area Planning & Zoning Commission or to submit applications, please contact:

Brian Parker
City Planner
216 East Park Street
McCall, ID 83638
bparker@mccall.id.us
(208) 634-4256

Design Review and Subdivision Preliminary Plat Applications

Full Application Deadline	Public Notification Deadline (for City to provide materials to applicants)	Meeting Date
November 23	December 13	January 4
December 21	January 10	February 1
January 18	February 7	March 1
February 22	March 14	April 5
March 22	April 11	May 3
April 26	May 16	June 7
May 24	June 13	July 5
June 21	July 11	August 2
July 26	August 15	September 6
August 23	September 12	October 4
September 20	October 10	November 1
October 25	November 14	December 6

Conditional Use Permit, Planned Unit Development, Subdivision Final Plat or Minor Plat Amendment, Variance, Rezone, Zoning Code Amendment, Annexation, and Vacation Applications

Pre-Application Submittal Deadline	Pre-Application Meeting Date	Application Submittal Deadline	P&Z Notice Date	P&Z Hearing Date	City Council Notice Date ⁴	Earliest City Council Hearing Date ⁴
December 14, 2021	January 4, 2022	January 18, 2022	February 7, 2022	March 1, 2022	March 7, 2022	March 24, 2022
January 11, 2022	February 1, 2022	February 22, 2022	March 14, 2022	April 5, 2022	April 11, 2022	April 28, 2022
February 8, 2022	March 1, 2022	March 22, 2022	April 11, 2022	May 3, 2022	May 9, 2022	May 26, 2022
March 15, 2022	April 5, 2022	April 26, 2022	May 16, 2022	June 7, 2022	June 27, 2022	July 14, 2022
April 12, 2022	May 3, 2022	May 24, 2022	June 13, 2022	July 5, 2022	July 11, 2022	July 28, 2022
May 17, 2022	June 7, 2022	June 21, 2022	July 11, 2022	August 2, 2022	August 8, 2022	August 25, 2022
June 14, 2022	July 5, 2022	July 26, 2022	August 15, 2022	September 6, 2022	September 26, 2022	October 13, 2022
July 12, 2022	August 2, 2022	August 23, 2022	September 12, 2022	October 4, 2022	October 10, 2022	October 27, 2022
August 16, 2022	September 6, 2022	September 20, 2022	October 10, 2022	November 1, 2022	November 7, 2022	November 24, 2022
September 13, 2022	October 4, 2022	October 25, 2022	November 14, 2022	December 6, 2022	December 26, 2022	January 12, 2023
October 11, 2022	November 1, 2022	November 22, 2022	December 12, 2022	January 3, 2023	January 9, 2023	January 26, 2023
November 15, 2022	December 6, 2022	December 27, 2022	January 16, 2023	February 7, 2023	February 20, 2023	March 9, 2023

Notes:

1. Public Notification Deadlines are the dates by which public notices must be submitted to the local newspaper, other notification deadlines (i.e. mailing and posting) must be completed 15 days prior to the meeting.
2. Full applications for land use permits requiring a pre-application presentation (i.e. CUP, SUB, PUD, ZON) will be accepted no earlier than the next deadline after the pre-application P&Z meeting date.
3. All dates are tentative and may be modified at the discretion of City staff.
4. For applications within the City of McCall. Area of Impact applications are to be scheduled with the Valley County Board of County Commissioners. Staff will coordinate scheduling with the County Clerk.